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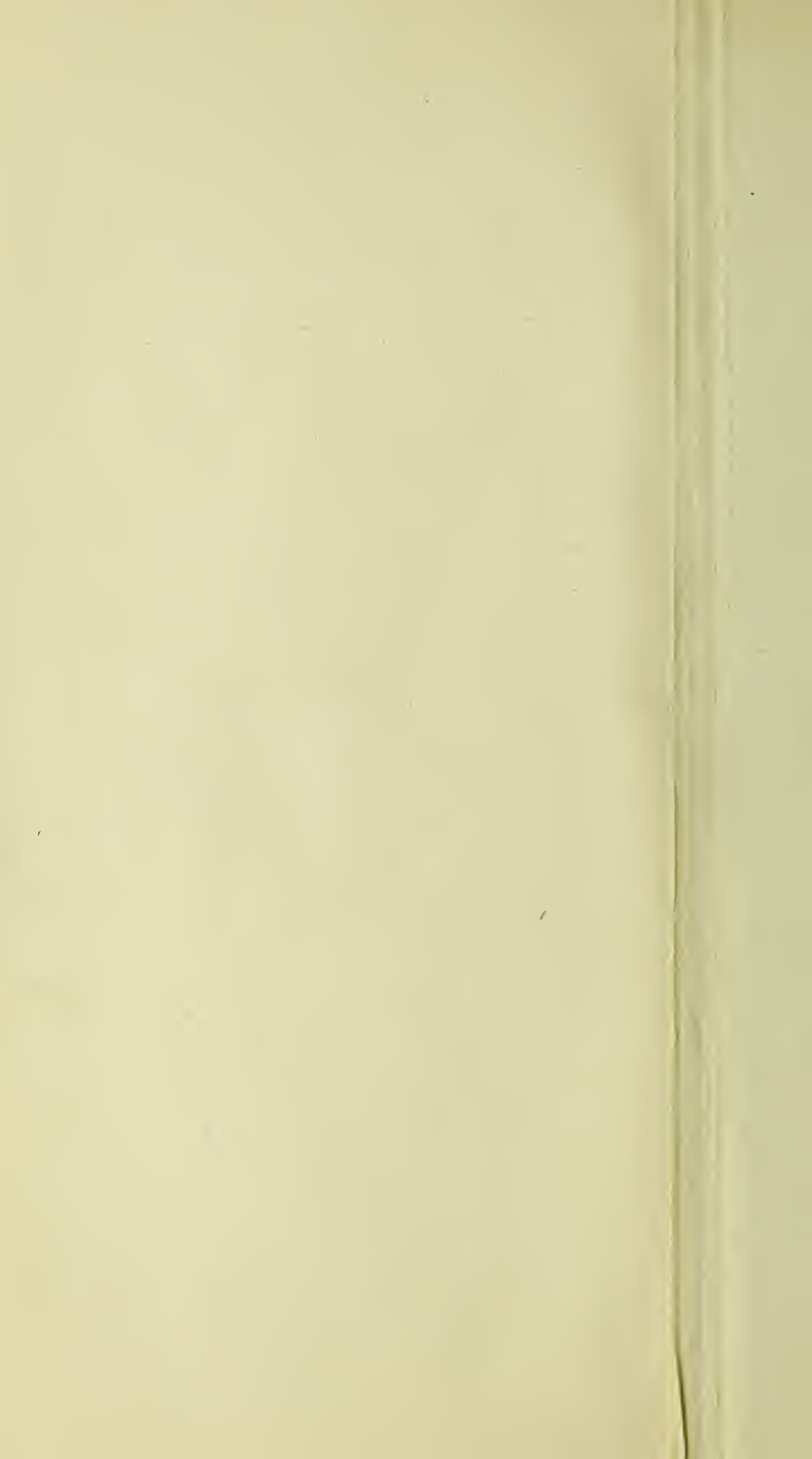
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TENTH
ANNUAL REPORT

OF THE
RAILROAD COMMISSIONER

OF THE
State of Vermont,

TO THE
GENERAL ASSEMBLY,

OCTOBER SESSION, 1865.

ST. ALBANS:
WHITING & DAVIS, PRINTERS.
1865.

1870

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RAILROAD COMMISSIONER'S REPORT.

To the General Assembly of the State of Vermont :

The undersigned respectfully submits the following report upon the condition and management of the railroads in this State, for the current official year :

In accordance with the duties of the Commissioner, I furnished to the several railroad companies of the State, blank forms, to be filled up by the officers thereof, agreeably to Chap. 28, Section 124, of the General Statutes, and I received the return of their several reports at the times here indicated, to wit :

Vermont Valley, August 25th,
Atlantic & St. Lawrence, August 28th,
Vermont & Massachusetts, August 30th,
Vermont & Canada, August 31st,
Vermont Central, September 2d,
Rutland & Whitehall, September 4th,
Connecticut & Passumpsic Rivers, September 13th,

to which I refer, as showing the financial condition and

management of the roads, for the current year. I have passed over the several roads at intervals during the year, and have walked over sections of the track of the different roads, but have given more attention to the examination of bridges, culverts, and high embankments, &c., than of the continuous line of track. I have found the roads comparatively in good condition, several of them in improved condition from that of last year. The branch road of the Vermont & Canada, from Swanton to the Provincial line, has been completed, and is now in fair running condition. The managers of the Vermont Central are now building some six hundred feet of most substantial bridge across White River, at Royalton.

The Rutland and Burlington Company has erected, at Rutland, one of the best designed and most substantial engine houses in the country. Apparently fire proof, it will be a durable monument of the creative intelligence and enterprise of the managers of that company.

The general management of passenger trains, so far as I have known, has been without complaint. Conductors have been uniformly courteous and attentive, as well as efficient for the comfort and safety of passengers while passing over their respective lines.

In the report of the last year, the attention of the Legislature was called to the fact, that there was no where existing any regulation as to the speed which passenger trains should be allowed to run upon Vermont railroads. From subsequent observation upon the subject, I am only confirmed in my own mind as to the need for some authoritative rule upon that particular matter. From the loose conclusions entertained even among railroad oper-

ators themselves, may be clearly seen the danger of leaving so important a matter to be determined by the courage, ambition, and even rashness and daring, of a single will. By some 'tis asserted, that there is less danger in running trains at high speed than at moderate. Facts are always more convincing than theories. In a late disaster upon the Hudson River Railroad, caused by a misplaced or forgotten switch, the engineer stated that he saw the danger forty rods ahead, but was running at the rate of sixty miles the hour, and it was impossible to check the train in the least, at so short distance. Had the speed been but twenty-five miles, that forty rods would have been equal to eighty, or more, and would have made escape possible. Engineers and brakemen understand this matter perfectly, and are generally found to jump for their lives, leaving the train to its fate.

Again, our railroads are not all laid in straight lines, but at intervals winding round hill sides, where the track is inclined according to the radius, and, of course, graduated to a medium rate of locomotion, so that if the speed were to be doubled, the inclination should be increased, or the tendency of the train to go off the track is inevitable.

But even on perfectly straight lines, too fast driving increases the hazard. Every locomotive while running at medium rate gives a vibratory motion to the truck, and as the train follows, there is a continuous concussion between the trucks and the track, and this is always increased in force by increase of speed, until some imperfect rail snaps at the sharp encounter, or some defective wheel bursts from the force of its own rapid revolutions.

Were our railroads made perfect, and kept so, the hazard of fast driving would be vastly less ; but they are not, though comparatively in as good repair as those of any State in the Union. Still, they are never perfect. Occasional sleepers get rotten before they are replaced by sound ones ; rails badly worn and splintered, before new ones take their place ; the extremes of temperature greatly affect the road bed itself ; so it is extremely difficult to keep a continuous line of Vermont railroad through all seasons of the year in perfect confident repair.

Now, I would suggest as a rule, that on all single track railroads, the speed of passenger trains should never exceed thirty miles the hour, and that rate would carry a man from his home, five hundred and fifty miles in every twenty-four hours, allowing about one-fourth of the entire time for delays. Locomotion at the rate of one mile in every two minutes ought to satisfy any reasonable ambition, especially on a road where other trains are approaching one at the same rate at all hours, in sunshine and storm, in day and darkness.

The managers of railroads, like those of all other greatly complicated enterprises, are not omniscient, but liable to mistake as to trustworthiness and efficiency of their subordinates, and if they are only prompt to discover and discharge from service every unworthy agent and employee, 'tis the best they can do, and should entitle them to commendation. There are but few who have a thought of the difficulties and embarrassments these managers have to encounter. The great demand from the traveling public is *haste*. The line that will carry them soonest

to their destination is always in favor. Hence, the time tables must be arranged with a view to satisfy such demand, and each separate road in the line must accept its allotted share of time, and keep up to it, in order to protect the whole against the rivalry of other lines. A popular New York journal makes the following assertion upon this point: "Railroad managers are masters of their own time tables. The public do not ask them to make fast schedules." This would all be well, provided it were true, but practically it is not. Suppose the Hudson River Company should decide to run their trains not exceeding twenty, or even twenty-five miles the hour, travel would all take to the steamers on the river, and the stock of that company would be worth less than Confederate bonds. So would it prove with the managers of all through lines. They would find that with the travelling public, distance is nothing, but time every thing. If a roundabout line makes better time than a direct one, the longer line will be sure to sell the tickets.

But, after all, the preservation of human life should be paramount to every other consideration, and while keeping pace with the improvements of the age, railroads will yet be made so perfect that trains will be run upon them sixty miles the hour with comparative safety. With our present roads 'tis rashness to attempt it, and should in no wise be allowed. The number of accidents reported by the officials of the several railroads in the State is less than last year. While the record of disaster, agony and death, resulting from recklessness and mismanagement of railroads in other States, is most shocking to contemplate, it is grateful to feel that the superintendence of our Ver-

mont roads is in more competent and safer keeping; that instead of becoming the instruments of wholesale slaughter, they have been made to minister so greatly to the convenience, the use and blessing of all; that men of business, the seeker after health or recreation, as well as our own war-worn, returning volunteers, have all found safe transit towards their several destinations; and as the year approaches its close, at various points along the lines, the fires are burning bright in forge and foundry; the shops are musical with the hum of machinery, while the ring and clatter of the busy workmen give promise of all needful providings for the work and wear of the coming years.

Respectfully submitted,

G. H. RICE, *Railroad Commissioner.*

October 17, 1865

DOCUMENTS.

Form of Annual Report required by the Commissioner of the several railroad companies for the year ending June 30, 1865 :

ANNUAL REPORT OF THE — RAILROAD COMPANY, FOR THE YEAR ENDING JUNE 30, 1865.

TABLE A.

STOCK AND DEBTS.

NOTE—Under this head state the amount paid for interest, discounts, &c., amount of funded debt, floating debt—rate of interest—amount of interest coupons due and unpaid.

TABLE B.

COST OF CONSTRUCTION.

Total cost of construction as per last report,
Expended since last report, viz. :
For graduation and masonry,
“ bridges,
“ rails,
“ chairs, spikes and ties,
“ laying superstructure,
“ buildings and fixtures,

NOTE—State, also, the progress of the work, cost of graduation, superstructure, and all incidental expense, on any extension or alteration of road, to June 30, 1865.

Also state the amount paid for interest, discount, &c., charged to construction account.

TABLE C.

EQUIPMENT.

Total cost of equipment as per last report, .
 Expended since last report,
 Total cost of road and equipment,

TABLE D.

CHARACTERISTICS OF ROAD.

Length of road,
 " " completed,
 " " branches,
 " " side track,
 Weight of rail per yard,

NOTE—State length of road, branches and side tracks within the State.

CHARACTER AND LENGTH OF BRIDGING.

	No. of structures	No. of spans.	Length of bridging in feet.
Trestle bridging,			
Truss bridging, 50 feet span and under,			
Truss do. from 50 to 100 feet span,			
Truss do. from 100 to 150 feet span,			
Truss do. 150 feet span and over,			
Draw bridges,			
Totals,			

Number of road crossings at grade,
 Number of road crossings above and below
 grade,
 Number of cross ties per mile,
 Chairs, number per mile,
 Whole number of switches on main track, ..

GRADIENTS AND ALIGNMENT.

Level, number of miles,
 Maximum grade,
 Amount of straight line, miles,
 Amount of curved line, miles,
 Maximum radius,
 Minimum radius,
 Sum of ascents going in one direction,
 Sum of ascents going in opposite direction,
 Height of termini & summit above tide water,

BUILDINGS AND FIXTURES.

Passenger houses,
 Freight houses,
 Engine houses,
 Repair shops,
 Water stations,
 Dwellings,
 Wood sheds,
 Turn tables,

Other buildings, as follows :

EQUIPMENT.

Number of cars owned by the company, June 30, 1865.

	Under 16 tons	16 to 20.	20 to 25.	25 to 30.	30 tons and over.
In good repair,					
Requiring slight repairs,					
Requiring heavy repairs,					
Worn out,					

First class 8 wheel passenger cars in good
 repair,
 First class 8 wheel passenger cars wanting
 repair,
 Second class 8 wheel passenger cars in good
 repair,

Second class 8 wheel passenger cars wanting
 repair,.....
 Baggage, express & mail cars in good repair,
 Baggage, express & mail cars wanting repair,
 Covered freight and cattle 8 wheel cars in
 good repair,.....
 Covered freight and cattle 8 wheel cars
 wanting repair,.....
 Platform 8 wheel cars in good repair,.....
 Other freight cars,.....
 Gravel cars,.....
 Average weight of passenger cars,.....
 Average weight of baggage cars,.....
 Average weight of box cars,.....
 Average weight of platform cars,.....

TABLE E.

BUSINESS OF THE YEAR.

Miles run by passenger trains,.....
 Miles run by freight trains,.....
 Miles run by gravel and construction trains,
 Miles run by wood trains,.....
 Number of through passengers carried in cars
 Number of way passengers,.....
 Number of passengers having passes,.....
 Number of miles traveled by way passengers,
 Average distance traveled by way pas-
 sengers,
 Number of miles traveled by passengers
 having passes,.....
 Number of tons of through freight,.....
 Number of tons of way freight,.....
 Number of tons of way freight carried 1 mile
 Number of tons of through freight moved
 towards market,.....
 Number of tons of through freight moved
 from market,.....
 Number of tons of way freight moved to-
 wards market,.....

Number of tons of way freight moved from market,	
Average rate of speed of ordinary passenger trains,	
Average rate of speed of express trains,	
Average rate of speed of freight trains,	
Rate of fare charged 1st class through passengers per mile,	
Rate of fare charged 1st class way passengers, per mile,	
Average rate of fare charged 2d class passengers, per mile,	
Rate per ton per mile charged on 1st class through freight,	
Rate per ton per mile charged on 2d class through freight,	
Rate per ton per mile charged on 3d class through freight,	
Rate per ton per mile charged on 1st class way freight,	
Rate per ton per mile charged on 2d class way freight,	
Rate per ton per mile charged on 3d class way freight,	
Rate per ton per mile charged on 4th class way freight,	
Total number of tons of freight carried 1 mile,	
Total number of passengers carried 1 mile,	

TABLE F.

EXPENSES OF MAINTAINING ROADWAY AND REAL ESTATE,

For the year ending June 30, 1865.

Ordinary repairs of road bed and super-structure,	
Extraordinary repairs of road bed,	
Cost of new rails used in repairs,	

Number and weight of chairs,.....	
Weight of spikes,.....	
Cost of repairs of rails,.....	
Number of cross ties used for renewals,...	
Cost of same,.....	
Cost of relaying rails and ties,.....	
Insurance and taxes on real estate,.....	
Repairs of bridges,.....	
" stations,.....	
" fences,.....	
" masonry,.....	
Total,.....	

COST OF REPAIRS OF MACHINERY.

Repairs of engines and tenders,.....	
Repairs of passenger and baggage cars,...	
Repairs of freight cars,.....	
Repairs of tools and machinery in work shops,.....	
Oil used about workshops,.....	
Fuel,	
Waste,	
Other items in detail as follows:	
Total,.....	

TABLE G.

COST OF OPERATING THE ROAD.

For the year ending June 30, 1865.

Wood, including cost of preparing the same,	
Number of cords of wood used by loco- tives,.....	
Number of tons of coal used by locomotives,	
Number of cords of wood used at stations,	
Number of tons of coal used at stations,...	
Number of cords lost by fire,.....	
Number of gallons of oil,.....	
Number of pounds of waste,.....	
Cost of oil & waste for engines & tenders,	

“	“	passenger & baggage cars,
“	“	freight cars,.....
Loss and damage of goods,.....		
Loss and damage of baggage,.....		
Damages for injuries to persons.....		
Damages to property, including fire and animals killed on road,.....		
Office expenses and stationery,.....		
Number of agents,....		
Number of clerks,.....		
Labor loading and unloading freight,.		
Porters and watchmen,.....		
Switchmen,		
Wood and water station attendance,.....		
Conductors and baggagemen,.....		
Brakemen,		
Enginemmen and firemen,.....		
For salaries of trustees, president, directors, secretaries, treasurer and superinten- dent,.....		
For printing, stationery and office expenses,		
For law expenses,		
Other expenses in detail, as follows :		
Total.....		

RECAPITULATION OF EXPENSES.

Maintaing roadway,.....
Repairs of machinery,.....
Operating,.....
Proportion of expenses due to passenger business,.....
Proportion of expenses due to freight busi- ness,.....

TABLE H.

EARNINGS, RECEIPTS AND PAYMENTS.

Earnings and Receipts.

From passengers.....

From freight,.....

From other sources, viz.:

Expresses,.....

Mails,.....

Rents,.....

Miscellaneous,.....

Payments other than for Construction.

For transportation expenses, viz.:

For passenger business,.....

For freight business,.....

For other business, and what,.....

For interest on funded debt,.....

For interest on floating debt,.....

For dividends,.....

For carried to surplus fund,.....

For amount of surplus fund,.....

VALUE OF MATERIALS ON HAND.

Wood, cords of,.....

Coal, tons of,.....

Oil, gallons of,.....

Waste, pounds of,.....

Iron rails, tons of, old,.....

Iron rails, tons of, new,.....

Chairs, pounds of,.....

Spikes, pounds of,.....

Ties, number of,.....

Iron and other metals, unwrought,.....

Iron and other metals, worked and partly

worked,.....

Lumber,.....

Other items specified as follows:

COST OF TRANSPORTATION.

Actual cost of transporting freight per ton,
per mile.....

Actual cost of transporting passengers, per
mile,.....

DETAILS OF EARNINGS FOR THE YEAR ENDING JUNE 30, 1865.

SOURCE.	JULY, 1864	AUGUST.	SEPT.	OCTOBER.	NOVEMBER	DECEMBER.
Through passengers.....						
Way passengers.....						
Through freight.....						
Way freight.....						
Express.....						
Transport of mails.....						
Use of engines.....						
Use of cars.....						
Rent.....						
Other earnings specified in detail as follows:						
Total.....						

DETAILS OF EARNINGS (CONTINUED.)

SOURCE.	JAN., 1865.	FEBRUARY.	MARCH.	APRIL.	MAY.	JUNE.
Through passengers.....						
Way passengers.						
Through freight.....						
Way freight.....						
Express.....						
Transport of mails.....						
Use of engines.....						
Use of cars.....						
Rent.....						
Other earnings specified in detail as follows:						
Total.....						

TABLE I.

ACCIDENTS.

State the number of persons injured in life or limb, and the cause thereof, and whether passengers or persons employed.

State whether any such accidents have arisen from carelessness or negligence of any person in the employ of the corporation, and whether such person is retained in the service of the corporation.

	EMPLOYEES.		OFFICERS.	
	killed.	injured.	killed.	injured.
Trains thrown from track,....				
Struck by bridge while on top of freight car.....				
Run over while walking on track				
Injured at road crossing.....				
Total.....				

Total number of persons killed,

Total number of persons injured but not killed,

In addition to which must be given a statement of the date of each accident, the place where it occurred, the train, the cause and the extent of the injuries inflicted upon each person, and the name of such person.

TABLE J.

NUMBER OF EMPLOYEES AND COMPENSATION.

Conductors of passenger trains,

Amount of compensation,

Conductors of freight trains,

Amount of compensation,

Conductors of wood and gravel trains,

Amount of compensation,

Master mechanics,
Amount of compensation,
Road masters,
Amount of compensation,
Men in repair shops,
Amount of compensation,
Enginemen of passenger trains,
Amount of compensation,
Enginemen of freight trains,
Amount of compensation,
Enginemen of wood and gravel trains,
Amount of compensation,
Firemen,
Amount of compensation,
Baggagemen,
Amount of compensation,
Switchmen,
Amount of compensation,
Section men, (foremen,)
Amount of compensation,
Section hands,
Amount of compensation,
Watchmen,
Amount of compensation,
Station agents,
Amount of compensation,
Other laborers,
Amount of compensation,
Clerks connected with passenger business,
Amount of compensation,
Clerks connected with freight business,
Amount of compensation.
Superintendent of bridges—salary,
Wood agent, “
Other agents—how employed, and the salary of each, as follows, viz. :

The treasurer is required to state the amount of surplus (if any) the amount of net earnings, on 31st Aug., 186—also the net earnings up to 30th June, 186—and to state amount of payments to surplus fund—payments of interest, coupons, on funded debt, and other disbursements in detail, so as to show the true condition of the finances of the company on the 30th day of June, 1865. Such statement may be in the form of a general account, and must be verified by the oath of the Treasurer.

STATE OF VERMONT, }
COUNTY, SS. }

day of 186

I, treasurer of
the railroad
company, do solemnly swear that the above is a true
statement of the condition of the finances of said company,
their trustees, or assignees or lessees, the 30th day of
June, 186

Treasurer

Sworn before me,

Justice of the peace.

OFFICERS OF THE COMPANY.

SALARIES.

Trustees,
President,
Superintendent,
Treasurer,

NOTE—State the amount of each.

STATE OF VERMONT, }
COUNTY, SS. }

depose and say

that the facts set forth, and statements made in the fore-
going report, which has been signed by are true and
correct according to the best of knowledge, infor-
mation and belief.

Signed,

Subscribed and sworn to before me this day of

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ANNUAL REPORT

OF THE VERMONT CENTRAL RAILROAD COM- PANY, FOR THE YEAR ENDING MAY 31, 1865.

TABLE A.

STOCK AND DEBTS.

One hundred thousand shares of Capital Stock issued.

\$2,000,000 first mortgage bonds issued at 7 per cent.
interest.

\$1,500,000 second mortgage bonds issued at 7 per cent.
interest.

TABLE B.

COST OF CONSTRUCTION.

Total cost of construction as per last report, \$8,402,054 92

TABLE C.

EQUIPMENT.

Within the year ending May 31, 1865, the following has
been added to the equipment of the road, in addition to
keeping up its equipment:

153 miles of telegraph line, with instruments, at a cost of.....	\$ 10,075 96
9 new first class locomotives,.....	180,000 00
6 new passenger cars,.....	21,000 00
20 new double decked stock cars,.....	20,000 00
54 new box freight cars,.....	48,600 00

TABLE D.

CHARACTERISTICS OF ROAD.

Length of road, Vermont Central,.....	117 miles
“ “ Vermont & Canada,.....	65 1-2 “
“ “ completed,.....	182 1-2 “
“ “ branches,.....	2 “
“ “ side tracks,.....	26 “
Weight of rail per yard,.....	56 to 60 lbs.

CHARACTER AND LENGTH OF BRIDGING.

	No. of structures	No. of spans.	Length of bridging in feet.
Trestle bridging,	8		7990
Truss bridging, 50 feet span and under,....	45	55	1595
Truss do. from 50 to 100 feet span,.....	6	6	357
Truss do. from 100 to 150 feet span,.....	19	20	3681
Truss do. 150 feet span and over,.....	13	27	4240
Draw bridges,	2		329
Totals,	93	113	18,202

Number of road crossings at grade,.....	93
Number of road crossings above and below grade,.....	34
Number of cross ties per mile,.....	2060
Chairs, number per mile,.....	440 to 700
Whole number of switches on main track, ..	99

GRADIENTS AND ALIGNMENT.

Level number of miles,.....	unknown
Maximum grade, 45 ft. to a mile on main line,	
Amount of straight line, miles, Vt. Central,	83
Amount of curved line, miles, do.,....	34
Maximum radius,.....	11,460 feet
Minimum radius,.....	1,146
Sum of ascents going in one direction,....	unknown
Sum of ascents going in opposite direction,	“
Height of termini & summit above tide water	“

BUILDINGS AND FIXTURES.

Passenger houses,.....	34
Freight houses,.....	23
Engine houses,.....	5
Repair shops,.....	3
Water stations,.....	28
Dwellings,.....	16
Wood sheds,.....	48
Turn tables,.....	4
Other buildings, as follows:	
Car houses,.....	3
Ice houses,.....	5

EQUIPMENT.

Number of locomotives owned by the Company on the 31st day of May, 1865.

	Under 16 tons	16 to 20.	20 to 25.	25 to 30.	30 tons and over.
In good repair,.....			2	29	
Requiring slight repair,.....			2	4	
Requiring heavy repairs,....		1		5	
Worn out,.....					

Number of cars owned by the Company, May 31, 1865:

First class 8 wheel passenger cars in good repair,.....	32
First class 8 wheel passenger cars wanting repair,.....	8
Second class 8 wheel passenger cars in good repair,.....	0
Second class 8 wheel passenger cars wanting repair,.....	0
Baggage, express & mail cars in good repair,.....	6
Baggage, express & mail cars wanting repair.....	3
Covered freight and cattle 8 wheel cars in good repair,.....	790

Covered freight and cattle 8 wheel cars wanting repair,.....	38
Platform 8 wheel cars in good repair,.....	175
Platform cars wanting repair,.....	25
Gravel cars,.....	40
Average weight of passenger cars,.....	12 tons
“ “ baggage “	10 “
“ “ box “	7 1-2 “
“ “ platform “	6 1-2 “
Derrick cars,.....	5 “
Iron plows,.....	4 “
Paymaster's car,.....	1 “

TABLE E.

BUSINESS OF THE YEAR.

Miles run by passenger trains,.....	362,418
Miles run by freight trains,.....	647,647
Miles run by gravel and construction trains,.....	4,188
Miles run by wood trains,.....	18,051
Number of through passengers carried in cars,.....	129,556 1-2
Number of way passengers,.....	202,732 1-2
Number of passengers having passes,.....	—
Number of miles traveled by way passengers.....	4,961,608
Average distance traveled by way passengers,.....	24 1-2 miles
Number of miles traveled by passengers having passes,.....	—
Number of tons of through freight.....	142,715 ¹⁴⁹⁵ ₂₀₀₀
Number of tons of way freight,.....	175,706 ²⁵⁰ ₂₀₀₀
Number of tons of way freight carried 1 mile,.....	15,325,207
Number of tons through freight moved towards market,	110,214 ⁸⁵³ ₂₉₀₀
Number of tons through freight moved from market,	32,501 ⁶⁴² ₂₀₀₀
Number of tons way freight moved towards market,	147,964 ¹⁷⁹⁹ ₂₀₀₀

Number of tons of way freight moved from market,.....	27,741 ⁴⁵ / ₂₀₀
Average rate of speed of ordinary passenger trains,.....	.22 miles per hour
Average rate of speed of express trains, 27 1-2	" "
Average rate of speed of freight trains, 12	" "
Rate of fare charged 1st class through passengers per mile,.....	3 1-4 cents
Rate of fare charged 1st class way passengers, per mile,.....	4 cents
Average rate of fare charged 2d class passengers, per mile,.....	2 cents
Rate per ton per mile charged on 1st class through freight,.....	} rates vary from one and a half to ten cents per mile, according to season & distance.
Rate per ton per mile charged on 2d class through freight,.....	
Rate per ton per mile charged on 3d class through freight,.....	
Rate per ton per mile charged on 1st class way freight,.....	
Rate per ton per mile charged on 2d class way freight,.....	
Rate per ton per mile charged on 3d class way freight,.....	
Rate per ton per mile charged on 4th class way freight,.....	
Total number of tons of freight carried 1 mile,.....	33,094,173
Total number of passengers carried 1 mile,	13,140,362

TABLE F.

EXPENSES OF MAINTAINING ROADWAY AND REAL ESTATE,

For the year ending May 31st, 1865.

Ordinary repairs of road bed and superstructure,.....	\$106,120 45
Cost of new rails used in repairs,.....	172,680 12
Cost of repairs of rails,.....	30,320 65
Cost of cross ties used for renewals,.....	9,544 00
Cost of relaying rails and ties,.....	15,879 20

VERMONT CENTRAL RAILROAD.

31

Insurance and taxes on real estate,.....	7,616 85
Repairs of bridges,....	29,462 32
“ stations and tenements,.....	33,267 47
“ fences,.....	5,190 90
Total,.....	410,081 96

COST OF REPAIRS OF MACHINERY.

Repairs of engines and tenders,.....	\$ 51,734 72
Repairs of passenger and baggage cars,...	46,193 01
Repairs of freight cars,.....	125,057 82
Repairs of tools and machinery in shops,..	9,172 47
Waste and oil used about workshops,.....	5,930 42
Fuel used about shops and stations,.....	17,629 00
Other items in detail as follows :	
Repairs of gravel cars,.....	785 09
Repairs of section and hand cars,.....	1,475 59
Repairs of road and track tools,.....	2,416 47
Repairs of snow plows,	216 01
Total,.....	260,610 60

TABLE G.

COST OF OPERATING THE ROAD.

For the year ending May 31, 1865.

Wood, including cost of preparing the same, \$	175 645 50
Number of cords of wood used by locomotives,.....	44,815
Number of cords of wood used at stations and shops,.....	1,200
Cost of oil and waste for engines & tenders,	25,197 30
Cost of oil and waste for passenger and baggage cars,.....	3,321 22
Cost of oil and waste for freight cars.....	15,355 47
Loss and damage of goods,.....	2,672 42
Loss and damage of baggage,.....	689 33
Damages for injuries to persons,.....	732 40
“ to property, including fire and animals killed on road,.....	790 02

Agents at stations, &c.,.....	25,360 51
Clerks in general offices, &c.,.....	8,566 92
Labor, loading and unloading freight,.....	26,531 41
Porters and watchmen,.....	8,888 84
Switchmen,.....	8,907 46
Conductors, baggage men and breakmen...	45,925 80
Enginemen and firemen,.....	46,193 89
For salaries of Trustees, President, Directors, Secretaries, Treasurer, and Superintendent,.....	14,317 78
For printing, stationery and office expenses	4,584 13
For law expenses,.....	3,909 00
Other expenses in detail as follows:	
For depot furniture,.....	3,107 83
For Advertising,.....	708 65
For Government taxes.....	29,928,55
For General expenses not included above,..	21,428 75
Total,.....	472,763 18

RECAPITULATION OF EXPENSES.

Maintaining roadway,.....\$	410,081 96
Repairs of machinery,.....	260,610 60
Operating,.....	472,763 18
Proportion of expenses due to passenger business,.....	
Proportion of expenses due to freight business,.....	
Total,.....	1,143,455 74
There has also been paid for new equipment as follows:	
153 miles telegraph line, with instruments, \$	10,075 96
9 first class locomotives,.....	180,000 00
6 passenger cars,.....	21,000 00
20 double decked stock cars,.....	20,000 00
54 box freight cars,.....	48,600 00
Total,.....	1,423,131 70

TABLE H.

EARNINGS, RECEIPTS AND PAYMENTS.

Earnings and Receipts.

From passengers,.....	\$ 502,611 31
From freight,.....	943,141 03
From other sources, viz.:	
Expresses,.....	6,800 07
Mails,.....	24,984 96
Rents,.....	244 89
Total.....	<hr/> 1,477,782 26

VALUE OF MATERIALS ON HAND.

Wood, cords of, 22,943,.....	\$73,768 31
Coal, tons of, 138,.....	1,416 00
Oil, gallons of, 1,383,.....	2,158 98
Waste, pounds of, 9,535,.....	2.860 50
Iron rails, tons of, old,.....	
Iron rails, tons of, new, 750,.....	60,000 00
Chairs, pounds of, 35,000,.....	3,150 00
Spikes, pounds of, 4,000,.....	280 00
Iron and other metals unwrought, 223,765 lbs.	15,535 61
Iron and other metals worked and partly worked,	4,604 69
Lumber, 162,553 feet,.....	9,743 53

DETAILS OF EARNINGS FOR THE YEAR ENDING MAY 31, 1865.

SOURCE.	JUNE.	JULY.	AUGUST.	SEPT.	OCTOBER.	NOVEMBER.
Through passengers	19,981 84	25,641 40	36,726 10	34,033 96	27,750 64	20,707 72
Way passengers	13,355 60	17,599 66	22,936 26	23,544 95	16,169 94	14,694 11
Through freight	71,296 46	73,868 98	82,252 99	80,077 65	78,072 09	75,951 15
Way freight	7,402 31	7,594 92	6,823 12	7,957 79	7,487 30	7,523 10
Express	441 67	441 67	441 67	608 34	608 34	608 34
Transport of mails	2,082 08	2,082 08	2,082 08	2,082 08	2,082 08	2,082 08
Use of engines						
Use of cars						
Rent						170 50
Totals	114,559 96	127,228 71	151,262 22	148,304 77	121,737 00	121,737 00

DETAILS OF EARNINGS (CONTINUED.)

SOURCE.	DECEMBER.	JANUARY.	FEBRUARY.	MARCH.	APRIL.	MAY.
Through passengers.....	17,290 91	16,417 36	17,888 17	18,135 89	19,060 65	21,380 67
Way passengers.	12,164 28	11,632 61	10,766 83	14,368 69	13,004 73	57,358 34
Through freight.....	62,199 30	48,683 88	48,932 20	66,147 50	71,634 68	90,720 07
Way freight.....	5,748 30	5,404 61	6,537 38	9,143 68	8,353 38	13,328 19
Express....	608 34	608 34	608 34	608 34	608 34	608 34
Transport of mails.....	2,082 08	2,082 08	2,082 08	2,082 08	2,082 08	2,082 08
Use of engines.....						
Use of cars.....						
Rent.....						74 89
Totals	100,093 21	84,828 88	86,815 00	110,486 18	114,743 86	185,552 08

TABLE I.

ACCIDENTS.

State the number of persons injured in life or limb, and the cause thereof, and whether passengers or persons employed.

State whether any such accidents have arisen from carelessness or negligence of any person in the employ of the corporation, and whether such person is retained in the service of the corporation.

	EMPLOYEES.		OTHERS.	
	killed.	injured.	killed.	injured.
Trains thrown from track.....				
Struck by bridge while on top of freight car.....				
Run over while walking on track	1		1	
Injured at road crossing.....				
Total.....				

Total number of persons killed,..... 4

Total number of persons injured but not
killed,.....

In addition to which must be given a statement of the date of each accident, the place where it occurred, the train, the cause and the extent of the injuries inflicted upon each person, and the name of such person.

July 4, 1864. John A. Luce, a brakeman, on a freight train, was killed while shackling cars at Randolph.

Aug. 13, 1864. James Harrington, an engineer, jumped from his engine in coming into the station at St. Albans, and falling across the track, was killed.

Sept. 10, 1864. A passenger car was thrown from the track near Charcoal Creek in Swanton, and two passengers slightly injured. Cause of accident unknown.

Sept. 21, 1864. Wm. Hough, a trackman, was killed near South Royalton, by being run over by the rear part of a freight train which had broken apart, he not noticing that the train had not all passed him.

Oct. 11, 1864. Two cars of the accommodation train were thrown from the track near Colchester, and a number of passengers slightly injured. Cause of accident not known.

Oct. 18, 1864. F. Seeley was found dead in the morning one mile south of Milton, apparently having been killed in the night by some passing train.

TABLE J.

NUMBER OF EMPLOYEES AND COMPENSATION.

11	Conductors of passenger trains, \$50 to 75 per month.	
38	Conductors of freight trains,	45 to 55 “
4	Conductors of wood and gravel trains,	52 to 60 “
1	Master Mechanic,	2,500 per year.
2	Road masters, per year for both	1,800
311	Men in repair shops,	1 25 to 4 per day.
11	Engine men of passenger trains,	3 00 “
30	Engine men of freight trains, . . .	2 00 to 3 00 “
4	Engine men of wood and gravel trains,	2 00 to 3 00 “
45	Fire men,	1 50 to 2 00 “
10	Baggage men,	45 to 50 per month.
24	Switch men,	1 50 per day.
43	Section men, (foreman,)	39 to 50 per month.
318	Section hands,	1 50 to 1 60 per day.
27	Watch men,	10 per month to 1 75 per day.
33	Station agents,	150 to 1100 per year.
278	Other laborers,	1 50 to 4 per day.
4	Clerks connected with passenger business,	183 33 per month.
6	Clerks connected with freight business,	491 66 “

Superintendent of bridges, salary,...	1,200	per year.
Wood agent,.....	1100	"

Trustees First Mortgage Bonds and Managers.

LAWRENCE BRAINERD,

JOSEPH CLARK,

J. GREGORY SMITH.

G MERRILL, *Supt. for Trustees and Managers.*

G. M. DEXTER, *President.*

W. C. SMITH, *Treasurer.*

EDWARD W. PECK, *Clerk.*

STATE OF VERMONT, } St. Albans, August 31st, 1865.
FRANKLIN COUNTY, SS. }

We depose and say that the facts set forth, and statements made in the foregoing report, which has been signed by us, are true and correct, according to the best of our knowledge, information and belief.

Signed,

LAWRENCE BRAINERD,
JOSEPH CLARK,
J. GREGORY SMITH.

Subscribed and sworn to before me this 31st day of August, 1865.

II. H. LOCKLIN, *Justice of the Peace.*

ANNUAL REPORT
OF THE VERMONT & CANADA RAILROAD COM-
PANY, FOR THE YEAR ENDING
JUNE 30, 1865.

TABLE A.

STOCK AND DEBTS.

The capital of this company, by compromise and decree of Court of Chancery, is fixed at \$2,000,000, exclusive of the cost of constructing the road from Swanton to Canada line in Highgate, for which extension stock is to be issued, not exceeding \$250,000. This extension has been completed, at a cost exceeding that sum, and stock therefor, to the amount of \$140,000, has been issued, leaving the sum of \$110,000 to be hereafter issued. The \$2,000,000 covers the original cost of construction, and building the road into Burlington through the sand bank, and all back rents due from the Vermont Central Company, as per report of last year. This company has no debt of any magnitude.

TABLE B.

COST OF CONSTRUCTION.

The particulars called for under this head cannot be more fully stated than what has already been given in Table A, as the constructing agents have not furnished

this company with a detailed statement of their expenditures. This statement, however, may be found, perhaps, in the report of the managers and receivers of the road of this company, and of the Vermont Central Company.

TABLE C.

EQUIPMENT.

This company have no equipment, as the road has been leased to the Vermont Central Company, and is run by the receivers under the lease. The receivers were appointed by the Court of Chancery.

TABLE D.

CHARACTERISTICS OF ROAD.

Reference is made to the report of the Managers and receivers of the Central road, for the information called for by this table, and all the subsequent tables, except as to J in part.

TABLE J.

The road was leased many years since to the Vermont Central Company, at the rent of eight per cent. on its cost of construction. The back rents have all been adjusted and settled, as previously stated, and the accruing rents are regularly paid by the receivers.

STATE OF VERMONT, } 30th day of August, 1865.
FRANKLIN COUNTY, SS. }

I, Worthington C. Smith, Treasurer of the Vermont and Canada Railroad Company, do solemnly swear that the above is a true statement of the condition of the finances of said Company, their trustees, or assignees or lessees, on the 30th day of June, 1865.

WORTHINGTON C. SMITH, *Treasurer.*

Sworn before me,

JOSEPH W. TAYLOR, *Justice of the Peace.*

OFFICERS OF THE COMPANY.

*Directors.*LUCIUS B. PECK, *President.*JOHN PORTER, *Vice President.*W. C. SMITH, *Treasurer and Clerk.*EDWARD BLAKE, *Transfer Agent.*

JEDD P. CLARK,

BENJ. P. CHENEY.

E. Mott Robinson, one of the Directors, having recently died, ——— Atherton has been elected Director in his place by the Board of Directors.

SALARIES.

President,.....	\$2,000
Transfer Agent,....	1,000
Treasurer and Clerk,.....	1,000

LUCIUS B. PECK, *President.*

I wish to state that the Directors voted me a salary of \$2,500 for the last year, but I understood it to be \$2,000 until some time subsequent to the vote, and have entered it here at \$2,000, that being all I intend to take.

L. B. PECK.

STATE OF VERMONT, } Montpelier, Aug. 29, 1865.
WASHINGTON COUNTY, SS. }

I, Lucius B. Peck, depose and say that the facts set forth, and statements made in the foregoing report, which has been signed by me, are true and correct, according to the best of my knowledge, information and belief.

Signed,

LUCIUS B. PECK.

Subscribed and sworn to before me this 29th day of August, A. D. 1865.

R. F. FIFIELD, *Master in Chancery.*

ANNUAL REPORT

OF THE ATLANTIC & ST. LAWRENCE RAILROAD
COMPANY, FOR THE YEAR ENDING
JUNE 30, 1865.

TABLE A.

Capital stock authorized by charter,	\$4,000,000
The amount paid in,	2,494,900
Funded debt,	3,472 000
Floating debt—nothing.	
Interest at rate of six per cent.	
The coupons are paid by the Lessees.	

TABLE B.

COST OF CONSTRUCTION.

Total cost of construction as per last report, \$6,788,354 72
Two thirds of the road were built at a stipulated price
per mile, consequently cannot be apportioned.

TABLE C.

EQUIPMENT.

Total cost of equipment as per last report, \$ 865,734 76
Total cost of road and equipment, 7,654,089 48

TABLE D.

CHARACTERISTICS OF ROAD

Length of road within State of Vermont,...	30 1-2 miles
" " completed, " " " "	20 1-2 "
" " branches, " " " "	none
" " side tracks, " " " "	about 1 1-5 miles
Weight of rail per yard,.....	63 lbs.

CHARACTER AND LENGTH OF BRIDGING.

	No. of structures	No. of spans.	Length of bridging in feet.
Pile bridging, with one draw,.....	1		1519
Truss bridging, 50 feet span and under, ...	10	10	400
Truss do. from 50 to 100 feet span,.....	2	2	120
Truss do. from 100 to 150 feet span,.....	14	14	1750
Truss do. 150 feet span and over,.....			
Stone arch bridges,	2	2	120
Totals,	29	28	3909

Number of road crossings at grade,..... 63

Number of road crossings above and below
grade,..... 6

Number of cross ties per mile,..... 2400

Chairs, number per mile,..... about 600

Whole number of switches on main track, about 100.

NOTE—In addition to the above bridges, there are on the line 16 iron bridges, 3 of which are 300 feet each in length, in spans of 75 feet; one over the Connecticut River, one over Wild River, and the other over Presumpscot River; whole length of above named bridges, 1,510 feet.

GRADIENTS AND ALIGNMENT.

Level number of miles, and grades to 20 feet 97.27

Maximum grade,..... 60 feet

Amount of straight line, miles,..... 89 1-2

Amount of curved line, miles,.....	59 1-2
Maximum radius,.....	5,730 feet
Minimum radius,.....	955 "
Sum of ascents going in one direction,....	1,929 "
Sum of ascents going in opposite direction,	757 "
Height of termini and summit above tide water,.....	1,178 "

The charter of the Atlantic and St. Lawrence Railroad within the State of Vermont extends from the Connecticut River in Bloomfield to the boundary line in Norton. But the entire line from Portland to Montreal is worked in divisions, of which the point of junction is at Island Pond, in Brighton.

BUILDINGS AND FIXTURES.

Passenger houses,.....	29
Freight Houses,.....	22
Engine houses,.....	8
Repair shops,.....	6
Water stations,.....	17
Dwellings,.....	2
Wood sheds,.....	26
Turn tables,.....	10

Other buildings, as follows:

Two hotels,

Ten store houses on wharves in Portland for Ocean and Boston steamers.

EQUIPMENT.

Number of locomotives owned by the Company on the 30th day of June, 1865.

	Under 16 tons	16 to 20.	20 to 25.	25 to 30.	30 tons and over.
In good repair,.....			3	53	1
Requiring slight repair.....			1	5	
Requiring heavy repairs,.....			1	1	
Worn out,.....					

Number of cars owned by the company June 30, 1865,	565
First class 8 wheel passenger cars in good repair,	17
Baggage, express and mail cars in good repair,	7
Covered freight and cattle 8 wheel cars } in good repair, }	541
Covered freight and cattle 8 wheel cars } wanting repair,	
Platform 8 wheel cars in good repair, ... }	
Other freight cars,	
Gravel cars,	

TABLE E.

BUSINESS OF THE YEAR, ENDING DEC. 31, 1864.

Miles run by passenger trains,	147,692
Miles run by freight trains,	498,828
Miles run by gravel and construction trains } Miles run by wood trains,	213,315
Average rate of speed of ordinary passenger trains, per hour,	
Average rate of speed of freight trains,	11 "
Rate of fare charged 1st class through passengers per mile, about	2 3-4 cents
Rate of fare charged 1st class way passengers per mile, about	3 "

TABLE F.

EXPENSES OF MAINTAINING ROADWAY AND REAL ESTATE,

For the year ending June 30, 1865.

Books so kept cannot be answered in detail.

COST OF REPAIRS OF MACHINERY.

Books so kept cannot be answered in detail.

TABLE G.

COST OF OPERATING THE ROAD.

Books not classified to conform to above table. Answers cannot be given.

RECAPITULATION OF EXPENSES.

Total, year ending Dec. 31, 1864,..... \$703,397 48

TABLE H.

EARNINGS, RECEIPTS AND PAYMENTS.

Earnings and Receipts.

From passengers, year ending Dec. 31, 1864, \$231,347 15

From freight,..... 720,737 12

From other sources, viz.:

Expresses,.....	}	26,004 89
Mails,.....		
Rents,.....	}	30,225 35
Miscellaneous,.....		

Payments other than for Construction.

For dividends, 1st January and 1st July at rate of 4 per cent., less Government tax.

The statement of receipts and expenses applying to the Portland Division, extending from Island Pond to Portland, cannot be answered in detail.

TABLE J.

NUMBER OF EMPLOYEES AND COMPENSATION.

The employees of the Company being employed upon the two divisions of the road, one South of Island Pond, extending to Portland, the other North of Island Pond, extending to the boundary line and Montreal, no specific statement can be made under the head of enquiry, as applied to the State of Vermont.

The earnings of the road are received by the lessees. The lessors have no interest in them while the rent is paid, and the accounts are so kept by the Grand Trunk Railway Company as not to admit of an answer to the above as contemplated.

The Directors of the Atlantic and St. Lawrence Railroad Company, on presenting this their annual report, beg leave to offer a copy of their statement made to you the last year, which is as follows :

“ The Directors of the Atlantic and St. Lawrence Railroad Company, in making their answers to the foregoing questions, beg to state, that their road being under lease to the Grand Trunk Railway Company of Canada, as set forth in a former report, and being under the exclusive management of that company, their information upon which these statements are based, is derived partly from their own records and files, but principally from the officers and agents of that company at Portland, and from their books and accounts there kept; and though they are not personally cognizant of many of the facts, and state them from information thus derived, and not as wholly within their own knowledge, they have no doubt of the correctness of the statements made. As the books of the company have been from the first, and are now, kept in a method to conform to the requirements of another jurisdiction, very many of the questions in the tables cannot be answered; and such as are answered must apply to the whole line of road from Portland to the line of Vermont. Answers are, however, given as fully as the information in their possession will admit of.

“ The same course substantially has been adopted by the Directors in their reports to the Vermont authorities during the time their road has been under lease, and they trust that in this case it will be satisfactory. They have no doubt, if more explicit answers are required upon any matter, it will be readily furnished by the lessees of the road, so far as it is in their power so to do, intimation to that effect being given to them, or to the authorized agents or officers on the line.”

STATE OF MAINE, } Portland, August, 1865.
CUMBERLAND COUNTY, SS. }

The subscribers, a majority of the Directors of the Atlantic and St. Lawrence Railroad Company, depose and say that the facts set forth, and statements made in the foregoing report, which has been signed by us, are true and correct, according to the best of our knowledge, information and belief.

Signed,

S. J. SMITH,
CHAS. E. BARRETT,
J. L. FARMER,
P. BARNES,
G. F. SHEPLEY,
H. J. LIBBY,
BYRON GREENOUGH,

} *Directors.*

Subscribed and sworn to before me this 25th day of August, 1865.

H. ILSLEY, *Justice of Peace.*

ANNUAL REPORT

OF THE VERMONT & MASSACHUSETTS RAILROAD
COMPANY, FOR THE YEAR ENDING
JUNE 30, 1865.

TABLE A.

STOCK AND DEBTS.

Capital stock, including the Greenfield	
Branch and Vermont portion,.....	\$3,200,000
Total amount of capital stock paid in,.....	2,860,000
Number of shares of capital stock issued,..	28,600
There is no debt excepting the funded debt,	
which amounts to.....	\$924,905
which amount includes \$141,700, held	
by the trustees as a sinking fund, and	
\$80,000 of bonds in the Treasury.	

The amount of interest paid each year is six per cent. on the balance of the above funded debt outstanding, less by the amount of interest received by the company upon loans.

Interest coupons have always been promptly paid when due.

TABLE B.

COST OF CONSTRUCTION.

Total cost of construction as per last report, \$3,287,960 41
Nothing expended on this account since last report.

TABLE C.

EQUIPMENT.

Total cost of equipment as per last report,	\$ 225,650 20
Total cost of road and equipment,.....	3,512,854 51

TABLE D.

CHARACTERISTICS OF ROAD.

Length of road,.....	69 miles
“ “ completed,.....	69 “
“ “ branches,.....	8 “
“ “ side tracks,.....	5 1-2 “

Weight of rail per yard,..... 56 lbs.

NOTE—State length of road, branches and side tracks within the State of Vermont.

Road,..... 10 mis. 1661 ft.

Branches,.....

Side tracks,..... 1 mile

	No. of structures	No. of spans	Length of bridging in feet.
Trestle bridging,.....			
Truss bridging, 50 feet span and under, ...	11	11	
Truss do. from 50 to 100 feet span,.....	5	5	
Truss do. from 100 to 150 feet span,.....	3	4	
Truss do. 150 feet span and over,.....	23	41	
Draw bridges,			
Totals,			

Number of road crossings at grade,..... 62

Number of road crossings above and below
grade,..... 21

Number of cross ties per mile,..... 2350

Chairs, number per mile,..... 530

Whole number of switches on main track,..... 60

GRADIENTS AND ALIGNMENT.

Maximum grade,..... 58 feet

Amount of straight line, miles,.....	31 ¹³ ₂₀
Amount of curved line, miles,.....	45 ⁷ ₁₀

BUILDINGS AND FIXTURES.

Passenger houses,.....	20
Freight houses,.....	20
Engine houses,....	5
Repair shops,.....	4
Water stations,.....	9
Dwellings,.....	9
Wood sheds,.....	21
Turn tables,.....	5

Other buildings, as follows:

1 general office at Fitchburg.....	
------------------------------------	--

EQUIPMENT.

Number of locomotives owned by the Company on the 30th day of June, 1865, eleven.

	Under 15 tons	15 to 20.	20 to 25.	25 to 30.	30 tons and over.
In good repair,.....			9		
Requiring slight repair.....			2		
Requiring heavy repairs.....					
Worn out,.....					

Number of cars owned by the company, June 30, 1865.

First class 8 wheel passenger cars in good repair,....	7
First class 8 wheel passenger cars wanting repair,.....	1
Second class 8 wheel passenger cars in good repair,.....	0
Second class 8 wheel passenger cars want- ing repair.....	0

Baggage, express and mail cars in good repair,.....	5.
Baggage, express and mail cars wanting repair,.....	0
Covered freight and cattle 8 wheel cars in good repair,.....	84
Covered freight and cattle 8 wheel cars wanting repair,.....	2
Platform 8 wheel cars in good repair,.....	84
Other freight cars,.....	10
Gravel cars,.....	0
Average weight of passenger cars,.....	12 1-2 tons
“ “ baggage, “	11 1-2 “
“ “ box, “	8 “
“ “ platform, “	6 1-2 “

TABLE E.

BUSINESS OF THE YEAR, ENDING NOV. 30, 1864.

Miles run by passenger trains,.....	88,379
Miles run by freight trains,.....	62,686
Miles run by gravel and construction trains, }	262
Miles run by wood trains,..... }	
Number of through passengers carried in cars,.....	103,360
Number of way passengers,.....	58,842
Number of passengers having passes,.....	1,074
Number of miles traveled by way passengers,.....	841,986
Average distance traveled by way passengers, miles,.....	14
Number of miles traveled by passengers having passes,.....	17,719
Number of tons of through freight,.....	82,881 ²⁸⁵ ₁₀₀₀
Number of tons of way freight,.....	31,253 ⁵¹² ₁₀₀₀
Number of tons of way freight carried 1 mile,	629,949 ¹⁶³ ₁₀₀₀

Number of tons of through freight moved towards market,.....	60,129 ⁵⁵¹ ₁₀₀₀
Number of tons of through freight moved from market,.....	22,751 ⁷³⁴ ₁₀₀₀
Number of tons of way freight moved to- wards market,.....	31,253 ⁸¹² ₁₀₀₀
Number of tons of way freight moved from market,.....	
Average rate of speed of ordinary passenger trains, per hour,.....	22 miles
Average rate of speed of freight trains,....	10
Rate of fare charged 1st class through pas- sengers per mile,.....	4 cts. nearly
Rate of fare charged 1st class way passen- gers per mile,.....	4 cts.
Rate per ton per mile charged on 1st class through freight,.....	7 cents
Rate per ton per mile charged on 2d class through freight,.....	6 cents
Rate per ton per mile charged on 3d class through freight,.....	5 cents
Rate per ton per mile charged on 4th class through freight,.....	4 cents
Rate per ton per mile charged on 1st class way freight,.....	15 cents
Rate per ton per mile charged on 2d class way freight,.....	13 cents
Rate per ton per mile charged on 3d class way freight,.....	11 cents
Rate per ton per mile charged on 4th class way freight,.....	8 cents
Total number of tons of freight carried 1 mile,.....	2,207,284 ¹⁶² ₁₀₀₀
Total number of passengers carried 1 mile,	3,004,468

TABLE F.

EXPENSES OF MAINTAINING ROADWAY AND REAL ESTATE.

For the year ending June 30, 1865.

Ordinary repairs of road bed and super-structure.....	\$32,647 89	
Cost of new rails used in repairs.....	38,512 02	
Number and weight of chairs.....		} included in above.
Weight of spikes.....		
Cost of repairs of rails.....		
Number of cross ties used for renewals,..		
Cost of same.....		
Cost of relaying rails and ties.....		
Insurance and taxes on real estate.....	2,973 75	
Repairs of bridges.....	4,917 50	
“ stations.....	8,080 08	
“ fences.....	1,047 18	
Total.....	88,178 42	

COST OF REPAIRS OF MACHINERY.

Repairs of engines and tenders,.....	\$31,587 82	
Repairs of passenger and baggage cars,...	4,611 75	
Repairs of freight cars,.....	20,675 92	
Repairs of tools and machinery in shops,..	894 87	
Oil used about workshops,.....	} stated elsewhere.	
Fuel,.....		
Waste,.....		
Other items in detail as follows :		
Repairs of gravel and hand cars,.....	480 33	
Total,	<hr/> 58,250 69	

TABLE G.

COST OF OPERATING THE ROAD.

For the year ending June 30, 1865.

Wood, including cost of preparing the same, \$ 22,500 76

Stock materials,.....	153 12
Cost of oil and waste for engines & tenders, }	
Cost of oil and waste for passenger and baggage cars,.....	5,171 33
Cost of oil and waste for freight cars,..... }	
Loss and damage of goods,..... }	
Loss and damage of baggage,..... }	
Damages for injuries to persons,..... }	3,942 34
Damages to property, including fire and animals killed on road,..... }	
Agents at stations, &c.,..... }	
Clerks in general offices, &c.,..... }	included in expenses charged freight & passenger departments.
Labor, loading and unloading freight,..... }	
Porters and watchmen,..... }	4,422 34
Switchmen,..... }	
Wood and water station expense,.....	2,985 32
Conductors and baggagemen,..... }	
Brakemen,..... }	included in expenses charged freight & passenger departments.
Enginemen and firemen,..... }	
For salaries of Trustees, President, Directors, Secretaries, Treasurer, and Superintendent,.....	7,645 82
For printing, stationery and advertising,..	784 39
For law expenses,.....	2,461 00
Other expenses in detail as follows :	
Clearing snow,.....	941 70
Expenses of passenger department,.....	20,232 24
Expenses of freight department,.....	24,718 27
Rent to Connecticut River Railroad Co.,...	1,500 00
Incidental expenses,.....	2,511 35
United States and State Taxes,.....	19,796 62
Total,.....	119,766 60

RECAPITULATION OF EXPENSES.

Maintaining roadway,.....	\$ 88,178 42
Repairs of machinery,.....	58,250 69

Operating and taxes,.....	119,766 60
Total,.....	266,195 71

TABLE II.

EARNINGS, RECEIPTS AND PAYMENTS.

Earnings and Receipts.

From passengers,.....	\$132,665 90
From freight,.....	184,519 76
From other sources, viz.:	
Tolls,	940 25
Expresses,	4,441 14
Mails,.....	6,000 00
Rents,.....	43,372 96
Miscellaneous,.....	332 83
Total,.....	372,272 84

Payments other than for Construction.

For transportation expenses, viz.:

For passenger business,.....	\$266,195 71	
For freight business,.....		
For other business, and what,.....		
For interest on funded debt,.....	} elsewhere expressed.	
For interest on floating debt,.....		
For dividends,.....		
For carried to surplus fund,.....		
For amount of surplus fund,.....		

VALUE OF MATERIALS ON HAND.

Wood, 8,072 cords, \$4 69 per cord,.....	\$37,857 68
Oil, gallons of,.....	} 533 56
Waste, pounds of,.....	
Iron rails, old, 300 tons, \$40 per ton,.....	12,000 00
Iron and other metals unwrought,.....	} 22,090 15
Iron and other metals worked and partly worked,	
Lumber,	

DETAILS OF EARNINGS FOR THE YEAR ENDING JUNE 30, 1865.

SOURCE.	JULY.	AUGUST.	SEPT.	OCTOBER.	NOVEMBER.	DECEMBER.
Through passengers ..	12,328 24	16,540 00	14,714 72	11,526 04	11,044 42	8,230 28
Way passengers						
Through freight	11,109 05	13,237 49	14,619 75	14,282 33	16,741 87	15,663 63
Way freight						
Express	362 74	362 74	362 74	362 74	362 74	362 74
Transport of mails	500 00	500 00	500 00	500 00	500 00	500 00
Tolls	89 25	62 50	89 75	97 00	77 25	62 75
Miscellaneous	8 10	40 85	25 50	60 25	23 53	18 40
Rent	3,591 83	3,270 50	3,911 67	3,688 75	3,559 50	3,780 00
Totals	27,989 21	34,014 08	34,224 13	30,517 11	32,309 31	28,617 80

DETAILS OF EARNINGS (CONTINUED.)

SOURCE.	JANUARY.	FEBRUARY.	MARCH.	APRIL.	MAY.	JUNE.
Through passengers.. }	10,730 36	5,411 11	10,640 16	9,635 38	9,722 86	10,012 33
Way passengers. }						
Through freight..... }	15,606 67	16,259 48	17,501 88	16,608 21	17,864 88	15,024 52
Way freight..... }						
Express..... }	362 74	392 16	377 45	377 45	377 45	377 45
Transport of mails	500 00	500 00	500 00	500 00	500 00	500 00
Tolls..... }	103 75	66 00	61 75	86 25	89 25	54 75
Miscellaneous..... }	45 35	12 13	27 57	39 52	16 23	15 40
Rent..... }	3,826 50	3,277 50	3,806 25	3,623 75	3,280 00	3,756 71
Totals	31,175 37	28,048 38	32,915 06	30,870 56	31,850 67	29,741 16

TABLE 1.

ACCIDENTS.

Total number of persons killed,	3
Total number of persons injured but not killed,	0

George Johnson, of Athol 15 years of age, was run over and killed, while lying on track, by evening passenger train, near Templeton station, December 6, 1864, between 8 and nine o'clock. Coroner's jury returned verdict, that "no blame is attached to the Railway Company."

February 28, 1865. Edwin H. Tottingham, of Waltham, was run over and killed, by passenger train, at Grout's station, while attempting to get upon the train after it had left the depot. Coroner's jury rendered verdict, "that no blame is attached to the company."

June 3, 1865. B. F. D. Gibbs, brakeman on freight train, in climbing from door on side of drover's saloon car to top of same, was struck by a telegraph pole standing beside the track, near Ewing station, and he died June 6th. No fault of the company.

TABLE J.

NUMBER OF EMPLOYEES AND COMPENSATION.

3 Conductors of passenger trains,	\$55 & 60	per month
2 Conductors of freight trains,	50 & 60	"
1 Master mechanic,	100	"
51 Men in repair shops,	1.50 to 3.50	per day
4 Enginemen of passenger trains, ...	70	per month
3 Enginemen of freight trains,	55 & 60	"
7 Firemen,	45	"
4 Baggage men,	40 & 60	"
4 Switch men,	25 to 40	"
7 Section men, (foreman,)	1.75 to 2.25	per day
54 Section hands,	1.50	"
5 Watch men,	0.76 to 1.60	"

20 Station agents,..... 15 to 75 per month

1 Clerk connected with passenger
business,..... 60 “

3 Clerks connected with freight bu-
siness,..... 45 to 100 “

Superintendent of bridges, wood agent
and road master, one person,.... 125 “

Other agents—how employed—and the salary of each
as follows, viz.:

1 Toll collector, Northfield bridge,.. 20 “

2 Freight receipters, Charlestown,.. 10 & 50 “

5 Station laborers, loading and un-
loading freight,..... 40 & 45 “

1 Master carpenter,..... 70 “

4 Bridge carpenters,..... 1.75 to 2.75 per day

1 Porter,..... 1.55 “

2 Flagmen, 30 & 37.50 per month

3 Passenger brakemen,..... 50 “

5 Freight brakemen,..... 45 “

Amount of net earnings June 30, 1864, for
one year,..... \$141,027 57

Amount of net earnings for year ending
June 30, 1865,..... 106,077 13

Total amount of payments to the sinking
fund, 90,000 00

Amount of cash in treasury,...\$ 40,950 05

Amount of notes receivable,... 155,500 00

\$80,000 Vt. & Mass. R. R. Co.

bonds,..... 75,924 39

————— 272,374 44

Amount of funded debt, 924,905 00

This sum, \$924,905, of the funded debt includes \$80,000
of bonds in the treasury, and \$141,700 of bonds held by
the trustees, in which they have invested the sinking fund.

Amount of interest paid during 12 months,
 ending June 30, 1865, more than re-
 ceived,..... 36,749 42

No dividend has yet been paid to the stockholders since the opening of the road, all of the surplus earnings having been appropriated to the payment of the debts of the company and interest.

STATE OF MASSACHUSETTS, } 28th day of Aug. 1865
 SUFFOLK COUNTY, SS. }

I, Franklin N. Poor, Treasurer of the Vermont and Massachusetts Railroad Company, do solemnly swear that the above is a true statement of the condition of the finances of said company, their trustees, or assignees or lessees, on the 30th day of June, 1865.

FRANKLIN N. POOR, *Treasurer.*

Sworn before me,

P. E. TESCHEMACHER, *Justice of the Peace.*

OFFICERS OF THE COMPANY.

SALARIES.

3 Trustees, J. C. Howe, Wm.

Minot, Jr., & J. McGregor,...	\$300, 200, 100	per annum
President, Daniel S. Richardson,.....	1500	per annum
Superintendent, Otis T. Ruggles,.....	2500	per annum
Treasurer, Franklin N. Poor,.....	2000	per annum
Clerk, B. D. Locke,.....	50	per annum

DANIEL S. RICHARDSON,	} <i>Directors of the Vermont & Mass. R. R. Company.</i>
WILLIAM H. HILL,	
FRANKLIN N. POOR,	
HENRY SAYLES,	
FRANCIS GOODHUE,	

STATE OF MASSACHUSETTS, }
SUFFOLK COUNTY, SS. }

We depose and say that the facts set forth, and statements made in the foregoing report, which has been signed by us, are true and correct according to the best of our knowledge, information and belief.

Signed,

DANIEL S. RICHARDSON,
WILLIAM H. HILL,
FRANKLIN N. POOR,
HENRY SAYLES,
FRANCIS GOODHUE,

} *Directors of the*
Vermont & Mass.
R. R. Co.

Subscribed and sworn to before me this 28th day of August, 1865.

P. E. TESCHEMACHER, *Justice of the Peace.*

ANNUAL REPORT

OF THE VERMONT VALLEY RAILROAD COMPANY,
FOR THE ELEVEN MONTHS ENDING
MAY 31, 1865.

TABLE A.

STOCK AND DEBTS.

1. The amount of capital stock, as by charter, with right to increase,	\$500,000 00
2. The amount of capital stock subscribed,	535,000 00
3. The amount of capital stock paid in as per last report,	516,163 82
4. The amount of capital stock now paid in,	516,163 82
Number of shares, 5358 original stock, par value \$100 per share, cash realized,	516,163 82
Funded debt as per last report,	793 200 00
Total funded and floating debt now,	793,200 00

Classes of Funded Debt.

	No. 1.	No. 2.
Amount,	\$500,000 00	\$293,200 00
Date of issue,	April 1, 1850	October 1, 1854
Date of payment,	April 1, 1866	October 1, 1859
Annual rate of interest,	386,000 7 per cent.	7 per cent.
“ “	114,000 6 per cent.	
Interest, when payable,	April 1, October 1.	October 1, April 1
Cash realized,	\$500,000 00	\$175,920 00
Nature and character of security,	Mortgage	Mortgage

Both classes of bonds are payable in New York, and are not convertible.

Coupons due October 1, 1858, and April 1, 1859, of first mortgage, have been paid since last report.

TABLE B.

COST OF CONSTRUCTION.

Total cost of construction as per last report, \$1,212,274 40

TABLE C.

EQUIPMENT.

Total cost of equipment as per last report, \$9,611 79

Total cost of road and equipment, 1,301,886 19

TABLE D.

CHARACTERISTICS OF ROAD.

Length of road, 23 ⁶⁰/₁₀₀

Weight of rail per yard, 57 lbs.

CHARACTER AND LENGTH OF BRIDGING.

	No. of structures	No. of spans.	Length of bridging in feet.
Trestle bridging,	2	10	100
Truss bridging, 50 feet span and under,	2	1	45
Truss do. from 50 to 100 feet span,	0	0	0
Truss do. from 100 to 150 feet span,	1	1	116
Truss do. 150 feet span and over,	2	6	132
Draw bridges,			
Totals,			

Number of road crossings at grade, 11

Number of road crossings above and below
grade, 3

Number of cross ties per mile, 2,000

Chairs, number per mile, 586

GRADIENTS AND ALIGNMENT.

Level number of miles,.....	7 $\frac{67}{100}$
Maximum grade,.....	32 feet
Amount of straight line, miles,.....	10 $\frac{72}{100}$
Amount of curved line, miles,.....	12 $\frac{97}{100}$
Maximum radius,.....	872 feet
Minimum radius,.....	600 "

BUILDINGS AND FIXTURES.

Passenger houses,.....	4
Freight houses,....	4
Engine houses,.....	1
Repair shops,	1
Water stations,.....	2
Dwellings,.....	0
Wood sheds,.....	3
Turn tables,.....	0

EQUIPMENT.

Number of locomotives owned by the Company on the 30th day of June, 1865.

	Under 15 tons	16 to 20.	20 to 25.	25 to 30.	30 tons and over.
In good repair,.....		3	1		
Requiring slight repair					
Requiring heavy repairs,....					
Worn out,.....					

Average weight of passenger cars,.....	} no means of weighing.
" " baggage "	
" " box "	
" " platform "	

TABLE E.

BUSINESS OF THE YEAR.

Number of through passengers carried in cars,	62,079
Number of way passengers,	7,070
Number of miles traveled by way passengers,	79,853
Average distance traveled by way passengers, miles,	11 1-7
Number of tons of through freight,	41,508 ⁵⁷⁸ / ₂₀₀₀
Number of tons of way freight,	1,949 ⁹⁰¹ / ₂₀₀₀
Number of tons of way freight carried 1 mile,	27,166 ¹⁶⁰³ / ₂₀₀₀
Number of tons through freight moved towards market,	29,074 ⁴⁹⁵ / ₂₀₀₀
Number of tons through freight moved from market,	11,934 ³⁰ / ₂₀₀₀
Number of tons way freight moved towards market,	1,106 ¹⁶⁷ / ₂₀₀₀
Number of tons way freight moved from market,	843 ⁸³⁹ / ₂₀₀₀
Average rate of speed of ordinary passenger trains per hour, including stops,	24 miles
Average rate of speed of express trains per hour, including stops,	24 "
Average rate of speed of freight trains per hour, including stops,	9 "
Rate of fare charged 1st class through passengers per mile,	3 3-4 cents
Rate of fare charged 1st class way passengers per mile,	4 1-6 "
Average rate of fare charged 2d class passengers per mile,	3 "
Rate per ton per mile charged on 1st class through freight,	5 1-2 "

Rate per ton per mile charge 1 on 2d class through freight,.....	4 1-2	"
Rate per ton per mile charged on 3d class through freight,.....	3	"
Rate per ton per mile charged on 4th class through freight,.....	2 1-2	"
Rate per ton per mile charged on 1st class way freight,.....	15	"
Rate per ton per mile charged on 2d class way freight,.....	11 1-2	"
Rate per ton per mile charged on 3d class way freight,.....	10	"
Rate per ton per mile charged on 4th class way freight,.....	8	"
Total number of tons of freight carried 1 mile,.....	1,023,359	²⁸⁵ / ₂₀₀₀
Total number of passengers carried 1 mile,	1,569,749	

TABLE F.

EXPENSES OF MAINTAINING ROADWAY AND REAL ESTATE.

For the year ending June 30, 1865.

Ordinary repairs of road bed and super-structure,.....	\$ 8,589 13
Extraordinary repairs of road bed,.....	719 58
Cost of new rails used in repairs,.....	} 9,148 00
Number and weight of chairs,.....	
Weight of spikes, 4,000,.....	359 20
Cost of repairs of rails,.....	2,050 00
Number of cross ties used for renewals,.....	} 1,844 30
Cost of same, 6,026.....	
Cost of relaying rails and ties,.....	included above
Insurance and taxes on real estate, say.....	500 00
Repairs of bridges,.....	500 00
Repairs of stations,.....	191 72
Repairs of fences,.....	8 00
Repairs of masonry, culvert,.....	45 00

New station house at Westminster, over and above insurance,.....	1,039 00
New engine house at Bellows Falls, over and above insurance,.....	1,669 51
Total,.....	26,163 44

COST OF REPAIRS OF MACHINERY.

Repairs of engines and tenders,.....	2,440 01
Repairs of passenger and baggage cars,...	741 94
Repairs of freight cars,.....	1,445 50
Total,.....	4,627 45

TABLE G.

COST OF OPERATING THE ROAD.

For the year ending June 30, 1865.

Number of cords of wood used by locomotives, 2,455,.....	}	6,769 78
Number of cords of wood used at stations, 50		
Number of gallons of oil,..... 1,060 1-2		1,577 49
Number of pounds of waste,..... 3,576		908 79
Damages to property, including fire and animals killed on road,.....		184 50
Agents at stations, &c.,.....		1,056 00
Clerks in general offices, &c.,.....		1,807 51
Labor, loading and unloading freight,...	}	1,144 04
Porters and watchmen,.....		
Switchmen,.....		
Conductors and baggagemen,.....		1,800 00
Brakemen,.....		2,150 00
Enginemen and firemen,.... .		2,789 37
For salaries of trustees, president, directors, secretaries, treasurer & superintendent,		2,666 67
For printing, stationery and office expenses,		600 00
For law expenses,.....		3 80

Other expenses in detail as follows:

Paid Vt. & Mass. R. R. for use of depot and track at Brattleboro,.....	1,666 68
Total,.....	25,124 63

RECAPITULATION OF EXPENSES.

Maintaining roadway,.....	26,163 44
Repairs of Machinery,...	4,627 45
Operating,.....	25,124 63
Total,.....	55,915 52

TABLE H.

EARNINGS, RECEIPTS AND PAYMENTS.

Earnings and Receipts.

From passengers,.....	49,899 83
From freight,.....	35,762 12
From other sources, viz.:	
Expresses,.....	953 44
Mails,	2,750 00
Rents,	5 00
Miscellaneous, use of cars,.....	902 45
Total,.....	90,272 84

VALUE OF MATERIALS ON HAND.

Road leased to Rutland and Burlington R. R. from June 1.

DETAILS OF EARNINGS FOR THE ELEVEN MONTHS ENDING MAY 31, 1865.

SOURCE.	JULY.	AUGUST.	SEPT.	OCTOBER.	NOVEMBER.	DECEMBER.
Through passengers.....	4,872 12	6,321 55	5,593 52	3,382 55	3,169 23	2,908 62
Way passengers.....	1,200 16	1,545 55	1,416 05	929 85	876 15	889 30
Through freight.....	3,288 89	2,573 60	2,882 19	3,282 56	3,194 38	2,789 09
Way freight.....	102 39	203 23	167 70	186 68	164 85	297 21
Express.....	70 59	70 59	70 59	70 59	70 59	70 59
Transport of mails.....	250 00	250 00	250 00	250 00	250 00	250 00
Use of engines.....						
Use of cars.....	126 64	54 02	70 44	70 76	99 89	70 58
Rent.....						
Totals.....						

DETAILS OF EARNINGS (CONTINUED.)

SOURCE.	JANUARY.	FEBRUARY.	MARCH.	APRIL.	MAY.	JUNE.
Through passengers.....	3,246 14	2,670 34	2,923 05	3,024 74	2,427 13	
Way passengers.	728 90	400 20	362 93	291 40	713 35	
Through freight.....	2,492 28	2,606 80	3,740 31	3,592 00	3,087 56	
Way freight.....	183 68	127 64	161 53	363 47	274 08	
Express.....	105 88	105 88	105 88	105 88	105 88	
Transport of mails.....	250 00	250 00	250 00	250 00	250 00	
Use of engines						
Use of cars.....	71 42	79 32	72 94	88 42	91 02	
Rent.....						
Totals						

TABLE I.

ACCIDENTS.

None.

TABLE J.

NUMBER OF EMPLOYEES AND COMPENSATION.

3 Conductors of passenger trains, each,	\$24 per month
1 Conductor of freight trains,.....	50 "
1 Master Mechanic,.....	75 "
6 Men in repair shops,.....	2 to 2 50 per day
2 Engine men of passenger trains,....	70 per month
1 Engine man of freight trains.....	70 "
3 Firemen,.....	40 "
3 Baggage men,.....	50 "
2 Switch men,.....	40 "
4 Section men, (foreman,).....	50 "
24 Section hands,.....	1 50 per day
2 Watch men,.....	40 per month
5 Station agents,.....	15 to 25 "
Clerks connected with passenger business, {	1,800 "
Clerks connected with freight business,... }	
Net earnings June 30, 1874,	\$46,607 44
Net earnings May 31, 1865,	44,357 32
Coupons paid for eleven months ending May	
31, 1865,	33,174 00

STATE OF VERMONT, {

WINDHAM COUNTY, SS. } 15th day of August, 1865.

I, James H. Williams, Treasurer of the Vermont Valley Railroad Company, do solemnly swear that the above is a true statement of the condition of the finances of said Company, their trustees, or assignees or lessees, on the 31st day of May, 1865.

J. H. WILLIAMS, *Treasurer.*

Sworn before me,

GEO. SLATE, *Notary Public.*

OFFICERS OF THE COMPANY.

SALARIES.

Trustees, Gouverneur Morris of Morrisania,
 Alexander Hamilton, Jr., and Hugh H.
 Henry,
 President, Hugh H. Henry, Chester, Vt.,..
 Superintendent, Madison Sloat, Bellows
 Falls, Vt.,
 Treasurer, James H. Williams, Bellows
 Falls, Vt., \$400 per annum
 Clerk, Larkin G. Mead, Brattleboro,

Directors.

HUGH H. HENRY,
 ALEXANDER HAMILTON, Jr.,
 GOVERNEUR MORRIS of Morrisania,
 G. R. J. BOWDOIN,
 CHARLES CHAPIN,
 JOEL PAGE,
 MADISON SLOAT,

STATE OF VERMONT, {
 WINDHAM COUNTY, SS. }

I, Henry F. Green, Chief Clerk of Vt. Valley R. R. Co.,
 depose and say that the facts set forth, and statements
 made in the foregoing report, which has been signed by
 me, are true and correct, according to the best of my
 knowledge, information and belief.

Signed, HENRY F. GREEN.

Subscribed and sworn to before me this 17th day of
 August, A. D. 1865.

RUSSELL HYDE, *Justice of the Peace.*

ANNUAL REPORT

OF THE CONNECTICUT & PASSUMPSIC RIVERS
RAILROAD COMPANY, FOR THE YEAR
ENDING MAY 31, 1865.

TABLE A.

STOCK AND DEBTS.

Capital stock as per charter,.....	\$3,000,000 00
Six per cent. preferred stock,.....	1,514,300 00
Stock not preferred,.....	163,700 00
Guaranteed stock,.....	1,600 00
Notes payable,.....	94,000 00
Bonded debt,.....	638,500 00
Average rate of interest on bonded debt, paid semi-annually,.....	6 per cent.

TABLE B.

COST OF CONSTRUCTION.

Total cost of construction as per last report, \$2,842,741 51
Road finished and operated to Newport, and graded to
Derby Line.

TABLE C.

EQUIPMENT.

Total cost of equipment as per last report, \$206,496 76

Water stations,.....	13
Dwellings,.....	6
Wood sheds,.....	18
Turn tables,.....	2
Other buildings, as follows:	
Car houses,.....	4
Building rented as a store, and occupied as general office,.....	1

EQUIPMENT.

Number of locomotives owned by the Company, on the
31st day of May, 1865,..... 9

	Under 16 tons	16 to 20.	20 to 25.	25 to 30.	30 tons and over.
In good repair,.....			5	1	
Requiring slight repair.....			2		
Requiring heavy repairs,.....				1	
Worn out,.....					

Number of cars owned by the company May 31, 1865,.....	288
First class 8 wheel passenger cars in good repair,.....	6
First class 8 wheel passenger cars wanting repair,.....	2
Baggage, express and mail cars in good re- pair,.....	3
Baggage, express and mail cars wanting re- pair,.....	1
Covered freight and cattle 8 wheel cars in good repair,.....	162
Covered freight and cattle 8 wheel cars wanting repair,.....	10
Platform 8 wheel cars in good repair,.....	56
Platform 8 wheel cars wanting repair,.....	6

Other cars,.....	29
Gravel cars,.....	11
Average weight of passenger cars,.....	24,000 lbs.
“ “ baggage “	17,000 “
“ “ box “	14,000 “
“ “ platform “	12,278 “

TABLE E.

BUSINESS OF THE YEAR.

Miles run by passenger trains.....	} 164,794
Miles run by freight trains,	
Miles run by gravel and construction trains,	
Miles run by wood trains,.....	
Number of through passengers carried in cars,.....	} See Table.
Number of way passengers,.....	
Number of tons of through freight.....	} See Table.
Number of tons of way freight,.....	
Average rate of speed of ordinary passenger trains, per hour, including stops,.....	23 miles
Average rate of speed of express trains per hour, including stops,.....	23 “
Average rate of speed of freight trains per hour, including stops,.....	10 “
Rate of fare charged 1st class through passengers per mile, about.....	3 1-4 cents
Rate of fare charged 1st class way passengers per mile, about	4 “
Rate per ton per mile charged on 1st class through freight,.....	5 ²⁰ / ₈₁ “
Rate per ton per mile charged on 2d class through freight,.....	4 ³⁷ / ₆₂ “
Rate per ton per mile charged on 3d class through freight,.....	3 ⁵⁹ / ₆₂ “
Rate per ton per mile charged on 4th class through freight,.....	3 ⁷ / ₃₁ “

Rate per ton per mile charged on 1st class way freight,.....	8 ¹⁴ ₂₁	"
Rate per ton per mile charged on 2d class way freight, ..	7 ¹ ₂₁	"
Total number of passengers carried 1 mile,	3,650,854	
Total number of soldiers carried 1 mile, not included in the above,.....	200,697	

TABLE F.

EXPENSES OF MAINTAINING ROADWAY AND REAL ESTATE.

For the year ending May 31, 1865.

Ordinary repairs of road bed and super- structure,.....	\$57,534 17	
Extraordinary repairs of road bed.....		
Cost of new rails used in repairs,.....		
Number and weight of chairs,.....		
Weight of spikes,.....		
Cost of repairs of rails,.....		
Number of cross ties used for renewals,...		
Cost of same,.....		
Cost of relaying rails and ties,.....		
Insurance and taxes on real estate,.....	1,871 50	
Repairs of bridges,.....	1,416 60	
Repairs of stations, and buildings, and new buildings,.....	5,071 70	
Repairs of fences, made by section men,...		
Total,.....	65,893 97	

COST OF REPAIRS OF MACHINERY.

Repairs of engines and tenders,.....	\$83,664 73	
Repairs of passenger and baggage cars,...		
Repairs of freight cars,.....		
Repairs of tools and machinery in shops,..		
Oil used about workshops,.....	see table G	
Total,.....	149,558 70	

TABLE G.

COST OF OPERATING THE ROAD.

For the year ending May 31, 1865.

Wood, including cost of preparing the same,	\$28,961 92
Cost of oil and waste for engines & tenders,	4,306 59
Cost of oil and waste for passenger and baggage cars,	
Cost of oil and waste for freight cars,	
Loss and damage of goods,	5,323 36
Loss and damage of baggage,	
Damages for injuries to persons,	none
Damages to property, including fire and animals killed on road,	included in above
18 agents at stations, &c.,	8,105 62
2 Clerks in general offices, &c.,	570 00
Labor, loading and unloading freight, mostly by station agents,	
9 Watchmen,	23,426 31
3 Switchmen,	
Wood and water station attendance,	
5 Conductors and 2 baggagemen,	
5 Brakemen,	
9 Enginemen and 9 firemen,	
For salaries of agent, treasurer and superintendent, and their office expenses, ..	5,345 53
For printing, stationery & office expenses, ..	16,799 91
For law expenses,	
Government tax, interest, general expenses, postage expenses as to joint business, and all other expenses not included in other accounts,	
Total,	243,710 10

RECAPITULATION OF EXPENSES.

Maintaining roadway, bridges, stations and buildings,	\$64,022 47
---	-------------

Repairs of machinery, insurance and taxes on real estate.....	85,536 23
Operating, losses and general expenses,...	94,151 40
Total,.....	243,710 10
Proportion of expenses due to passenger business,.....	\$90,263 00
Proportion of expenses due to freight busi- ness,.....	153,447 10
Total,.....	243,710 10

TABLE H.

EARNINGS, RECEIPTS AND PAYMENTS.

Earnings and Receipts.

From passengers.....	172,385 17
From freight,.....	240,975 67
From other sources, viz:	
Expresses,.....	3,000 00
Mails,.....	10,600 00
Rents,.....	1,219 50
Total,.....	\$428,180 34

Payments other than for Construction.

For transportation expenses, viz:

For passenger business,.....	13,983 61
For freight business,.....	19,498 67
For interest on funded debt,.....	48,000 00
For interest on floating debt,.....	5,341 00
For dividends, \$95,503 68,.....	} 100,278 36
Government tax on do., \$4,774 68,.....	

VALUE OF MATERIALS ON HAND.

Wood, 3,772 cords of,.....	\$11,495 00
Coal, 10 tons of,.....	201 60

Oil, 573 1-2 gallons of,.....	586 60
Waste, 3,925 pounds of,.....	339 25
Iron rails, tons of, old, 50,098 lbs.,.....	1,077 20
Iron rails, tons of, new, 10,.....	1,217 06
Chairs, pounds of, 9,337.....	420 16
Spikes, pounds of 750.....	60,00
Iron and other metals unwrought, 236,542lbs	10,756 00
Iron and other metals worked and partly worked, 59,500 lbs.,.....	8,501 22
Lumber, 274,377 feet,.....	6,111 51
Other items, specified as follows:	
3,000 cedar posts,.....	210 00
Sundries,.....	57,055 53
<hr/>	
Total,.....	98,131 13

DETAILS OF EARNINGS FOR THE YEAR ENDING JUNE 30, 1865.

SOURCE.	JULY.	AUGUST.	SEPT.	OCTOBER.	NOVEMBER.	DECEMBER.
Through passengers	4,865 50	7,241 50	5,201 50	3,467 50	2,819 00	2,120 00
Way passengers	8,006 00	10,357 00	10,472 50	5,757 00	4,600 50	4,052 50
Through freight, in lbs.,	6,692,885	6,299,240	6,518,306	7,990,146	6,799,865	6,762,579
Way freight, no acct. kept						
Express	250 00	250 00	250 00	250 00	250 00	250 00
Transport of mails	883 34	883 33	883 33	883 34	883 33	883 33
Use of engines	included in					
Use of cars	freight.					
Rent	101 62½	101 62½	101 62½	101 62½	101 62½	101 62½
Other earnings specified in detail as follows :						
Lumber, in feet, down						
Conn. river	1,299,230	1,128,009	842,133	978,728	1,034,688	613,874

DETAILS OF EARNINGS (CONTINUED.)

SOURCE.	JANUARY.	FEBRUARY.	MARCH.	APRIL.	MAY.	JUNE.
2,235 soldiers.....	2,095 00	2,085 50	2,545 50	2,542 50	2,543 50	2,924 50
Through passengers....	3,598 50	3,517 00	5,123 00	5,062 50	3,934 00	5,322 00
Way passengers.	6,031,361	3,511,097	6,669,472	4,971,259	7,025,481	8,074,383
Through freight, in lbs.,						
Way freight no acct. kept						
Express.....	250 00	250 00	250 00	250 00	250 00	250 00
Transport of mails	883 34	883 33	883 33	883 34	883 33	883 33
Use of engines.....	included in					
Use of cars.....	freight.					
Rent.....	101 62½	101 62½	101 62½	101 62½	101 62½	101 62½
Other earnings specified						
in detail as follows:						
Lumber, in feet, down						
Conn. river.....	504,965	431,095	661,693	748,318	1,212,029	1,198,865

TABLE I.

ACCIDENTS.

Total number of persons killed,..... 2

In October last, Wallace Howard, laborer on wood train, was instantly killed at McIndoes Falls, by being run over while attempting to pass from the engine to car attached.

January 21, 1865, Henry Foote, laborer in wood yard, at Newport, was injured while attempting to couple cars which were being pushed by hand, and lived but eight hours.

TABLE J.

NUMBER OF EMPLOYEES AND COMPENSATION.

2 Conductors of passenger trains.....	1,560 00
3 Conductors of freight trains,.....	2,070 00
1 Master Mechanic,.....	1,200 00
1 Road master,.....	900 00
52 Men in repair shops, average number,...	30,613 95
4 Engine men of passenger trains*.....	2,262 00
5 Engine men of freight trains,.....	4,729 00
9 Fire men,**	3,510 00
2 Baggage men,.....	1,200 00
3 Switch men,.....	1,075 31
26 Section men (foremen).....	12,357 16
49 Section hands, average number,.....	20,225 88
9 Watch men,.....	4,320 00
18 Station agents,.....	8,105 62
9 Other laborers,.....	4,538 50
5 Brakemen,	2,700 00
35 Wood hands (average number.).....	17,591 57
20 Gravel Hands, "	7,423 91
1 Clerk connected with passenger business,	300,00

* 2 Engineers all the year; 2 Engineers, extra 3 months.

** 2 Firemen extra 3 months.

1 Clerk connected with freight business,...	270 00
1 Superintendent of bridges—salary,.....	} 958 00
Wood Agent,.....	
Other agents—how employed—and the salary of each as follows :	
Master Transportation.....	} 2,524 00
Ticket Master and Cashier,.....	

OFFICERS OF THE COMPANY.

SALARIES.

Agent,.....	1,200 00
Trustees,.....	None.
President,.....	
Superintendent,.....	2,000 00
Treasurer,.....	1,500 00

HENRY KEYES, *President & Agent, Newbury, Vt.*

JOSIAH STICKNEY, *Boston, Vice President.*

NATH'L P. LOVERING, *Treasurer, Boston.*

A. H. PERRY, *Superintendent, St. Johnsbury, Vt.*

STATE OF VERMONT, }
 CALEDONIA COUNTY, SS. }

I, Henry Keyes, President, depose and say that the facts set forth, and statements made in the foregoing report which has been signed by me are true and correct according to the best of my knowledge, information and belief.

Signed, HENRY KEYES, *President.*

Subscribed and sworn to before me this 9th day of September, A. D., 1865.

HUBBARD HASTINGS, *Master in Chancery.*

ANNUAL REPORT

OF THE BOARD OF DIRECTORS OF THE RUTLAND
AND WHITEHALL RAILROAD COMPANY,
FOR THE YEAR ENDING
JUNE 30, 1865.

The road of this company extends from its eastern terminus, in the village of Castleton, where it forms a connection with the road of the Rutland and Washington Railroad Co., westwardly through Hydeville and Fairhaven, to the west line of the State in Fairhaven, where it connects with the Saratoga and Whitehall Railroad.

The length of this road, including the branch at Hydeville, is seven miles and a fraction, and the same is leased perpetually to the Saratoga and Whitehall R. R. Co., at a rent equal to six per cent. upon its whole capital stock, payable quarter-yearly. Its whole capital stock is \$255,700, including cost of road, and branch, and rolling stock, and this stock is divided into shares of \$100 each, and the same is owned by 106 different persons, residing in the Eastern, Middle and Western States. The quarterly rents have been promptly paid, and as promptly distributed to the stockholders during the past year.

The company have upon the line of the road four bridges, three station houses and five turn-outs.

The condition and state of repair of the road and bridges are believed to be good, and safe for travelling.

By an arrangement betwixt the lessees of this company and the Rensselaer and Saratoga Railroad Company, the control and direction of this road passed into the hands of the latter company about the first of May last. At the same time, also, the same company obtained by contract the control of the Rutland and Washington Railroad, extending from Rutland, by way of Castleton and Poultney, to Eagle Bridge. The beneficial consequences of this arrangement, as well to the business and travelling public, as to those interested pecuniarily, in the successful operation of this vastly important thoroughfare, have already become manifest and satisfactory.

The *natural* channel of intercourse, *through Vermont*, from the Eastern cities to the great West, is now being used without interruption, as it should have been for the past fifteen years, and the only wonder which seems to be felt, is that a few men have so long been permitted to place and keep a stumbling block upon this natural line of intercommunication. The Act of the Legislature of this State, at its last session incorporating an independent Rail Road from Rutland to Castleton, effectually removed this stumbling block forever, and in our judgment, that Honorable body have, and deserve, the gratitude of the public for so doing.

This company owe no debts and receive no revenue from their road except the rent aforesaid.

The Board of Directors and Officers for the current year, are the following:

A. W. HYDE, Castleton,	} <i>Directors.</i>
WM. C. KITTREDGE, Fairhaven,	
ISRAEL DAVEY, Fairhaven,	
W. W. COOK, Whitehall,	
G. R. I. BOWDOIN, New York,	

A. W. HYDE, *President.*

W. W. COOK, *Vice President.*

WM. C. KITTREDGE, *Clerk, Treasurer and Transfer Agent.*

The earnings as well as the expenses of operating, this road, are so identified with the business of the other roads of the lessees, as to render it utterly impossible for the Directors of this road to exhibit in detail the particulars, or the amount of either.

By order of the Board of Directors,

WM. C. KITTREDGE, *Director.*

STATE OF VERMONT, {
RUTLAND COUNTY, SS. }

At Fairhaven, in said County, this 2d day of September, A. D. 1865, personally appeared Wm. C. Kittredge, who signed the foregoing statement in my presence, and made oath that the same is true, according to his best knowledge, information and belief.

Before me,

A. ALLEN, *Justice of the Peace.*

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ELEVENTH

ANNUAL REPORT

OF THE

RAILROAD COMMISSIONER

OF THE

STATE OF VERMONT,

TO THE

GENERAL ASSEMBLY,

OCTOBER SESSION, 1866.



RUTLAND:
TUTTLE, GAY & CO., PRINTERS.
1866.



ELEVENTH
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OF THE
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1866.



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1865/66

RAILROAD COMMISSIONER'S REPORT.

To the General Assembly of the State of Vermont.

The undersigned respectfully reports upon the condition and administration of the Railroads of this State, for the current official year, as follows :

In pursuance of my duties as defined by law, I furnished to the proper officers of all the Railroad Corporations of this State (including such without the limits of the State, as are immediately and by some real connexion associated with the Roads in this State) tabular blank interrogatories, involving answers adapted to develop all of the needed information for the use of the public, through your honorable body, and have received for answer such returns as are embodied in this report, being all of the returns seasonably made for publication herewith.

I have traveled over the different Roads in the State, and over large sections thereof on foot, and made particular examination by all available tests, as to the condition for public and individual safety of grade, slopes, curves, track, sub-structure and superstruction, and have found more or less to invite the attention of the proper departments to, as demanding attention, and am happy to say that influenced by a common danger, and moved by a common prudence, and stimulated by a superior interest, no lack of gratitude or prompt attention to the subject, has in any single instance been manifested.

Our Roads are really, and in comparison with connecting roads, safely and obligingly administered and operated, and with commendable liberality unite to facilitate the development of the power and progress of the State, and seem to hold such relations to each other and the public as will guard against undue monopoly or oppression. Many of the Roads are and have been making large improvements in their stations, manufactories and rolling stock.

The immediate employees upon our Roads are particularly interested in such improvement in the height of bridges and above grade crossings, as shall abate the peril of brakemen on the freight trains, which occasionally has proved fatal to them on our various Roads.

Whether this evil will as a business necessity regulate itself between employer and employee, or whether your honorable body should interpose, is submitted to your superior wisdom.

Respectfully submitted,

D. E. NICHOLSON,
Railroad Commissioner.

DOCUMENTS.

Form of Annual Report required by the Commissioner of the
several Railroad Companies for the year ending June 30, 1866 :

ANNUAL REPORT OF THE—RAILROAD COMPANY, FOR THE YEAR ENDING JUNE 30, 1866.

TABLE A.

STOCK AND DEBTS.

NOTE—Under this head state the amount paid for interest, discounts, &c.,
amount of funded debt, floating debt—rate of interest—amount of interest
coupons due and unpaid.

TABLE B.

COST OF CONSTRUCTION.

Total cost of construction as per last report,.....
Expended since last report, viz :.....
For graduation and masonry,.....
“ bridges,.....
“ rails,.....
“ chairs, spikes and ties,.....
“ laying superstructure,.....
“ buildings and fixtures,.....

NOTE—State, also, the progress of the work, cost of graduation, super-
structure, and all incidental expense, on any extension or alteration of road
to June 30, 1866.

Also state the amount paid for interest, discount, &c., charged to construc-
tion account.

TABLE C.

EQUIPMENT.

Total cost of equipment as per last report,.....
 Expended since last report,.....
 Total cost of road and equipment,....

TABLE D.

CHARACTERISTICS OF ROAD.

Length of road,.....
 “ “ completed,.....
 “ “ branches,.....
 “ “ side track,.....
 Weight of rail per yard,....

NOTE—State length of road, branches and side tracks within the State.

CHARACTER AND LENGTH OF BRIDGING.

	No. of structures.	No. of spans.	Length of bridging in feet.
Trustle bridging,			
Trustle bridging, 50 feet span and under,			
Trustle bridging from 50 to 100 ft. span,			
Trustle bridging from 100 to 150 ft. span,			
Trustle bridging 150 ft. span and over,			
Draw bridges,			
Totals,			

Number of road crossings at grade,.....
 Number of road crossings above and below grade,
 Number of cross ties per mile,.....
 Chairs, number per mile,.....
 Whole number of switches on main track,.....

GRADIENTS AND ALIGNMENT.

Level, number of miles,.....
 Maximum grade,.....
 Amount of straight line, miles,

Amount of curved line, miles,
 Maximum radius,
 Minimum radius,
 Sum of ascents going in one direction,
 Sum of ascents going in opposite direction,
 Height of termini and summit above tide water, . .

BUILDINGS AND FIXTURES.

Passenger houses,
 Freight houses,
 Engine houses,
 Repair shops,
 Water stations,
 Dwellings,
 Wood sheds,
 Turn tables,

Other buildings, as follows :

EQUIPMENT.

Number of locomotives owned by the Company June 30, 1866.

	Under 16 tons.	16 to 20.	20 to 25.	25 to 30.	30 tons and over.
In good repair,					
Requiring slight repairs,					
Requiring heavy repairs,					
Worn out,					

Number of cars owned by the Company, June 30, 1866.

First class 8 wheel passenger cars in good repair,
 First class 8 wheel passenger cars wanting repair,
 Second class 8 wheel passenger cars in good re-
 pair,
 Second class 8 wheel passenger cars wanting re-
 pair,
 Baggage, express and mail cars in good repair, . .

Baggage, express and mail cars wanting repair,..
Covered freight and cattle 8 wheel cars in good repair,.....
Covered freight and cattle 8 wheel cars wanting repair,.....
Platform 8 wheel cars in good repair,.
Other freight cars,.....
Gravel cars,
Average weight of passenger cars,.....
Average weight of baggage cars,.....
Average weight of box cars,.....
Average weight of platform cars,.....

TABLE E.

BUSINESS OF THE YEAR.

Miles run by passenger trains,.....
Miles run by freight trains,....
Miles run by gravel and construction trains,....
Miles run by wood trains,.....
Number of through passengers carried in cars,..
Number of way passengers,.....
Number of passengers having passes,.....
Number of miles traveled by way passengers,....
Average distance traveled by way passengers,....
Number of miles traveled by passengers having passes,.....
Number of tons through of freight,.....
Number of tons of way freight,
Number of tons of way freight carried 1 mile,..
Number of tons of through freight moved towards market,.....
Number of tons of through freight moved from market,.....
Number of tons of way freight moved towards market,...
Number of tons of way freight moved from market,
Average rate of speed of ordinary passenger trains,

Average rate of speed of express trains,...	.....
Average rate of speed of freight trains,.....	.....
Rate of fare charged 1st class through passengers per mile,...	.....
Rate of fare charged 1st class way passengers per mile,.....	.....
Average rate of fare charged 2d class passengers per mile,.....	.....
Rate per ton per mile charged on 1st class through freight,.....	.....
Rate per ton per mile charged on 2d class through freight,..	.....
Rate per ton per mile charged on 3d class through freight,.....	.....
Rate per ton per mile charged on 4th class through freight,.....	.....
Rate per ton per mile charged on 1st class way freight,.....	.....
Rate per ton per mile charged on 2d class way freight,.....	.....
Rate per ton per mile charged on 3d class way freight,.....	.....
Rate per ton per mile charged on 4th class way freight,.....	.....
Total number of tons of freight carried 1 mile,...	.....
Total number of passengers carried 1 mile,.....	.....

TABLE F.

EXPENSES OF MAINTAINING ROADWAY AND REAL ESTATE,
For the year ending June 30, 1866.

Ordinary repairs of road bed and superstructure,.	
Extraordinary repairs of road bed,.....	
Cost of new rails used in repairs,...	...
Number and weight of chairs,...	.....
Weight of spikes,..	.....
Cost of repairs of rails,.	.....
Number of cross ties used for renewals,.....	
Cost of same,.	.....

Cost of relaying rails and ties,.....	
Insurance and taxes on real estate,.....	
Repairs of bridges,.....	
Repairs of stations,.....	
Repairs of fences,.....	
Repairs of masonry,.....	
Total,.....	

COST OF REPAIRS OF MACHINERY.

Repairs of engines and tenders,.....	
Repairs of passenger and baggage cars,.....	
Repairs of freight cars,.....	
Repairs of tools and machinery in work-shops,...	
Oil used about work-shops,.....	
Fuel,.....	
Waste,.....	

Other items in detail as follows :

Total,

TABLE G.

COST OF OPERATING THE ROAD.

For the year ending June 30, 1866.

Wood, including cost of preparing the same,.. ..	
Number of cords of wood used by locomotives,.. ..	
Number of tons of coal used by locomotives,.....	
Number of cords of wood used at stations,.....	
Number of tons of coal used at stations,.....	
Number of cords lost by fire,.....	
Number of gallons of oil,.....	
Number of pounds of waste,.....	
Cost of oil and waste for engines and tenders,....	
Cost of oil and waste for passenger and baggage cars,.....	
Cost of oil and waste for freight cars,.. ..	
Loss and damage of goods,.....	
Loss and damage of baggage,	
Damages for injuries to persons,.....	

Damages to property, including fire and animals killed on road,.....	
Agents at stations, &c.,.....	
Clerks in general offices, &c.,.....	
Labor loading and unloading freight,...	
Porters and watchmen,.....	
Switchmen,.....	
Wood and water station attendance,.....	
Conductors and baggagemen,.....	
Brakemen,.....	
Enginemen and firemen,...	
For salaries of trustees, president, directors, secre- taries, treasurer and superintendent,..	
For printing, stationery and offices expenses,....	
For law expenses,.....	
Other expenses in detail, as follows :	
Total, .. .	

RECAPITULATION OF EXPENSES.

Maintaining roadway,	
Repairs of machinery,.....	
Operating,.....	
Proportion of expenses due to passenger business,	
Proportion of expenses due to freight business,...	
Total,	

TABLE H.

EARNINGS, RECEIPTS AND PAYMENTS.

Earnings and Receipts.

From passengers,.....	
From freight,.....	
From other sources, viz :	
Expresses,.....	
Mails,	
Rents,.....	
Miscellaneous,.....	

Payments other than for Construction.

For transportation expenses, viz :

For passenger business,.....	
For freight business,.....	
For other business, and what,.....	
For interest on funded debt,..	.....
For interest on floating debt,.....	
For dividends,.....	
For carried to surplus fund,.....	
For amount of surplus fund,..	.....

VALUE OF MATERIALS ON HAND.

Wood, cords of,.....	.....
Coal, tons of,.....	.....
Oil, gallons of,.....	.....
Waste, pounds of,.....	.....
Iron rails, tons of, old,.....	.....
Iron rails, tons of, new,...	.....
Chairs, pounds of,.....	.....
Spikes, pounds of,.....	.....
Ties, number of,.....	.....
Iron and other metals, unwrought,.....	.....
Iron and other metals, worked and partly worked,	
Lumber, .....	.....

Other items specified as follows :

COST OF TRANSPORTATION.

Actual cost of transporting freight per ton, per mile,	
Actual cost of transporting passengers, per mile,..	

DETAILS OF EARNINGS FOR THE YEAR ENDING JUNE 30, 1866.

SOURCE.	JULY.	AUGUST.	SEPTEMBER.	OCTOBER.	NOVEMBER.	DECEMBER.
Through passengers.....						
Way passengers.....						
Through freight.....						
Way freight.....						
Express.....						
Transport of mails.....						
Use of engines.....						
Use of cars.....						
Rent.. ..						
Other earnings specified in detail as follows :						
Total... ..						

DETAILS OF EARNINGS (CONTINUED.)

SOURCE.	JANUARY.	FEBRUARY.	MARCH.	APRIL.	MAY.	JUNE.
Through passengers,.....						
Way passengers,.....						
Through freight,.....						
Way freight,.....						
Express,.....						
Transport of mails,...						
Use of engines,.....						
Use of cars,.....						
Rents,.....						
Other earnings specified in detail as follows:						
Total						

TABLE I.

ACCIDENTS.

State the number of persons injured in life or limb, and the cause thereof, and whether passenger or persons employed.

State whether any such accidents have arisen from carelessness or negligence of any person in the employ of the corporation, and whether such person is retained in the service of the corporation.

	EMPLOYEES.		OTHERS.	
	Killed.	Injured.	Killed.	Injured.
Trains thrown from track, . . .				
Struck by bridge while on top of freight car,				
Run over while walking on track, .				
Injured at road crossing,				
Total,				

Total number of persons killed,

Total number of persons injured but not killed, ..

In addition to which must be given a statement of the date of each accident, the place where it occurred, the train, the cause and the extent of the injuries inflicted upon each person, and the name of such person.

TABLE J.

NUMBER OF EMPLOYEES AND COMPENSATION.

Conductors of passenger trains.

Amount of compensation,

Conductors of freight trains.

Amount of compensation,

Conductors of wood and gravel trains.

Amount of compensation,

Master mechanics.

Amount of compensation,.....

Road masters.

Amount of compensation,.....

Men in repair shops.

Amount of compensation,.....

Enginemen of passenger trains.

Amount of compensation,.....

Enginemen of freight trains.

Amount of compensation,.....

Enginemen of wood and gravel trains.

Amount of compensation,.....

Firemen.

Amount of compensation,.....

Baggagemen.

Amount of compensation,.....

Switchmen.

Amount of compensation,.....

Section men, (foremen.)

Amount of compensation,.....

Section hands.

Amount of compensation,.....

Watchmen.

Amount of compensation,.....

Station agents.

Amount of compensation,.....

Other laborers.

Amount of compensation,.....

Clerks connected with passenger business.

Amount of compensation,.....

Clerks connected with freight business.

Amount of compensation,.....

Superintendent of bridges—salary,

Wood agent—salary,.....

Other agents—how employed, and the salary of each, as follows,
viz :

Do your stationmen allow boys without business to frequent and remain in or about the stations, platforms, and trains?

Are your road fences and cattle guards sufficiently constructed and supported, and kept in repair as the law demands?

STATE OF VERMONT, }
COUNTY, SS. }

depose and say
that the fact set forth, and statements made in the foregoing report which has been signed by are true and correct according to the best of knowledge, information and belief.

Signed,

Subscribed and sworn before me this day of

ANNUAL REPORT

OF THE

VT. CENTRAL & VT. & CANADA R. R. CO.,

FOR THE

YEAR ENDING MAY 31, 1866.

TABLE A.

STOCK AND DEBTS.

One hundred thousand shares of stock issued:

\$2,000,000 First Mortgage Bonds issued at 7 per cent. interest.

\$1,500,000 Second Mortgage Bonds issued at 7 per cent. interest.

TABLE B.

COST OF CONSTRUCTION.

Total cost of construction as per last report, 8,402,054 92

Expended since last report, viz :

For graduation and masonry,

“ bridges,

“ rails,

“ chairs, spikes and ties,

“ laying superstructure,

“ buildings and fixtures,

TABLE C.

EQUIPMENT.

Total cost of equipment as per last report,

Expended since last report,

Total cost of road and equipment,

TABLE D.

CHARACTERISTICS OF ROAD.

Length of road,.....	{	Vt. Central,	117 miles.
	{	Vt. & Canada,	65½ miles.
“ “ completed,.....			182½ miles.
“ “ branches,.....			2 miles.
“ “ side track,.....			26½ miles.
Weight of rail per yard,....			56 to 60 pounds.

CHARACTER AND LENGTH OF BRIDGING.

	No. of structures.	No. of spans.	Length of bridging in feet.
Trustle bridging,	8		7990
Trustle bridging, 50 feet span and under,	45	55	1595
Trustle bridging from 50 to 100 ft. span,	6	6	357
Trustle bridging from 100 to 150 ft. span,	19	30	3681
Trustle bridging 150 ft. span and over,	13	27	4240
Draw bridges,	2		339
Totals,	93	118	18,202

Number of road crossings at grade,.....	124
Number of road crossings above and below grade,	34
Number of cross ties per mile,.....	2200
Chairs, number per mile,.....	440 to 700
Whole number of switches on main track,.....	138

GRADIENTS AND ALIGNMENT.

Level, number of miles,.....	unknown.
Maximum grade, 45 feet to a mile.	
Amount of straight line, miles, on Vt. Central,	83
Amount of curved line, miles, on Vt. Central,	34
Maximum radius,.....	11,460 feet.
Minimum radius,.....	1,146 feet.
Sum of ascents going in one direction,.....	unknown.
Sum of ascents going in opposite direction,.....	“
Height of termini and summit above tide water,	“

BUILDINGS AND FIXTURES.

Passenger houses,.....	36
Freight houses,.....	35
Engine houses,.....	6
Repair shops,.....	3
Water stations,..	28
Dwellings,.....	22
Wood sheds,..	45
Turn tables,..	4
Other buildings, as follows:	
Car Houses,...	2
Ice Houses,.....	5

EQUIPMENT.

Number of locomotives owned by the Company, May 31, 1866.

	Under 16 tons.	16 to 20.	20 to 25.	25 to 30.	30 tons and over.
In good repair,			3	38	
Requiring slight repairs,				6	
Requiring heavy repairs,			4	8	
Worn out,		1		1	

Number of cars owned by the Company, May 31, 1866.

First class 8 wheel passenger cars in good repair,	37
First class 8 wheel passenger cars wanting repair,	4
Second class 8 wheel passenger cars in good repair,.....	0
Second class 8 wheel passenger cars wanting repair,.....	0
Baggage, express and mail cars in good repair,..	11
Baggage, express and mail cars wanting repair,..	3
Covered freight and cattle 8 wheel cars in good repair,.....	885
Covered freight and cattle 8 wheel cars wanting repair,.....	56

Platform 8 wheel cars in good repair,	158
Other freight cars,	34
Gravel cars,	40
Average weight of passenger cars,	15 tons.
Average weight of baggage cars,	10 tons.
Average weight of box cars,	7 $\frac{3}{4}$ tons.
Average weight of platform cars,	6 $\frac{1}{2}$ tons.
Derrick Cars,	5
Paymaster's Car,	1
Snow Plows,	4

TABLE E.

BUSINESS OF THE YEAR.

Miles run by passenger trains,	396,046
Miles run by freight trains,	654,324
Miles run by gravel and construction trains,	43,471
Miles run by wood trains,	28,930
Number of through passengers carried in cars,	160,345 $\frac{1}{2}$
Number of way passengers,	204,736 $\frac{1}{2}$
Number of passengers having passes,	
Number of miles traveled by way passengers,	4,378,808
Average distance traveled by way passengers,	21 $\frac{1}{2}$ miles.
Number of miles traveled by passengers having passes,	
Number of tons through of freight,	193,309
Number of tons of way freight,	168,451
Number of tons of way freight carried 1 mile,	13,048,613
Number of tons of through freight moved towards market,	Don't know.
Number of tons of through freight moved from market,	"
Number of tons of way freight moved towards market,	"
Number of tons of way freight moved from market,	"
Average rate of speed of ordinary passenger trains, 22 m's per hour	
Average rate of speed of express trains,	27 $\frac{1}{2}$ " "
Average rate of speed of freight trains,	12 " "

Rate of fare charged 1st class through passengers per mile,	3 $\frac{1}{4}$ cents.
Rate of fare charged 1st class way passengers per mile,	4 cents.
Average rate of fare charged 2d class passengers per mile,	2 cents.
Rate per ton per mile charged on 1st class through freight,	Rates
Rate per ton per mile charged on 2d class through freight,	vary
Rate per ton per mile charged on 3d class through freight,	from
Rate per ton per mile charged on 4th class through freight,	one
Rate per ton per mile charged on 4th class through freight,	and
Rate per ton per mile charged on 1st class way freight,	a half
Rate per ton per mile charged on 2d class way freight,	to
Rate per ton per mile charged on 3d class way freight,	ten
Rate per ton per mile charged on 4th class way freight,	cents
Rate per ton per mile charged on 1st class way freight,	a mile,
Rate per ton per mile charged on 2d class way freight,	according
Rate per ton per mile charged on 3d class way freight,	to
Rate per ton per mile charged on 4th class way freight,	season
Rate per ton per mile charged on 1st class way freight,	and
Rate per ton per mile charged on 2d class way freight,	distance.
Total number of tons of freight carried 1 mile, . . .	38,130,512
Total number of passengers carried 1 mile,	14,674,582

TABLE F.

EXPENSES OF MAINTAINING ROADWAY AND REAL ESTATE,

For the year ending May 31, 1866.

Ordinary repairs of road bed and superstructure, .	\$149,997 71
Extraordinary repairs of road bed,	
Cost of new rails used in repairs,	79,629 84
Number and weight of chairs,	
Weight of spikes,	
Cost of repairs of rails,	25,702 63
Number of cross ties used for renewals,	
Cost of same,	16,975 63
Cost of relaying rails and ties,	15,207 28

Insurance and taxes on real estate,.....	8,983 65
Repairs of bridges,.....	76,582 01
Repairs of stations,.....	46,752 93
Repairs of fences,.....	6,699 22
Repairs of masonry,.....	
Depot Furniture,.....	2,636 08
Total,.....	\$429,166 98

COST OF REPAIRS OF MACHINERY.

Repairs of engines and tenders,.....	\$185,450 02
Repairs of passenger and baggage cars,.....	13,146 62
Repairs of freight cars,.....	59,941 45
Repairs of tools and machinery in work-shops,...	1,078 63
Oil used about work-shops,.....	3,160 43
Fuel,.....	17,850 00
Waste,.....	520 80

Other items in detail as follows :

Repairs of gravel, section and hand cars,.....	4,444 43
“ “ snow plows,.....	1,555 64
“ “ road tools,.....	5,805 87
Total,.....	\$292,953 89

TABLE G.

COST OF OPERATING THE ROAD.

For the year ending May 31, 1866.

Fuel, including cost of preparing the same,..	\$215,944 71
Number of cords of wood used by locomotives,...	
Number of tons of coal used by locomotives,.....	
Number of cords of wood used at stations,.....	
Number of tons of coal used at stations,.....	
Number of cords lost by fire,.....	
Number of gallons of oil,.....	
Number of pounds of waste,.....	
Cost of oil and waste for engines and tenders,...	32,867 92
Cost of oil and waste for passenger and baggage cars,.....	2,896 35
Cost of oil and waste for freight cars,...	14,499 20

Loss and damage of goods,	8,237 66
Loss and damage of baggage,	900 69
Damages for injuries to persons,	2,975 51
Damages to property, including fire and animals killed on road,	1,298 17
Agents at stations, &c.,	28,648 13
Clerks in general offices, &c.,	13,982 63
Labor loading and unloading freight,	31,650 04
Porters and watchmen,	9,733 43
Switchmen,	9,427 71
Wood and water station attendance,	
Conductors, baggagemen and brakemen,	59,616 84
Enginemen and firemen,	56,263 46
For salaries of trustees, president, directors, secre- taries, treasurer and superintendent,	19,159 47
For printing, stationery and offices expenses,	5,687 25
For law expenses,	5,153 94
Other expenses in detail, as follows :	
Advertising,	1,281 08
Government taxes,	43,347 50
Interest,	30,071 29
Paid by decree of Chancellor,	19,066 49
General expenses, not included above,	27,557 36
Total,	\$640,266 84

RECAPITULATION OF EXPENSES.

Maintaining roadway,	429,166 98
Repairs of machinery,	292,953 89
Operating,	640,266 84
Proportion of expenses due to passenger business,	
Proportion of expenses due to freight business,	
Total,	1,362,387 71

TABLE H.

EARNINGS, RECEIPTS AND PAYMENTS.

Earnings and Receipts.

From passengers,	525,433 15
----------------------------	------------

From freight,.....	1,089,156 17
From other sources, viz :	
Expresses,.....	17,425 04
Mails,.....	24,984 96
Rents,.....	428 12
Miscellaneous,.....	
	<hr/>
	\$1,657,427 44

Payments other than for Construction.

For transportation expenses, viz :

For passenger business,.....	
For freight business,.....	
For other business, and what,.....	
For interest on funded debt,..	
For interest on floating debt,.....	
For dividends,.....	
For carried to surplus fund,.....	
For amount of surplus fund,..	

VALUE OF MATERIALS ON HAND.

Wood, cords of,.....	71,228
Coal, tons of,.....	1,000
Oil, gallons of,.....	956
Waste, pounds of,.....	2,000
Iron rails, tons of, old,.....	1,200
Iron rails, tons of, new,..	
Chairs, pounds of,.....	
Spikes, pounds of,.....	
Ties, number of,.....	
Iron and other metals, unwrought,.....	(lbs) 128,299
Iron and other metals, worked and partly worked, (lbs)	360,822
Lumber,.....	(ft.) 475,750

Other items specified as follows :

COST OF TRANSPORTATION.

Actual cost of transporting freight per ton, per mile,
Actual cost of transporting passengers, per mile,.

DETAILS OF EARNINGS FOR THE YEAR ENDING MAY 31, 1866.

SOURCE.	JUNE, 1865.	JULY.	AUGUST.	SEPTEMBER.	OCTOBER.	NOVEMBER.
Through passengers.....	24,238 61	32,177 71	44,479 25	37,718 22	36,241 96	27,050 91
Way passengers.....	15,961 35	24,573 47	22,432 55	21,061 35	15,740 64	17,843 63
Through freight...	76,794 44	78,464 78	87,531 59	93,922 50	105,272 65	105,917 00
Way freight.....	7,853 91	6,559 74	8,463 05	7,062 79	8,267 44	10,669 97
Express.....	608 33	608 33	608 34	608 33	608 33	608 34
Transport of mails.....	2,082 08	2,082 08	2,082 08	2,082 08	2,082 08	2,082 08
Use of engines.....						
Use of cars.....						
Rent.. ..						
Other earnings specified in detail as follows :						
Total... ..	127,538 72	144,466 11	165,596 86	162,455 27	168,213 10	164,171 93

DETAILS OF EARNINGS (CONTINUED.)

SOURCE.	DEC., 1865.	JAN., 1866.	FEBRUARY.	MARCH.	APRIL.	MAY, 1866.
Through passengers,.....	22,746 73	17,331 83	16,695 76	23,141 09	24,519 24	23,203 31
Way passengers,.....	11,807 92	9,999 23	8,806 62	12,308 97	12,192 90	23,159 90
Through freight,.....	94,826 81	71,462 58	61,312 24	68,093 89	65,712 95	84,150 94
Way freight,.....	7,773 70	6,009 64	4,605 19	8,505 26	9,143 24	10,779 87
Express,.....	603 33	608 34	608 33	608 33	608 33	10,733 38
Transport of mails,...	2,082 08	2,082 08	2,082 08	2,082 08	2,082 08	2,082 08
Use of engines,.....						
Use of cars,.....						
Rents,						428 12
Other earnings specified in detail as follows:						
Total.....	139,845 57	107,493 70	94,110 22	114,739 62	114,258 74	154,537 60

TABLE I.

ACCIDENTS.

State the number of persons injured in life or limb, and the cause thereof, and whether passenger or persons employed.

State whether any such accidents have arisen from carelessness or negligence of any person in the employ of the corporation, and whether such person is retained in the service of the corporation.

	EMPLOYEES.		OTHERS.	
	Killed.	Injured.	Killed.	Injured.
Trains thrown from track,			1	
Struck by bridge while on top of freight car,				
Run over while walking on track, .	2		2	
Injured at road crossing,				
Total,				

Total number of persons killed, 8

Total number of persons injured but not killed, 5

In addition to which must be given a statement of the date of each accident, the place where it occurred, the train, the cause and the extent of the injuries inflicted upon each person, and the name of such person.

AUGUST 15, 1865.—Frederick Willey, in trying to get upon a freight train at Northfield, fell between the cars and was killed.

OCTOBER 25, 1865.—Patrick McCarty, while walking on the track between Northfield and Falls Village, was run over and killed by a freight train.

OCTOBER 26, 1865.—John Mullen was run over and killed by a passenger train in St. Albans depot.

DECEMBER 13, 1865.—Cyriac Moreau was killed near Georgia while standing on the platform of a sleeping car—the train having been stopped by an accident to the locomotive.

FEBRUARY 23, 1866.—Edward King, a brakeman on a freight

train, was killed in Berlin—the car he was on being thrown from the track by breaking of an axle.

MARCH 3, 1866.—Michael Fitzmorris, a section man, was run over and killed while walking on the track between White River Junction and White River Village.

APRIL 13, 1866.—The night express train going north ran into a bridge across a small stream, which was burnt in the night, and Joseph Harris, a passenger, was killed and a number of passengers were injured—the conductor was also severely injured, and the engineer slightly.

MAY 28, 1866.—James Brady, a brakeman on a freight train, was killed by an engine, while waiting on the track in the yard at White River Junction.

TABLE J.

NUMBER OF EMPLOYEES AND COMPENSATION.

13 Conductors of passenger trains.		
Amount of compensation, (per month)		\$50 to 75
30 Conductors of freight trains.		
Amount of compensation, (per month,)... ..		45 to 55
3 Conductors of wood and gravel trains.		
Amount of compensation, (per month,).....		60
1 Master mechanic.		
Amount of compensation, (per year)		4,000
2 Road masters.		
Amount of compensation, (per year) both,		1,800
295 Men in repair shops.		
Amount of compensation, (per day)		1 to 4
12 Enginemen of passenger trains.		
Amount of compensation, (per day).		2 to 3
31 Enginemen of freight trains.		
Amount of compensation, (per day)		2 to 3
3 Enginemen of wood and gravel trains.		
Amount of compensation, (per day)		2 to 3
42 Firemen.		
Amount of compensation, (per day)		1 75

10 Baggage men.	
Amount of compensation, (per month)	45 to 50
31 Switchmen.	
Amount of compensation, (per month)	26 to 39
42 Section men, (foremen.)	
Amount of compensation, (per month)	39 to 60
316 Section hands.	
Amount of compensation, (per day)	1 25 to 1 40
30 Watchmen.	
Amount of compensation, (per day)	1 to 2
35 Station agents.	
Amount of compensation, (per year)	60 to 1,100
359 Other laborers.	
Amount of compensation, (per day)	1 to 4
4 Clerks connected with passenger business.	
Amount of compensation, (per month)	220
7 Clerks connected with freight business.	
Amount of compensation, (per month)	606 66
Superintendent of bridges—salary, (per year)	1,200
Wood agent—salary, (per year),	1,100
Other agents—how employed, and the salary of each, as follows :	

OFFICERS OF THE COMPANY.

SALARIES.

Trustees,	
President,	
Superintendent,	
Treasurer,	

STATE OF VERMONT, }
COUNTY, SS. }

day of 186

I, _____ Treasurer of
the _____ Railroad
company, do solemnly swear the above is a true statement of the
condition of the finances of said Company, their Trustees, or
assignees or lessees, on the 30th day of June, 186

Treasurer.

Sworn before me,

Justice of the Peace.

TABLE K.

Does any conductor or engineman in the services of your road indulge in intoxication ?

Please answer the same question as applied to sectionmen, brakemen and switchmen.

Does your road or any of its operatives knowingly furnish facilities for traffic in intoxicating liquors in violation of the Prohibitory law of this state ?

Are the trains on your road allowed to stand across highway crossings to the unnecessary delay of the highway travel or business ?

Do your stationmen allow boys without business to frequent and remain in or about the stations, platforms, and trains ?

Are your road fences and cattle guards sufficiently constructed and supported, and kept in repair as the law demands ?

STATE OF VERMONT, }

FRANKLIN COUNTY, ss. }

St. Albans, Sept. 15, 1866.

We depose and say that the fact set forth, and statements made in the foregoing report which has been signed by us, are true and correct according to the best of our knowledge, information and belief.

Signed,

LAWRENCE BRAINERD,
JOSEPH CLARK,
J. GREGORY SMITH.

Subscribed and sworn before me this 15th day of September, 1866.

JULIAN H. DEWEY,
Master in Chancery.

ANNUAL REPORT

OF THE

VERMONT & CANADA R. R. COMPANY,

FOR THE

YEAR ENDING JUNE 30, 1866.

TABLE A.

STOCK AND DEBTS.

The capital of this Company, by compromise and decree of Court of Chancery, is fixed at \$2,000,000, exclusive of the cost of constructing the road from Swanton to Canada line in Highgate, for which stock has been issued for the sum of \$250,000, making the whole capital \$2,250,000. This Company has no debt of any magnitude.

TABLE B.

COST OF CONSTRUCTION.

Total cost of construction as per last report,.....	
Expended since last report, viz :.....	
For graduation and masonry,.....	
“ bridges, ..	
“ rails,	
“ chairs, spikes and ties,.....	
“ laying superstructure,.....	
“ buildings and fixtures,...	

The particulars called for under this head cannot be more fully stated than what has already been given in Table A. for reasons stated in last report. The information called for may perhaps be found in the report of the Managers and Receivers of the road of this Company, and of the Vermont Central Company.

TABLE C.

EQUIPMENT.

Total cost of equipment as per last report,.....

Expended since last report,.....

Total cost of road and equipment,.....

This Company have no equipment, as the road has been leased to the Vermont Central Company, and is operated by the Receivers under the lease. The Receivers were appointed by the Court of Chancery.

TABLE D.

CHARACTERISTICS OF ROAD.

Length of road,.....

“ “ completed,.. ..

“ “ branches,.....

“ “ side tracks,.....

Weight of rail per yard,.....

	No. of structures.	No. of spans.	Length of bridging in feet.
Trussle bridging,			
Trussle bridging, 50 feet span and under,			
Trussle bridging from 50 to 100 ft. span,			
Trussle bridging from 100 to 150 ft. span,			
Trussle bridging 150 ft. span and over,			
Draw bridges,			
Totals,			

Number of road crossings at grade,.....
 Number of road crossings above and below grade,
 Number of cross ties per mile,.....
 Chairs, number per mile,.....
 Whole number of switches on main track,.....

Reference is made to the report of the Managers and Receivers of the Central Road for the information called for by this table and all the subsequent tables except I. in part.

GRADIENTS AND ALIGNMENT.

Level, number of miles,.....
 Maximum grade,.. ..
 Amount of straight line, miles.....
 Amount of curved line, miles.....
 Maximum radius,.....
 Minimum radius,.....
 Sum of ascents going in one direction,.....
 Sum of ascents going in opposite direction,.....
 Height of termini and summit above tide water,..

BUILDINGS AND FIXTURES.

Passenger houses,.....
 Freight houses,.....
 Engine houses,.....
 Repair shops,.....
 Water stations,.. ..
 Dwellings,
 Wood sheds,.. ..
 Turn tables,.

Other buildings, as follows :

EQUIPMENT.

Number of locomotives owned by the Company on the 30th day of June, 1866.

	Under 16 tons.	16 to 20.	20 to 25.	25 to 30.	30 tons and over.
In good repair,					
Requiring slight repairs,					
Requiring heavy repairs,					
Worn out,					

Number of cars owned by the Company, June 30, 186

First class 8 wheel passenger cars in good repair,

First class 8 wheel passenger cars wanting repair,

Second class 8 wheel passenger cars in good repair,

Second class 8 wheel passenger cars wanting repair,

Baggage, express and mail cars in good repair, . .

Baggage, express and mail cars wanting repair, . .

Covered freight and cattle 8 wheel cars in good repair,

Covered freight and cattle 8 wheel cars wanting repair,

Platform 8 wheel cars in good repair,

Other freight cars,

Gravel cars,

Average weight of passenger cars,

Average weight of baggage cars,

Average weight of box cars,

Average weight of platform cars,

TABLE E.

BUSINESS OF THE YEAR.

Miles run by passenger trains,.....	
Miles run by freight trains,....	
Miles run by gravel and construction trains,.....	
Miles run by wood trains,.....	
Number of through passengers carried in cars,...	
Number of way passengers,.....	
Number of passengers having passes,.....	
Number of miles traveled by way passengers,....	
Average distance traveled by way passengers,....	
Number of miles traveled by passengers having passes,.....	
Number of tons of through freight,.....	
Number of tons of way freight,	
Number of tons of way freight carried 1 mile,...	
Number of tons of through freight moved towards market,.....	
Number of tons of through freight moved from market,.....	
Number of tons of way freight moved towards market,... ..	
Number of tons of way freight moved from market,	
Average rate of speed of ordinary passenger trains,	
Average rate of speed of express trains,... ..	
Average rate of speed of freight trains,.....	
Rate of fare charged 1st class through passengers per mile,...	
Rate of fare charged 1st class way passengers per mile,.....	
Average rate of fare charged 2d class passengers per mile,.....	
Rate per ton per mile charged on 1st class through freight,.....	
Rate per ton per mile charged on 2d class through freight,.. ..	

Rate per ton per mile charged on 3d class through freight,.....	
Rate per ton per mile charged on 4th class through freight,.....	
Rate per ton per mile charged on 1st class way freight,.....	
Rate per ton per mile charged on 2d class way freight,.....	
Rate per ton per mile charged on 3d class way freight,.....	
Rate per ton per mile charged on 4th class way freight,.....	
Total number of tons of freight carried 1 mile,...	
Total number of passengers carried 1 mile,.....	

TABLE F.

EXPENSES OF MAINTAINING ROADWAY AND REAL ESTATE,

For the year ending June 30, 186 .

Ordinary repairs of road bed and superstructure,.	
Extraordinary repairs of road bed,.....	
Cost of new rails used in repairs,...	
Number and weight of chairs,.....	
Weight of spikes,..	
Cost of repairs of rails,. ...	
Number of cross ties used for renewals,.....	
Cost of same,. ...	
Cost of relaying rails and ties,.....	
Insurance and taxes on real estate,.....	
Repairs of bridges,.....	
Repairs of stations,.....	
Repairs of fences,.....	
Repairs of masonry,.....	
Total,.....	

COST OF REPAIRS OF MACHINERY.

Repairs of engines and tenders,.....	...
Repairs of passenger and baggage cars,.....	...
Repairs of freight cars,.....	...
Repairs of tools and machinery in work-shops,...	...
Oil used about work-shops,.....	...
Fuel, .....	...
Waste,.....	...
Other items in detail as follows :	
Total,.....	...

TABLE G.

COST OF OPERATING THE ROAD.

For the year ending June 30, 1866.

Wood, including cost of preparing the same,...	...
Number of cords of wood used by locomotives,...	...
Number of tons of coal used by locomotives,....	...
Number of cords of wood used at stations,.....	...
Number of tons of coal used at stations,.....	...
Number of cords lost by fire,...	...
Number of gallons of oil,.....	...
Number of pounds of waste,.....	...
Cost of oil and waste for engines and tenders,...	...
Cost of oil and waste for passenger and baggage cars,...	...
Cost of oil and waste for freight cars,...	...
Loss and damage of goods, .....	...
Loss and damage of baggage, .....	...
Damages for injuries to persons,...	...
Damages to property, including fire and animals killed on road,.....	...
Agents at stations, &c.,.....	...
Clerks in general offices, &c.,.....	...
Labor loading and unloading freight,...	...
Porters and watchmen,.....	...
Switchmen, .....	...
Wood and water station attendance,.....	...

Conductors, baggagemen and brakemen,.....	.
Enginemmen and firemen,....	.....
For salaries of trustees, president, directors, secre-	
taries, treasurer and superintendent,..	....
For printing, stationery and offices expenses,....	
For law expenses,.....	.....
Other expenses in detail, as follows :	

Total,

RECAPITULATION OF EXPENSES.

Maintaining roadway,	
Repairs of machinery,.....	
Operating,.....	
Proportion of expenses due to passenger business,	
Proportion of expenses due to freight business,...	
Total,	

TABLE H.

EARNINGS, RECEIPTS AND PAYMENTS.

Earnings and Receipts.

From passengers,.....	
From freight,.....	
From other sources, viz :	
Expresses,.....	
Mails,.....	
Rents,.....	
Miscellaneous,.....	

Payments other than for Construction.

For transportation expenses, viz :	
For passenger business,.....	
For freight business,.....	
For other business, and what,.....	
For interest on funded debt,.....	
For interest on floating debt,.....	

For dividends,.....
 For carried to surplus fund,.....
 For amount of surplus fund,.. ..

VALUE OF MATERIALS ON HAND.

Wood, cords of,.....
 Coal, tons of,.....
 Oil, gallons of,.....
 Waste, pounds of,.....
 Iron rails, tons of, old,.....
 Iron rails, tons of, new,.. ..
 Chairs, pounds of,.....
 Spikes, pounds of,.....
 Ties, number of,.....
 Iron and other metals, unwrought,.....
 Iron and other metals, worked and partly worked,
 Lumber,

Other items specified as follows :

COST OF TRANSPORTATION.

Actual cost of transporting freight per ton, per mile,
 Actual cost of transporting passengers, per mile,.

DETAILS OF EARNINGS FOR THE YEAR ENDING JUNE 30, 1866.

SOURCE.	JULY.	AUGUST.	SEPTEMBER.	OCTOBER.	NOVEMBER.	DECEMBER.
Through passengers.....						
Way passengers.....						
Through freight.....						
Way freight.....						
Express.....						
Transport of mails.....						
Use of engines.....						
Use of cars.....						
Rent.....						
Other earnings specified in detail as follows :						
Total... ..						

DETAILS OF EARNINGS (CONTINUED.)

SOURCE.	JANUARY.	FEBRUARY.	MARCH.	APRIL.	MAY.	JUNE.
Through passengers,.....						
Way passengers,.....						
Through freight,.....						
Way freight,.....						
Express,.....						
Transport of mails,.....						
Use of engines,.....						
Use of cars,.....						
Rents,.....						
Other earnings specified in detail as follows:						
Total						

TABLE I.

ACCIDENTS.

State the number of persons injured in life or limb, and the cause thereof, and whether passenger or persons employed.

State whether any such accidents have arisen from carelessness or negligence of any person in the employ of the corporation, and whether such person is retained in the service of the corporation.

	EMPLOYEES.		OTHERS.	
	Killed.	Injured.	Killed.	Injured.
Trains thrown from track,				
Struck by bridge while on top of freight car,				
Run over while walking on track, .				
Injured at road crossing,				
Total,				

Total number of persons killed,

Total number of persons injured but not killed, ..

In addition to which must be given a statement of the date of each accident, the place where it occurred, the train, the cause and the extent of the injuries inflicted upon each person, and the name of such person.

TABLE J.

NUMBER OF EMPLOYEES AND COMPENSATION

Conductors of passenger trains.	
Amount of compensation,	
Conductors of freight trains.	
Amount of compensation,	
Conductors of wood and gravel trains.	
Amount of compensation,	
Master mechanics.	
Amount of compensation,	
Road masters.	
Amount of compensation,	
Men in repair shops.	
Amount of compensation,	
Enginemen of passenger trains.	
Amount of compensation,	
Enginemen of freight trains.	
Amount of compensation,	
Enginemen of wood and gravel trains.	
Amount of compensation,	
Firemen.	
Amount of compensation,	
Baggagemen.	
Amount of compensation,	
Switchmen.	
Amount of compensation,	
Section men, (foremen.)	
Amount of compensation,	
Section hands.	
Amount of compensation,	
Watchmen.	
Amount of compensation,	
Station agents.	
Amount of compensation,	
Other laborers.	
Amount of compensation,	

Clerks connected with passenger business.

Amount of compensation,

Clerks connected with freight business.

Amount of compensation,.....

Superintendent of bridges, (salary).....

Wood agent, (salary)

Other agents—how employed, and the salary of each, as follows :

OFFICERS OF THE COMPANY.

DIRECTORS.

LUCIUS B. PECK, *President.*

JOHN PORTER, *Vice-President.*

W. C. SMITH, *Treasurer and Clerk.*

EDWARD BLAKE, *Transfer Agent.*

JEDD P. CLARK,

BENJAMIN P. CHENEY,

S. H. ATHERTON.

SALARIES.

Trustees,	
President,	\$1,500
Superintendent,	
Treasurer,.....	1,500
Transfer Agent,.....	2,000

STATE OF VERMONT, }
 FRANKLIN COUNTY, ss. } 6th day of September, 1866.

I, W. C. Smith, Treasurer of the Vermont & Canada Railroad Company, do solemnly swear the above is a true statement of the condition of the finances of said Company, their Trustees, or assignees or lessees, on the 30th day of June, 186

W. C. SMITH,
Treasurer.

Sworn before me,

ABNER FORBES,
Master in Chancery.

TABLE K.

Does any conductor or engineman in the services of your road indulge in intoxication ?

Please answer the same question as applied to sectionmen, brakemen and switchmen.

Does your road or any of its operatives knowingly furnish facilities for traffic in intoxicating liquors in violation of the Prohibitory law of this state ?

Are the trains on your road allowed to stand across highway crossings to the unnecessary delay of the highway travel or business ?

Do your stationmen allow boys without business to frequent and remain in or about the stations, platforms, and trains ?

Are your road fences and cattle guards sufficiently constructed and supported, and kept in repair as the law demands ?

Do your stationmen allow boys without business to frequent and remain in or about the stations, platforms, and trains ?

Are your road fences and cattle guards sufficiently constructed and supported, and kept in repair as the law demands ?

STATE OF VERMONT, }
WASHINGTON COUNTY, ss. } Montpelier, Aug. 27, 1866.

I, Lucius B. Peck, depose and say that the facts set forth, and statements made in the foregoing report which has been signed by me, are true and correct according to the best of my knowledge, information and belief.

Signed,

LUCIUS B. PECK,

President of said Company.

Subscribed and sworn before me this 27th day of August, 1866.

B. F. FIFIELD,

Master in Chancery.

ANNUAL REPORT

OF THE

ATLANTIC & ST. LAWRENCE R. R. CO.,

FOR THE

YEAR ENDING JUNE 30, 1866.

TABLE A.

STOCK AND DEBTS.

Capital Stock authorized by Charter,.....	\$4,000,000
The amount paid in is.....	2,494,900
Funded Debt,.... .	3,472,000
Floating Debt,.....	

Interest at rate of six per cent.

The coupons are paid by the Lessees.

TABLE B.

COST OF CONSTRUCTION.

Total cost of construction as per last report,.....	\$6,788,354 72
Expended since last report, viz :.....	
For graduation and masonry,.....	
“ bridges, ..	
“ rails,	
“ chairs, spikes and ties,.....	
“ laying superstructure,.....	
“ buildings and fixtures,...	

Two thirds of the Road were built at a stipulated price per mile—consequently cannot be apportioned.

TABLE C.

EQUIPMENT.

Total cost of equipment as per last report,.....	\$865,734 76
Expended since last report,	
Total cost of road and equipment,.....	\$7,654,089 48

TABLE D.

CHARACTERISTICS OF ROAD.

Length of road within State of Vermont,.....	30½ miles
“ “ completed, “	30½ “
“ “ branches, “	none
“ “ side tracks,.. “	1 1-5 miles
Weight of rail per yard,.....	63 lbs.

CHARACTER AND LENGTH OF BRIDGING.

	No. of structures.	No. of spans.	Length of bridging in feet.
Pile bridging,	1		1519
Trustle bridging, 50 feet span and under,	10	10	400
Trustle bridging from 50 to 100 ft. span,	2	2	120
Trustle bridging from 100 to 150 ft. span,	14	14	1750
Trustle bridging 150 ft. span and over,			
Stone arch bridges,	2	2	120
Totals,	29	28	3909

Number of road crossings at grade,.....	63
Number of road crossings above and below grade,	6
Number of cross ties per mile,.....	2400
Chairs, number per mile,.....	about 600
Whole number of switches on main track,.....	about 100

NOTE.—In addition to the above bridges there are on the line 16 iron bridges, three of which are 300 feet each in length, in spans of 75 feet ; one over the Connecticut River, one over Wild River and the other over Presumpscot River—whole length of above named bridges, 1,510 feet.

GRADIENTS AND ALIGNMENT.

[illegible]

The Charter of the Atlantic and St. Lawrence Railroad within the State of Vermont extends from the Connecticut River in Bloomfield to the boundary line in Norton. But the entire line from Portland to Montreal is worked in divisions, of which the point of junction is at Island Pond, in Brighton.

BUILDINGS AND FIXTURES.

Passenger houses,.....	29
Freight houses,.....	22
Engine houses,.....	8
Repair shops,.....	6
Water stations,..	17
Dwellings,	2
Wood sheds,..	26
Turn tables,..	10

Other buildings, as follows :

Two Hotels,

Ten store houses on wharves in Portland for Ocean and Boston steamers.

EQUIPMENT.

Number of locomotives used by the Company on the 30th day of June, 1866, on the Portland line.

	Under 16 tons.	16 to 20.	20 to 25.	25 to 30.	30 tons and over.
In good repair,			7	19	7
Requiring slight repairs,			2	8	
Requiring heavy repairs,				2	
Worn out,					

Number of cars appropriated by the Company June 30, 1866, to the Portland line,.....	565
First class 8 wheel passenger cars in good repair,	16
First class 8 wheel passenger cars wanting repair,	1
Second class 8 wheel passenger cars in good repair,.....	
Second class 8 wheel passenger cars wanting repair,.....	
Baggage, express and mail cars in good repair,..	7
Baggage, express and mail cars wanting repair,..	
Covered freight and cattle 8 wheel cars in good repair,.....	
Covered freight and cattle 8 wheel cars wanting repair,.....	20
Platform 8 wheel cars in good repair,.....	541
Other freight cars,.....	
Gravel cars,	
Average weight of passenger cars,.....	
Average weight of baggage cars,.....	
Average weight of box cars,.....	
Average weight of platform cars,.....	

TABLE E.

BUSINESS OF THE YEAR ENDING DEC. 31, 1865.

Miles run by passenger trains,.....	146,396
Miles run by freight trains,....	567,540
Miles run by gravel and construction trains,.....	} 164,937
Miles run by wood trains,.....	
Number of through passengers carried in cars,...	} 200,505
Number of way passengers,.....	
Number of passengers having passes,.....	
Number of miles traveled by way passengers,....	
Average distance traveled by way passengers,....	
Number of miles traveled by passengers having passes,.....	
Number of tons of through freight,.....	} 376,931
Number of tons of way freight,	
Number of tons of way freight carried 1 mile,...	
Number of tons of through freight moved towards market,.....	
Number of tons of through freight moved from market,.....	
Number of tons of way freight moved towards market,...	
Number of tons of way freight moved from market,	
Average rate of speed of ordinary passenger trains, per hour,.....	19 miles
Average rate of speed of express trains,...	
Average rate of speed of freight trains, per hour,.	11 miles
Rate of fare charged 1st class through passengers per mile,...	2 $\frac{3}{4}$ cents
Rate of fare charged 1st class way passengers per mile,.....	3 cents
Average rate of fare charged 2d class passengers per mile,.....	
Rate per ton per mile charged on 1st class through freight,.....	
Rate per ton per mile charged on 2d class through freight,..	

Rate per ton per mile charged on 3d class through freight,.....	
Rate per ton per mile charged on 4th class through freight,.....	
Rate per ton per mile charged on 1st class way freight,.....	
Rate per ton per mile charged on 2d class way freight,.....	
Rate per ton per mile charged on 3d class way freight,.....	
Rate per ton per mile charged on 4th class way freight,.....	
Total number of tons of freight carried 1 mile,...	
Total number of passengers carried 1 mile,.....	

TABLE F.

EXPENSES OF MAINTAINING ROADWAY AND REAL ESTATE,

For the year ending June 30, 186 .

Ordinary repairs of road bed and superstructure,.	
Extraordinary repairs of road bed,.....	
Cost of new rails used in repairs,... . . .	
Number and weight of chairs,...	
Weight of spikes,..	
Cost of repairs of rails,.	
Number of cross ties used for renewals,.....	
Cost of same,.	
Cost of relaying rails and ties,.....	
Insurance and taxes on real estate,.....	
Repairs of bridges,.....	
Repairs of stations,.....	
Repairs of fences,.....	
Repairs of masonry,.....	
Total,.....	

Books so kept cannot be answered in detail.

COST OF REPAIRS OF MACHINERY.

Repairs of engines and tenders,.....	...
Repairs of passenger and baggage cars,.....	...
Repairs of freight cars,.....	...
Repairs of tools and machinery in work-shops,...	...
Oil used about work-shops,.....	...
Fuel, ..	.....
Waste,.....	.....
Other items in detail as follows :	
Total,.....	.....
Books so kept cannot be answered in detail.	

TABLE G.

COST OF OPERATING THE ROAD.

For the year ending Dec. 31, 1865.

Wood, including cost of preparing the same,..	..
Number of cords of wood used by locomotives,..	...
Number of tons of coal used by locomotives,....	...
Number of cords of wood used at stations,.....	...
Number of tons of coal used at stations,.....	...
Number of cords lost by fire,..	.....
Number of gallons of oil,.....	...
Number of pounds of waste,.....	.....
Cost of oil and waste for engines and tenders,..	...
Cost of oil and waste for passenger and baggage cars,...	.....
Cost of oil and waste for freight cars,..	...
Loss and damage of goods, ..	.....
Loss and damage of baggage, ..	...
Damages for injuries to persons,..	.....
Damages to property, including fire and animals killed on road,.....	.....
Agents at stations, &c.,.....	.....
Clerks in general offices, &c.,.....	.....
Labor loading and unloading freight,...	.....
Porters and watchmen,.....	.....
Switchmen, ..	.....
Wood and water station attendance,.....	...

Conductors, baggagemen and brakemen,.....
 Enginemen and firemen,... ..
 For salaries of trustees, president, directors, secretaries, treasurer and superintendent,.. ..
 For printing, stationery and offices expenses,....
 For law expenses,.....
 Other expenses in detail, as follows :

Total,

Books not classified to conform to above table, answers cannot be given.

RECAPITULATION OF EXPENSES.

Maintaining roadway,
 Repairs of machinery,.....
 Operating,.....
 Proportion of expenses due to passenger business,
 Proportion of expenses due to freight business,...

Total, year ending Dec. 31, 1865,..... \$938.239 66

TABLE H.

EARNINGS, RECEIPTS AND PAYMENTS.

Earnings and Receipts year ending Dec. 31, 1865.

From passengers,.....	\$254,953 56
From freight,.....	22,376 23
From other sources, viz :	
Expresses,.....	} 29,916 58
Mails,.....	
Rents,.....	} 8,866 44
Miscellaneous,.....	

Payments other than for Construction.

For transportation expenses, viz :

For passenger business,.....
 For freight business,.....
 For other business, and what,.....

For interest on funded debt,	
For interest on floating debt,	
For dividends 1st January and 1st July. at rate of per cent. less Government tax,	
For carried to surplus fund,	
For amount of surplus fund,	

VALUE OF MATERIALS ON HAND.

Wood, cords of,	
Coal, tons of,	
Oil, gallons of,	
Waste, pounds of,	
Iron rails, tons of, old,	
Iron rails, tons of, new,	
Chairs, pounds of,	
Spikes, pounds of,	
Ties, number of,	
Iron and other metals, unwrought,	
Iron and other metals, worked and partly worked,	
Lumber,	

Other items specified as follows :

COST OF TRANSPORTATION.

Actual cost of transporting freight per ton, per mile,
Actual cost of transporting passengers, per mile, . .

The statements of Receipts and Expenses apply to the Portland division, extending from Island Pond to Portland.

DETAILS OF EARNINGS FOR THE YEAR ENDING JUNE 30, 1866.

SOURCE.	JULY.	AUGUST.	SEPTEMBER.	OCTOBER.	NOVEMBER.	DECEMBER.
Through passengers.....						
Way passengers.....						
Through freight.....						
Way freight.....						
Express.....						
Transport of mails.....						
Use of engines.....						
Use of cars.....						
Rent.. ..						
Other earnings specified in detail as follows :						
Total... ..						

Cannot be answered in detail.

DETAILS OF EARNINGS (CONTINUED.)

SOURCE.	JANUARY.	FEBRUARY.	MARCH.	APRIL.	MAY.	JUNE.
Through passengers,.....						
Way passengers,.....						
Through freight,.....						
Way freight,.....						
Express,.....						
Transport of mails,.....						
Use of engines,.....						
Use of cars,.....						
Rents,.....						
Other earnings specified in detail as follows:						
Total						

Cannot be answered in detail.

TABLE I.

ACCIDENTS.

State the number of persons injured in life or limb, and the cause thereof, and whether passenger or persons employed.

State whether any such accidents have arisen from carelessness or negligence of any person in the employ of the corporation, and whether such person is retained in the service of the corporation.

	EMPLOYEES.		OTHERS.	
	Killed.	Injured.	Killed.	Injured.
Trains thrown from track, . . .				
Struck by bridge while on top of freight car,				
Run over while walking on track, .				
Injured at road crossing,				
Total,				

None killed or injured in Vermont.

Total number of persons killed,

Total number of persons injured but not killed, . .

In addition to which must be given a statement of the date of each accident, the place where it occurred, the train, the cause and the extent of the injuries inflicted upon each person, and the name of such person.

Ordin Poland, employee of Company, was injured at Island Pond, January 17th, 1865, by slipping off engine—had foot crushed.

H. Punthton, employee of Company, had arm crushed at Island Pond, December 7th, 1865—slipped while coupling cars.

TABLE J.

NUMBER OF EMPLOYEES AND COMPENSATION

Conductors of passenger trains.	
Amount of compensation,.....	
Conductors of freight trains.	
Amount of compensation,.....	
Conductors of wood and gravel trains.	
Amount of compensation,.....	
Master mechanics.	
Amount of compensation.....	
Road masters.	
Amount of compensation,.....	
Men in repair shops.	
Amount of compensation,.	
Enginemen of passenger trains.	
Amount of compensation,.. . . .	
Enginemen of freight trains.	
Amount of compensation,.....	
Enginemen of wood and gravel trains.	
Amount of compensation,.....	
Firemen.	
Amount of compensation,.... .	
Baggagemen.	
Amount of compensation,.....	
Switchmen.	
Amount of compensation,.....	
Section men, (foremen.)	
Amount of compensation,.....	
Section hands.	
Amount of compensation,	
Watchmen.	
Amount of compensation,.....	
Station agents.	
Amount of compensation,.....	
Other laborers.	
Amount of compensation,.....	

Clerks connected with passenger business.

Amount of compensation,

Clerks connected with freight business.

Amount of compensation,

Superintendent of bridges, (salary)

Wood agent, (salary)

Other agents—how employed, and the salary of each, as follows :

The employees of the Company, being employed upon the two divisions of the road, one south of Island Pond extending to Portland, the other north of Island Pond extending to the boundary line and Montreal, no specific statement can be made under the head of inquiry, as applied to the state of Vermont.

Lessees, Grand Trunk Railway Co. of Canada.

OFFICERS OF THE COMPANY.

ST. JOHN SMITH, *President.*

HENRY BAILEY, *Superintendent.*

CHAS. E. BARRETT, *Treasurer.*

SALARIES.

Trustees,

President,

Superintendent,

Treasurer,

Transfer Agent,

The earnings of the Road are received by the Lessees. The Lessors have no interest in them while the rent is paid, and the accounts are so kept by the Grand Trunk Railway Company as not to admit of an answer to the above as contemplated.

STATE OF MAINE,	}	21st day of August, 1866.
CUMBERLAND COUNTY, SS.		

I, Chas. E. Barrett, Treasurer of the Atlantic & St. Lawrence Railroad Company, do solemnly swear the above is a true statement of the condition of the finances of said Company, their Trustees, or assignees or lessees, on the 30th day of June, 1866.

CHAS. E. BARRETT,

Treasurer.

Sworn before me,

H. ILSLEY,

Justice of the Peace.

TABLE K.

Does any conductor or engineman in the services of your road indulge in intoxication?

Not to our knowledge.

Please answer the same question as applied to sectionmen, brakemen and switchmen.

There is a rule of the Company subjecting an employee to discharge for intoxication.

Does your road or any of its operatives knowingly furnish facilities for traffic in intoxicating liquors in violation of the Prohibitory law of this state?

Not to our knowledge.

Are the trains on your road allowed to stand across highway crossings to the unnecessary delay of the highway travel or business?

No.

Do your stationmen allow boys without business to frequent and remain in or about the stations, platforms, and trains?

Not allowed to our knowledge. There is but one station in Vermont which is much frequented by any persons.

Are your road fences and cattle guards sufficiently constructed and supported, and kept in repair as the law demands?

The fences are believed to be in usual good condition. Much the largest part of the line in Vermont is through forest land.

STATE OF MAINE,	}	Portland, August, 1866.
CUMBERLAND COUNTY, SS.		

The subscribers, a majority of the Directors of the Atlantic and St. Lawrence Railroad Company depose and say that the facts set forth, and statements made in the foregoing report which has been signed by us, are true and correct according to the best of our knowledge, information and belief.

Signed,

P. BARNES,
J. B. BROWN,
CHAS. E. BARRETT,
J. L. FARMER,
H. J. LIBBY,
BYRON GREENOUGH.

Subscribed and sworn before me this 21st day of August, 1866.

H. ILSLEY,
Justice of the Peace.

ANNUAL REPORT

OF THE

VERMONT & MASSACHUSETTS R. R. CO.,

FOR THE

YEAR ENDING JUNE 30, 1866.

TABLE A.

STOCK AND DEBTS.

Capital Stock including the Greenfield Branch and

Vermont portion,..... \$3,200,000 00

Total amount of capital stock paid in..... 2,860,000 00

Number of shares of capital stock issued,..... 28,600

There is no debt excepting the funded debt, which

amounts to..... \$551,700 00

The amount of interest paid each year is six per cent. on the funded debt. Interest coupons have always been promptly paid when due.

TABLE B.

COST OF CONSTRUCTION.

Total cost of construction as per last report,.....\$3,287,960 41

Expended since last report, viz :.....

For graduation and masonry,.....

“ bridges,

“ rails,

“ chairs, spikes and ties,.....

“ laying superstructure,.....

“ buildings and fixtures,... ..

Reduction of construction acc.t since last report, 47,181 10

Total cost of construction,.....-\$3,240,779 31

TABLE C.

EQUIPMENT.

Total cost of equipment as per last report,.....	\$225,650 20
Expended since last report,	
Total cost of road and equipment,.....	\$3,466,429 51

TABLE D.

CHARACTERISTICS OF ROAD.

Length of Road,	69 miles
“ “ completed, “	69 “
“ “ branches, “	8 miles
“ “ side tracks,	5½ miles
Weight of rail per yard,	56 lbs.
Within this State 10 miles and 1661 ft.; Side tracks, 1 mile.	

CHARACTER AND LENGTH OF BRIDGING.

	No. of structures.	No. of spans.	Length of bridging in feet.
Trustle bridging,			
Trustle bridging, 50 feet span and under,	11	11	
Trustle bridging from 50 to 100 ft. span,	5	5	
Trustle bridging from 100 to 150 ft. span,	3	4	
Trustle bridging 150 ft. span and over,	23	41	
Stone arch bridges,	none.		
Totals,	42	28	

Number of road crossings at grade,	62
Number of road crossings above and below grade,	21
Number of cross ties per mile,	2350
Chairs, number per mile,	530
Whole number of switches on main track,	60

GRADIENTS AND ALIGNMENT.

Level, number of miles,	
Maximum grade,..	58 feet
Amount of straight line, miles.....	31 13-20 “
Amount of curved line, miles.....	45 7-10 “
Maximum radius,.....	
Minimum radius,.....	
Sum of ascents going in one direction,.....	
Sum of ascents going in opposite direction,.....	
Height of termini and summit above tide water,..	

BUILDINGS AND FIXTURES.

Passenger houses,.....	19
Freight houses,.....	20
Engine houses,.....	5
Repair shops,.....	4
Water stations,..	9
Dwellings,	8
Wood sheds,..	19
Turn tables,.	5
Other buildings, as follows :	
General Office at Fitchburg,..	1

EQUIPMENT.

Number of locomotives used by the Company on the 30th day of June, 1866, eleven.

	Under 16 tons.	16 to 20.	20 to 25.	25 to 30.	30 tons and over.
In good repair,			9		
Requiring slight repairs,			2		
Requiring heavy repairs,					
Worn out,					

Number of cars owned by the Company June 30,
1866,

First class 8 wheel passenger cars in good repair,	7
First class 8 wheel passenger cars wanting repair,	1
Second class 8 wheel passenger cars in good repair,.....	none
Second class 8 wheel passenger cars wanting repair,.....	none
Baggage, express and mail cars in good repair,..	5
Baggage, express and mail cars wanting repair,..	none
Covered freight and cattle 8 wheel cars in good repair,.....	84
Covered freight and cattle 8 wheel cars wanting repair,.....	3
Platform 8 wheel cars in good repair,	86
Other freight cars,.....	7
Gravel cars,	none
Average weight of passenger cars,.....	12½ tons
Average weight of baggage cars,.....	11½ "
Average weight of box cars,.....	8 "
Average weight of platform cars,.....	6½ "

TABLE E.

BUSINESS OF THE YEAR ENDING NOV. 30, 1865.

Miles run by passenger trains,.....	102,769
Miles run by freight trains,....	60,952
Miles run by gravel and construction trains,....	1,133
Miles run by wood trains,.....	
Number of through passengers carried in cars,...	103,777
Number of way passengers,.....	81,970
Number of passengers having passes,.....	1,204
Number of miles traveled by way passengers,....	1,339,992
Average distance traveled by way passengers,....	16 miles
Number of miles traveled by passengers having passes,.....	30,388
Number of tons of through freight,.....	83,105 ⁷¹⁰ / ₁₀₀₀
Number of tons of way freight,	29,405 ⁵⁰³ / ₁₀₀₀
Number of tons of way freight carried 1 mile,...	321,018 ⁹¹⁴ / ₁₀₀₀
Number of tons of through freight moved towards market,.....	58,507 ⁷³¹ / ₁₀₀₀
Number of tons of through freight moved from market,.....	24,597 ⁸⁷⁹ / ₁₀₀₀
Number of tons of way freight moved towards market,...	29,405 ⁵⁰³ / ₁₀₀₀
Number of tons of way freight moved from market,	
Average rate of speed of ordinary passenger trains, per hour,.....	22 miles
Average rate of speed of express trains,...	
Average rate of speed of freight trains, per hour,.	10 miles
Rate of fare charged 1st class through passengers per mile,...	4 cents nearly
Rate of fare charged 1st class way passengers per mile,.....	4 cents
Average rate of fare charged 2d class passengers per mile,.....	
Rate per ton per mile charged on 1st class through freight,.....	7cents
Rate per ton per mile charged on 2d class through freight,..	6 cents

Rate per ton per mile charged on 3d class through freight,.....	5 cents
Rate per ton per mile charged on 4th class through freight,.....	4 cents
Rate per ton per mile charged on 1st class way freight,.....	15 cents
Rate per ton per mile charged on 2d class way freight,.....	13 cents
Rate per ton per mile charged on 3d class way freight,.....	11 cents
Rate per ton per mile charged on 4th class way freight,.....	8 cents
Total number of tons of freight carried 1 mile,...	2,193,348 ⁹¹⁵ / ₁₀₀₀
Total number of passengers carried 1 mile,.....	3,414,777

TABLE F.

EXPENSES OF MAINTAINING ROADWAY AND REAL ESTATE,

For the year ending June 30, 1866.

Ordinary repairs of road bed and superstructure,.	\$45,546 62
Extraordinary repairs of road bed,.....	
Cost of new rails used in repairs,...	23,040 66
Number and weight of chairs,...	} Included in above.
Weight of spikes,..	
Cost of repairs of rails,...	
Number of cross ties used for renewals,.....	
Cost of same,.....	
Cost of relaying rails and ties,.....	
Insurance and taxes on real estate,.....	4,392 63
Repairs of bridges,.....	3,385 13
Repairs of stations,.....	22,065 44
Repairs of fences,.....	547 23
Repairs of masonry,.....	
Total,.....	\$98,977 71

COST OF REPAIRS OF MACHINERY.

Repairs of engines and tenders,.....	\$15,250 25	
Repairs of passenger and baggage cars,.....	9,326 89	
Repairs of freight cars,.....	13,803 49	
Repairs of tools and machinery in work-shops,...	910 99	
Oil used about work-shops,.....	} Stated elsewhere.	
Fuel,		
Waste,.....		
Other items in detail as follows :		
Repairs of gravel and hand cars,.....	308 02	
Total,.....	<u>\$39,599 64</u>	

TABLE G.

COST OF OPERATING THE ROAD.

For the year ending June 30, 1866.

Wood, including cost of preparing the same,.. .	\$26,336 18
Number of cords of wood used by locomotives,...	
Number of tons of coal used by locomotives,....	
Number of cords of wood used at stations,.....	
Number of tons of coal used at stations,.....	
Number of cords lost by fire,.....	
Number of gallons of oil,.....	
Number of pounds of waste,.....	
Cost of oil and waste for engines and tenders,..	} 5,291 38
Cost of oil and waste for passenger and baggage cars,...	
Cost of oil and waste for freight cars,.. .	
Loss and damage of goods,.....	} 4,250 60
Loss and damage of baggage,	
Damages for injuries to persons,.....	
Damages to property, including fire and animals killed on road,.....	
Agents at stations, &c., included in expenses....	}
Clerks in general offices, &c., charged freight and passenger,.....	
Labor loading and unloading freight departments,	

Porters and watchmen,.....	}	4,914 16
Switchmen,.....	}	
Wood and water station expenses,..		1,331 60
Conductors and baggagemen, included in expenses,	}	
Brakemen, charged freight and passenger,.....		
Enginemmen and firemen departments,.....		
For salaries of trustees, president, directors, secre-		
taries, treasurer and superintendent,..		7,558 33
For printing, stationery and advertising,.....		824 98
For law expenses,.....		712 10
Other expenses in detail, as follows :		
Clearing snow,.....		171 33
Expenses of passenger department,..		20,160 37
Expenses of freight department,		24,606 09
Rent to Connecticut River Railroad company,.....		1,500 00
Incidental expenses,.....		5,240 36
United States and State taxes,.....		25,422 48
Total, ..		<u>\$128,319 96</u>

RECAPITULATION OF EXPENSES.

Maintaining roadway,	98,977 71
Repairs of machinery,.....	39,599 64
Operating,.....	128,319 96
Proportion of expenses due to passenger business,	
Proportion of expenses due to freight business,...	
Total,.....	<u>\$266,897 31</u>

TABLE H.

EARNINGS, RECEIPTS AND PAYMENTS.

Earnings and Receipts.

From passengers,.....	\$135,211 56
From freight,.....	217,135 98
From other sources, viz: tolls,.....	1,017 05
Expresses,.....	5,045 75
Mails,.....	6,000 00

Rents,.....	42,983 16
Miscellaneous,.....	428 31
Total..	<u>\$407,821 81</u>

Payments other than for Construction.

For transportation expenses, viz :

For passenger business,.....	}	\$266,897 31
For freight business,.....		
For other business, and what,.....		
For interest on funded debt,.....		20,002 63
For interest on floating debt,.....		
For dividends No. 1 and 2, Vt. and U. S. tax,..		60,210 52
For carried to surplus fund,.....		
For amount of surplus fund,..		

VALUE OF MATERIALS ON HAND.

Wood, costs of,.....		\$33,356 28
Coal, tons of,.....		
Oil, gallons of,.....	}	925 40
Waste, pounds of,.....		
Iron rails, tons of, old, (300).....		\$40 per ton.
Iron rails, tons of, new,...		
Chairs, pounds of,.....		
Spikes, pounds of,.....		
Ties, number of,.....		
Iron and other metals, unwrought,.....	}	24,341 44
Iron and other metals, worked and partly worked,		
Lumber,		

Other items specified as follows :

COST OF TRANSPORTATION.

Actual cost of transporting freight per ton, per mile,
 Actual cost of transporting passengers, per mile,..

DETAILS OF EARNINGS FOR THE YEAR ENDING JUNE 30, 1866.

SOURCE.	JULY.	AUGUST.	SEPTEMBER.	OCTOBER.	NOVEMBER.	DECEMBER.
Through passengers.....	15,055 54	17,375 14	15,478 44	12,522 43	9,945 00	9,445 90
Way passengers.....						
Through freight.....	12,836 40	16,609 13	17,416 23	17,808 29	19,101 46	16,104 61
Way freight.....						
Express.....	377 45	377 45	377 45	377 45	449 09	509 65
Transport of mails.....	500 00	500 00	500 00	500 00	500 00	500 00
Tolls.....	61 75	120 30	82 50	101 00	101 50	81 00
Miscellaneous,.....	23 13	11 20	36 90	54 71	29 85	12 66
Rent,.....	3,687 98	3,250 00	3,759 02	3,677 50	3,262 50	3,780 00
Other earnings specified in detail as follows :						
Total....	\$32,542 25	38,243 22	37,650 54	35,041 38	33,389 40	30,433 82

DETAILS OF EARNINGS (CONTINUED.)

SOURCE.	JANUARY.	FEBRUARY.	MARCH.	APRIL.	MAY.	JUNE.
Through passengers,..... }	9,426 94	7,315 17	8,926 77	9,629 18	9,100 49	10,990 56
Way passengers,..... }						
Through freight,..... }	18,517 22	16,657 74	19,583 78	20,921 65	21,661 57	19,917 90
Way freight,..... }						
Express,.....	348 95	305 37	353 33	529 78	502 13	537 65
Transport of mails,...	500 00	500 00	500 00	500 00	500 00	500 00
Tolls,.....	83 75	65 25	67 00	103 00	90 00	60 00
Miscellaneous,.....	116 86	29 95	38 81	7 09	25 12	42 03
Rents,.....	3,841 50	3,253 00	3,785 25	3,658 91	3,262 50	3,765 00
Other earnings specified in detail as follows :						
Total.....	32,835 22	28,126 48	33,254 94	35,349 61	35,141 81	35,813 14

TABLE I.

ACCIDENTS.

State the number of persons injured in life or limb, and the cause thereof, and whether passenger or persons employed.

State whether any such accidents have arisen from carelessness or negligence of any person in the employ of the corporation, and whether such person is retained in the service of the corporation.

	EMPLOYEES.		OTHERS.	
	Killed.	Injured.	Killed.	Injured.
Trains thrown from track,				
Struck by bridge while on top of freight car,				
Run over while walking on track, .				
Injured at road crossing,				
Total,				

Total number of persons killed,

Total number of persons injured but not killed, . .

In addition to which must be given a statement of the date of each accident, the place where it occurred, the train, the cause and the extent of the injuries inflicted upon each person, and the name of such person.

NOVEMBER 3, 1865.—Bartholomew Kennedy in jumping from passenger train approaching Greenfield, fell and his foot was crushed by the train passing over it. The Company was in no way responsible for the accident.

APRIL 2, 1866.—Elijah White, attempting to cross the track at Templeton Station in front of an approaching engine was struck and seriously injured.

TABLE J.

NUMBER OF EMPLOYEES AND COMPENSATION PER JUNE ROLL,
1866.

4 Conductors of passenger trains.	
Amount of compensation, per month,	\$50 to \$75
2 Conductors of freight trains.	
Amount of compensation, per month,	\$50 to \$75
Conductors of wood and gravel trains.	
Amount of compensation,	
1 Master mechanic.	
Amount of compensation per month,	\$115
Road masters.	
Amount of compensation, per day,	\$.150 to \$2.87
40 Men in repair shops.	
Amount of compensation,	
5 Enginemen of passenger trains.	
Amount of compensation, per month,	\$60 to \$70
3 Enginemen of freight trains.	
Amount of compensation, per month,	\$55 to \$70
Enginemen of wood and gravel trains.	
Amount of compensation,	
8 Firemen.	
Amount of compensation, per month,	\$45
5 Baggage-men.	
Amount of compensation, per month,	\$45 to \$60
4 Switchmen.	
Amount of compensation, per month,	\$40 to \$45
7 Section men, (foremen.)	
Amount of compensation, per day,	\$1.75 to \$2.25
60 Section hands.	
Amount of compensation, per day,	\$1.50 to \$1.60
6 Watchmen.	
Amount of compensation, per day,	\$1.50 to \$2.00
20 Station agents.	
Amount of compensation, per month,	\$15 to \$75
3 Other laborers.	
Amount of compensation, per day,	\$1.50

1 Clerk connected with passenger business.

Amount of compensation, per month, \$70

2 Clerks connected with freight business.

Amount of compensation, per month,..... \$65 and \$110

Superintendent of bridges, Road and Wood Agent

(one person)—salary, per month,..... \$150

Other agents—how employed, and the salary of each, as follows :

1 Master Carpenter, per month,.... . \$70

7 Bridge and Building Carpenters, per day,... \$2 to \$2.75

1 Porter, per day,..... \$1.55

2 Flagmen, per month,..... \$30 and \$37

5 Station Agents laborers, per month,..... \$40

3 Telegraph Operators, per month,..... \$10 and \$12.50

3 Freight Receiptors, per month,..... \$12.50 to \$65

2 Brakemen on passenger trains, per month,.. \$40

5 Brakemen on freight trains, per month,.... \$40

1 Toll collector, per month,..... \$20

Trustees—J. C. Howe, William Minot, Jr., James McGregor.

OFFICERS OF THE COMPANY.

DANIEL S. RICHARDSON, *President*.

OTTIS T. RUGGLES, *Superintendent*.

FRANKLIN N. POOR, *Treasurer*.

B. D. LOCKE, *Clerk*.

SALARIES.

Trustees, per annum, \$300, \$100, \$100

President, per annum,..... 1,500

Superintendent, per annum,..... 2,500

Treasurer, per annum,.... 2,000

Clerk, per annum,..... 50

Amount of net earnings for one year ending June	
30, 1865,.....	\$106,077 13
Amount of net earnings for one year ending June	
30, 1866,.....	140,924 50
Amount of Funded Debt June 30, 1866,.....	551,700 00
Amount of Cash in Treasury,.....	\$17,883 01
Amount of Loans,.....	66,116 67
\$22,000 Vt. & Mass. R.R. Co. Bonds, 20,555 17	
	<hr/>
	\$104,554 85

Amount of interest paid during 12 months ending
June 30, 1866, more than received for loans, \$20,002 63

COMMONWEALTH OF MASS. }

SUFFOLK COUNTY, ss.

28th day of August, 1866.

I, Franklin N. Poor, Treasurer of the Vermont & Massachusetts Railroad Company, do solemnly swear the above is a true statement of the condition of the finances of said Company, their Trustees, or assignees or lessees, on the 30th day of June, 1866.

FRANKLIN N. POOR,

Treasurer.

Sworn before me,

P. E. TESCHEMACHER,

Justice of the Peace.

TABLE K.

Does any conductor or engineman in the services of your road indulge in intoxication?

No.

Please answer the same question as applied to sectionmen, brakemen and switchmen.

Does your road or any of its operatives knowingly furnish facilities for traffic in intoxicating liquors in violation of the Prohibitory law of this state?

No.

Are the trains on your road allowed to stand across highway crossings to the unnecessary delay of the highway travel or business ?

No.

Do your stationmen allow boys without business to frequent and remain in or about the stations, platforms, and trains ?

No.

Are your road fences and cattle guards sufficiently constructed and supported, and kept in repair as the law demands ?

Yes.

COMMONWEALTH OF MASS. }

SUFFOLK COUNTY, ss. }

We depose and say that the facts set forth, and statements made in the foregoing report which has been signed by us, are true and correct according to the best of our knowledge, information and belief.

Signed,

DANIEL S. RICHARDSON,	} <i>Directors of the</i>
WILLIAM H. HILL,	
JAMES A. DUPEE,	
F. GOODHUE,	
FRANKLIN N. POOR,	} <i>Vt. & Mass.</i>
	} <i>R. R. Co.</i>

Subscribed and sworn before me this 28th day of August,
A. D., 1866.

P. E. TESCHEMACHER,
Justice of the Peace.

ANNUAL REPORT

OF THE

CONN. & PASSUMPSIC RIVERS R. R. CO.,

FOR THE

YEAR ENDING MAY 31, 1866.

TABLE A.

STOCK AND DEBTS.

Capital Stock as per charter,.....	\$3,000,000 00
Six per cent preferred Stock,.....	1,673,300 00
Stock not preferred,.....	163,300 00
Guaranteed Stock,.....	900 00
Notes payable,.....	130,700 00
Bonded Debt,.....	604,700 00
Average rate of interest on bonded debt, paid semi-annually,.....	6 per cent
Coupons uncalled for,..	2,151 00

TABLE B.

COST OF CONSTRUCTION.

Total cost of construction as per last report,.....	\$2,842,741 51
Expended since last report, viz :	2,991 90
For graduation and masonry,.....	
“ bridges, ..	
“ rails,	
“ chairs, spikes and ties,.....	
“ laying superstructure,.....	
“ buildings and fixtures,...	

TABLE C.

EQUIPMENT.

Total cost of equipment as per last report,.....	\$236,425 52
Expended since last report,	52,615 00
Total cost of road and equipment,.....	\$289,040 52

TABLE D.

CHARACTERISTICS OF ROAD.

Length of Road,	110 miles
“ “ completed, “	105 “
“ “ branches, “	
“ “ side tracks,.. “	
Weight of rail per yard,.....	56 lbs.

CHARACTER AND LENGTH OF BRIDGING.

	No. of structures.	No. of spans.	Length of bridging in feet.
Pile and Trustle, bridging,	10		1990
Trustle bridging, 50 feet span and under,	13	18	769
Trustle bridging from 50 to 100 ft. span,	7	10	666
Trustle bridging from 100 to 150 ft. span,	5	5	580
Trustle bridging 150 ft. span and over,	11	14	2014
Draw bridges, - -	none.		
Totals,	46	47	6019

Number of road crossings at grade,.....	70
Number of road crossings above and below grade,	6
Number of cross ties per mile,.....	2000
Chairs, number per mile, cast iron,.....	680
Whole number of switches on main track,.....	68
Howe Chairs, per mile,	440

GRADIENTS AND ALIGNMENT.

Level, number of miles,	
Maximum grade,..	52 $\frac{4}{5}$ feet
Amount of straight line, miles.....	
Amount of curved line, miles.....	
Maximum radius,.....	
Minimum radius,.....	
Sum of ascents going in one direction,.....	
Sum of ascents going in opposite direction,.....	
Height of termini and summit above tide water,..	

BUILDINGS AND FIXTURES.

Passenger houses,.....	3
Freight houses,.....	18
Engine houses,.....	2
Repair shops,.....	3
Water stations,..	13
Dwellings,	6
Wood sheds,..	20
Turn tables,.	2
Other buildings, as follows:	
Temporary shop,...	1

EQUIPMENT.

Number of locomotives owned by the Company on the 31st day of May, 1866, ten.

	Under 16 tons.	16 to 20.	20 to 25.	25 to 30.	30 tons and over.
In good repair,			4	2	
Requiring slight repairs,			2	1	
Requiring heavy repairs,			1		
Worn out,					

Number of cars owned by the Company May 31, 1866,	312
First class 8 wheel passenger cars in good repair,	8
First class 8 wheel passenger cars wanting repair,	3
Second class 8 wheel passenger cars in good re- pair,.....	none
Second class 8 wheel passenger cars wanting re- pair,.....	none
Baggage, express and mail cars in good repair,..	4
Baggage, express and mail cars wanting repair,..	1
Covered freight and cattle 8 wheel cars in good repair,.....	145
Covered freight and cattle 8 wheel cars wanting repair,.....	24
Platform 8 wheel cars in good repair,.	71
Platform 8 wheel cars wanting repair,.....	8
Other freight cars,.....	
Hand cars,.....	26
Gravel cars,.....	10
Station wood cars,.....	10
Average weight of passenger cars,.....	24000, lbs
Average weight of baggage cars,.....	17,000, lbs
Average weight of box cars,.....	14,000, lbs
Average weight of platform cars,...	12,278 lbs

TABLE E.
BUSINESS OF THE YEAR.

Miles run by passenger trains,.....	}	172,500
Miles run by freight trains,....		
Miles run by gravel and construction trains,....		
Miles run by wood trains,.....		
Number of through passengers carried in cars,...	}	107,774
Number of way passengers,.....		
Number of passengers having passes,.....		
Number of miles traveled by way passengers,....		
Average distance traveled by way passengers,....		
Number of miles traveled by passengers having passes,.....		
Number of tons of through freight,.....	}	See table
Number of tons of way freight,		
Number of tons of way freight carried 1 mile,...		
Number of tons of through freight moved towards market,.....		
Number of tons of through freight moved from market,.....		
Number of tons of way freight moved towards market,...		
Number of tons of way freight moved from market,		
Average rate of speed of ordinary passenger trains, including stops, per hour,.....	}	23 miles
Average rate of speed of express trains,...		
Average rate of speed of freight trains, per hour,.		10 miles
Rate of fare charged 1st class through passengers per mile,...		3 $\frac{1}{4}$ cents
Rate of fare charged 1st class way passengers per mile,.....		4 cents
Average rate of fare charged 2d class passengers per mile,.....		none
Rate per ton per mile charged on 1st class through freight,.....		5 $\frac{20}{31}$ cents
Rate per ton per mile charged on 2d class through freight,..		4 $\frac{37}{62}$ cents

Rate per ton per mile charged on 3d class through freight,.....	3 ⁵⁹ / ₆₂ cents
Rate per ton per mile charged on 4th class through freight,.....	3 ⁷ / ₃₁ cents
Rate per ton per mile charged on 1st class way freight,.....	8 ¹⁴ / ₂₁ cents
Rate per ton per mile charged on 2d class way freight,.....	7 ¹ / ₂₁ cents
Rate per ton per mile charged on 3d class way freight,.....	
Rate per ton per mile charged on 4th class way freight,.....	
Total number of tons of freight carried 1 mile,...	3,543,262
Total number of passengers carried 1 mile,.....	3,570,226

TABLE F.

EXPENSES OF MAINTAINING ROADWAY AND REAL ESTATE,

For the year ending June 30, 1866.

Ordinary repairs of road bed and superstructure,.	}	\$74,159 98
Extraordinary repairs of road bed,.....		
Cost of new rails used in repairs,...		
Number and weight of chairs,...		
Weight of spikes,..		
Cost of repairs of rails,.		
Number of cross ties used for renewals,.....		
Cost of same,.		
Cost of relaying rails and ties,.....	}	2,540 84
Insurance and taxes on real estate,.....		
Repairs of bridges,.....		
Repairs of stations, and new buildings,.....		
Repairs of fences, made by section men,.....		
Repairs of masonry,.....		
Total,...		\$87,9 8 51

COST OF REPAIRS OF MACHINERY.

Repairs of engines and tenders,.....	}	110,327 93
Repairs of passenger and baggage cars,.....		
Repairs of freight cars,.....		
Repairs of tools and machinery in work-shops,...		
Oil used about work-shops,.....	}	See table G
Fuel,.....		
Waste,.....		
Other items in detail as follows:		
Repairs of gravel and hand cars,.....		
Total,.....		\$110,327 93

TABLE G.

COST OF OPERATING THE ROAD.

For the year ending June 30, 1866.

Wood, including cost of preparing the same,..	\$32,921	26
Number of cords of wood used by locomotives,..		
Number of tons of coal used by locomotives,....		
Number of cords of wood used at stations,.....		
Number of tons of coal used at stations,.....		
Number of cords lost by fire,....		
Number of gallons of oil,.....		
Number of pounds of waste,.....		
Cost of oil and waste for engines and tenders,..		
Cost of oil and waste for passenger and baggage cars,....	}	5,627 80
Cost of oil and waste for freight cars,..		
Loss and damage of goods, ..		
Loss and damage of baggage, ..	}	3,928 93
Damages for injuries to persons, none,.....		
Damages to property, including fire and animals killed on road, none,.....		
19 Agents at stations, &c.,....	8,088	50
2 Clerks in general offices, &c.,..	650	00
Labor loading and unloading freight,.....	2,469	82

9 watchmen,.....	}	\$22,191 50
3 Switchmen,.....		
Wood and water station attendance,..		
5 Conductors and 2 baggagemen,.....		
5 Brakemen,.....		
7 Enginemen and 7 firemen,	}	1,700,00
Masters Transportation and Ticket Master,		
For salaries of Treasurer and Superintendent, and agent and office expenses,.....		5,934 92
For printing, stationery and office expenses,.....	}	22,784 92
For law expenses,.....		
Other expenses in detail, as follows :		
Government tax, interest, general expenses, post- age, expenses as to joint business, and all other business not included in other accounts		
Total,		\$106,297 65

RECAPITULATION OF EXPENSES.

Maintaining roadway,	87,958 51
Repairs of machinery,.....	110,327 93
Operating,.....	106,297 65
	<u>\$304,584 09</u>
Proportion of expenses due to passenger business,	101,528 03
Proportion of expenses due to freight business,...	203,056 06
Total,	<u>\$304,584 09</u>

TABLE H.

EARNINGS, RECEIPTS AND PAYMENTS.

Earnings and Receipts.

From passengers,.....	\$174,501 44
From freight,.....	290,724 28
From other sources, viz :	
Expresses,.....	3,500 00
Mails,.....	10,600 00

Rents,.....	1,251 62
Miscellaneous,.....	

Total..	\$480,577 34
---------	--------------

Payments other than for Construction.

For transportation expenses, viz :

For passenger business,.....	13,061 73
For freight business,.....	22,038 09
For other business, and what,.....	
For interest on funded debt,.....	
For interest on floating debt,.....	
For dividends,...	
For carried to surplus fund,.....	
For amount of surplus fund,..	

VALUE OF MATERIALS ON HAND.

Wood, cords of, 8579.	\$22,788 75
Teams, machinery, tools &c., in wood dept.....	9,114 38
Coal, tons of, 20.....	366 00
Oil, gallons of, 709... ..	944 11
Waste, pounds of, 1250.....	181 00
Iron rails, tons of, old, $107\frac{50}{2000}$ —214,050 lbs..	4,281 00
Iron rails, tons of, repaired $2\frac{700}{2000}$ —4,700 lbs.....	94 00
Chairs, pounds of, 14,790 lbs.....	597 21
Spikes, pounds of, 4,350 lbs.....	292 50
Ties, number of, 5000.....	1,250 00
Iron and other metals, unwrought, 547,859 lbs...	25,458 69
Iron and other metals, worked and partly worked, 50,370 lbs.....	6,601 75
Lumber, 326,507 feet,.....	9,981 76

Other items specified as follows :

Sundries.....	14,771 66
Cedar posts, 4000.....	320 00
Boards, 33,000 feet,.....	330 00

Total,	\$97,372 81
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COST OF TRANSPORTATION.

Actual cost of transporting freight per ton, per mile,

Actual cost of transporting passengers, per mile,.

DETAILS OF EARNINGS FOR THE YEAR ENDING JUNE 30, 1866.

SOURCE.	JULY.	AUGUST.	SEPTEMBER.	OCTOBER.	NOVEMBER.	DECEMBER.
Through passengers.....	5,125 00	7,179 $\frac{1}{2}$	8,086 $\frac{1}{2}$	3,930 $\frac{1}{2}$	2,922 $\frac{1}{2}$	2,335 $\frac{1}{2}$
Way passengers.....	7,919 00	9,201 $\frac{1}{2}$	8,958 $\frac{1}{2}$	5,349 00	4,573 00	4,681 $\frac{1}{2}$
Through freight..	17,861 02	26,189 97	27,629 76	32,808 43	30,423 22	24,537 19
Way freight.....						
Express.....	250 00	250 00	250 00	250 00	250 00	250 00
Transport of mails.....	883 34	883 33	883 33	883 34	883 33	883 33
Use of Engines,.....	included in freight acct.					
Use of cars,.....	104 30	104 30	104 30	104 30	104 30	104 30
Rent,.....						
Other earnings specified in detail as follows :						
Lumber down Conn. River in feet,...	802,582	728,890	997,667	1,099,939	1,264,557	1,022,070

DETAILS OF EARNINGS (CONTINUED.)

SOURCE.	JANUARY.	FEBRUARY.	MARCH.	APRIL.	MAY.	JUNE.
*Through passengers,.....	2,114 00	1,990 00	2,974 00	2,694 1 $\frac{1}{2}$	2,637 1 $\frac{1}{2}$	2,856 1 $\frac{1}{2}$
*Way passengers,.....	3,457 1 $\frac{1}{2}$	3,098 1 $\frac{1}{2}$	5,375 00	4,589 1 $\frac{1}{2}$	4,282 1 $\frac{1}{2}$	4,442 00
Through freight,.....	24,482 44	17,633 62	19,462 02	24,242 42	26,420 01	19,035 18
Way freight,.....						
Express,.....	350 00	350 00	350 00	350 00	350 00	250 00
Transport of mails,...	883 34	883 33	883 33	883 34	883 33	883 33
Use of Engines,.....	included in freight.					
Use of Cars,.....	104 30	104 30	104 30	104 30	104 31	104 31
Rents,						
Other earnings specified in detail as follows:						
Lumber down Conn. River in feet,....	850,085	572,614	150,840	1,283,551	1,406,360	961,424

* 1085 soldiers not included.

TABLE I.

ACCIDENTS.

State the number of persons injured in life or limb, and the cause thereof, and whether passenger or persons employed.

State whether any such accidents have arisen from carelessness or negligence of any person in the employ of the corporation, and whether such person is retained in the service of the corporation.

	EMPLOYEES.		OTHERS.	
	Killed.	Injured.	Killed.	Injured.
Trains thrown from track,				
Struck by bridge while on top of freight car,				
Run over while walking on track, .				
Injured at road crossing,				
Total,				

Total number of persons killed,

Total number of persons injured but not killed, 2

In addition to which must be given a statement of the date of each accident, the place where it occurred, the train, the cause and the extent of the injuries inflicted upon each person, and the name of such person.

Jason Babcock killed in car shop by screw with beam attached—falling upon his head.

TABLE J.
NUMBER OF EMPLOYEES AND COMPENSATION.

2 Conductors of passenger trains.	
Amount of compensation,..	\$1,560 00
3 Conductors of freight trains.	
Amount of compensation,.....	2,160 00
1 Conductors of wood and gravel trains.	
Amount of compensation,.....	469 50
1 Master mechanic.	
Amount of compensation,..	1,500 00
2 Road masters.	
Amount of compensation,.....	1,840 00
68 Men in repair shops, average No.	
Amount of compensation,..	37,395 88
4 Enginemen of passenger trains.*	
Amount of compensation,.....	2,352 00
6 Enginemen of freight trains.	
Amount of compensation,..	5,321 00
1 Enginemen of wood and gravel trains.	
Amount of compensation,.....	470 00
10 Firemen.**	
Amount of compensation,.....	4,433 83
2 Baggage-men.	
Amount of compensation,.....	1,200 00
4 Switchmen.	
Amount of compensation,.....	1,799 75
25 Section men, (foremen.)	
Amount of compensation,.....	12,847 20
65 Section hands, average No.	
Amount of compensation,	25,442 16
10 Watchmen.	
Amount of compensation,.....	4,640 10
18 Station agents.	
Amount of compensation,.....	8,264 75

* Two engineers all the year; two engineers, extra three months.

**Two firemen extra three months.

Other laborers,—28 gravel hands, average No.	
Amount of compensation,.....	7,151 58
50 Wood hands, average No.	
Amount of compensation,.....	19,905 81
1 Clerk connected with passenger business.	
Amount of compensation,.....	\$325 00
1 Clerk connected with freight business.	
Amount of compensation,.....	300 00
Superintendent of bridges,—salary,.....	
Wood Agent—salary,.....	
Other agents—how employed, and the salary of each, as follows:	
Cashier,.....	1,000 00

OFFICERS OF THE COMPANY.

SALARIES.

Agent,.....	\$1,200 00
Trustees,	
President,	
Superintendent, per annum,.....	2,000 00
Treasurer, per annum,.....	1,500 00

STATE OF VERMONT, }
COUNTY, ss. }

day of 186

I, Treasurer of
Company, do solemnly swear that the above is a true statement
of the condition of the finances of said Company, their Trustees,
or assignees, or lessees, on the 30th day of June 186

Treasurer.

Sworn before me,

Justice of the Peace.

TABLE K.

Does any conductor or engineman in the services of your road
indulge in intoxication ?

Please answer the same question as applied to sectionmen,
brakemen and switchmen.

Does your road or any of its operatives knowingly furnish
facilities for traffic in intoxicating liquors in violation of the
Prohibitory law of this state ?

Are the trains on your road allowed to stand across highway
crossings to the unnecessary delay of the highway travel or
business ?

No.

Do your stationmen allow boys without business to frequent
and remain in or about the stations, platforms, and trains ?

Are your road fences and cattle guards sufficiently constructed
and supported, and kept in repair as the law demands ?

STATE OF VERMONT,

CALEDONIA COUNTY, ss. }

I, A. H. Perry, depose and say that the facts set forth, and statements made in the foregoing report which has been signed by me, are true and correct according to the best of my knowledge, information and belief.

Signed,

A. H. PERRY,

Superintendent.

Subscribed and sworn before me this 18th day of September, A. D., 1866.

HUBBARD HASTINGS,

Master in Chancery.

ANNUAL REPORT

OF THE

RUT. & BURL'N & VT. VALLEY R. R. CO.,

FOR THE

YEAR ENDING AUGUST 31st, 1865.

TABLE A.

STOCK AND DEBTS.

TABLE B.

COST OF CONSTRUCTION.

Total cost of construction as per last report,.....
Expended since last report, viz :.....
For graduation and masonry,.....
“ bridges,
“ rails,
“ chairs, spikes and ties,.....
“ laying superstructure,.....
“ buildings and fixtures,... ..

TABLE C.

EQUIPMENT.

Total cost of equipment as per last report,.....	
Expended since last report,	
Total cost of road and equipment,.....	

TABLE D.

CHARACTERISTICS OF ROAD.

Length of Road,	144 miles
“ “ completed, “	
“ “ branches, “	
“ “ side tracks,.. “	
Weight of rail per yard,.....	56 and 60 lbs.

CHARACTER AND LENGTH OF BRIDGING.

	No. of structures.	No. of spans.	Length of bridging in feet.
Trustle bridging,	6	22	257
Trustle bridging, 50 feet span and under,	7	6	242
Trustle bridging from 50 to 100 ft. span,	11	12	852
Trustle bridging from 100 to 150 ft. span,	19	30	3639
Trustle bridging 150 ft. span and over,	6	10	1180
Draw bridges, - -			
Totals,	49	80	6170

Number of road crossings at grade,.....	96
Number of road crossings above and below grade,	20
Number of cross ties per mile,.....	2112
Chairs, number per mile,	588
Whole number of switches on main track,.....	68

GRADIENTS AND ALIGNMENT.

Level, number of miles,	
Maximum grade,	
Amount of straight line, miles	
Amount of curved line, miles	
Maximum radius,	
Minimum radius,	
Sum of ascents going in one direction,	
Sum of ascents going in opposite direction,	
Height of termini and summit above tide water, . .	

The books of the Rutland and Burlington Railroad having burned, to which we would refer, we are unable to answer the questions under this head.

BUILDINGS AND FIXTURES.

Passenger houses,	34
Freight houses,	11
Engine houses,	3
Repair shops,	3
Water stations,	19
Dwellings,	7
Wood sheds,	19
Turn tables,	4

Other buildings, as follows :

Rail repairs shops,	
Lake House, Burlington,	

EQUIPMENT.

Number of locomotives owned by the Company on the 31st day of August, 1865.

	Under 16 tons.	16 to 20.	20 to 25.	25 to 30.	30 tons and over.
In good repair,	1	6	12		
Requiring slight repairs,		4	4	1	
Requiring heavy repairs,			6		
Worn out,			1		

Number of cars owned by the Company Aug.

30th, 1865, 745.

First class 8 wheel passenger cars in good repair,	17
First class 8 wheel passenger cars wanting repair,	8
Second class 8 wheel passenger cars in good repair,.....	2
Second class 8 wheel passenger cars wanting repair,.....	.
Baggage, express and mail cars in good repair,..	9
Baggage, express and mail cars wanting repair,..	
Covered freight and cattle 8 wheel cars in good repair,.....	450
Covered freight and cattle 8 wheel cars wanting repair,.....	68
Platform 8 wheel cars in good repair,.	168
Other freight cars,.....	23
Total,	745
Gravel cars,	
Average weight of passenger cars,.....	28,000, lbs
Average weight of baggage cars,.....	18,000, lbs
Average weight of box cars,.....	16,000, lbs
Average weight of platform cars,.....	14,278 lbs

TABLE E.
BUSINESS OF THE YEAR.

Miles run by passenger trains,.....	256,429
Miles run by freight trains,....	289,975
Miles run by gravel and construction trains,....	38,577
Miles run by wood trains,.....	27,582
Number of through passengers carried in cars,...	90,852
Number of way passengers,.....	139,444
Number of passengers having passes,.....	
Number of miles traveled by way passengers,....	3,856,746
Average distance traveled by way passengers,....	24 miles
Number of miles traveled by passengers having passes,.....	
Number of tons of through freight,.....	145,840
Number of tons of way freight,	22,563
Number of tons of way freight carried 1 mile,...	846,095
Number of tons of through freight moved towards market,.....	9,848,162
Number of tons of through freight moved from market,.....	
Number of tons of way freight moved towards market,...	
Number of tons of way freight moved from market,	
Average rate of speed of ordinary passenger trains, per hour,.....	20 miles
Average rate of speed of express trains,...	30 miles
Average rate of speed of freight trains, per hour,.	15 miles
Rate of fare charged 1st class through passengers per mile,....	3½ cents
Rate of fare charged 1st class way passengers per mile,.....	4 cents
Average rate of fare charged 2d class passengers per mile,.....	1½ emg't
Rate per ton per mile charged on 1st class through freight,.....	5¼ cents
Rate per ton per mile charged on 2d class through freight,..	4½ cents

Rate per ton per mile charged on 3d class through freight,	4 cents
Rate per ton per mile charged on 4th class through freight,	3 $\frac{1}{2}$ cents
Rate per ton per mile charged on 1st class way freight,	7 cents
Rate per ton per mile charged on 2d class way freight,	6 $\frac{1}{2}$ cents
Rate per ton per mile charged on 3d class way freight,	4 cents
Rate per ton per mile charged on 4th class way freight,	4 cents
Total number of tons of freight carried 1 mile,...	10,662,617
Total number of passengers carried 1 mile,.....	8,361,028

TABLE F.

EXPENSES OF MAINTAINING ROADWAY AND REAL ESTATE,

For the year ending August 31, 1865.

Ordinary repairs of road bed and superstructure,.	} 118,703 82
Extraordinary repairs of road bed,.....	
Cost of new rails used in repairs,...	93,415 83
Number and weight of chairs,...	
Weight of spikes,..	
Cost of repairs of rails, .	
Number of cross ties used for renewals, 91703,	
Cost of same, .	36,664 14
Cost of relaying rails and ties,.....	
Insurance and taxes on real estate,.....	5,196 03
Repairs of bridges,.....	5,552 34
Repairs of stations, including new stations,.....	32,177 83
Repairs of fences,.....	4,460 20
Repairs of masonry,.....	1,357 04
Taxes, Internal Revenue,.....	24,111 78
Total, ..	<hr/> \$321,639 28

COST OF REPAIRS OF MACHINERY.

*Repairs of engines and tenders,.....	173,533 25
*Repairs of passenger and baggage cars,.....	28,099 56
*Repairs of freight cars,...	126,336 79
Repairs of tools and machinery in work-shops,...	
Oil used about work-shops,.....	
Fuel,.....	
Waste,.....	
Other items in detail as follows :	
Repairs of gravel and hand cars,.....	

Total,..... \$327,969 60

*Including four new Locomotives, new Freight and Passenger Cars.

TABLE G.

COST OF OPERATING THE ROAD.

For the year ending August 31, 1865.

Wood, including cost of preparing the same,..	\$126,914 41
Number of cords wood used by locomotives,31,727	
Number of tons of coal used by locomotives,....	
Number of cords of wood used at stations,.....	
Number of tons of coal used at stations,.....	
Number of cords lost by fire,.....	
Number of gallons of oil,.....	} 24,045 14
Number of pounds of waste,.....	
Cost of oil and waste for engines and tenders,..	
Cost of oil and waste for passenger and baggage cars, ...	
Cost of oil and waste for freight cars,..	
Loss and damage of goods,.....	5,804 45
Loss and damage of baggage,	570 81
Damages for injuries to persons,.....	} 7,966 35
Damages to property, including fire and animals killed on road,.....	
Agents at stations, &c.,...	
Clerks in general offices, &c.,...	
Labor loading and unloading freight,.....	

Porters and watchmen	}	7,286 43
Switchmen,		
Wood and water station attendance,		3,165 01
Conductors and baggagemen,		
Brakemen and passenger department		47,106 61
Enginemen and firemen, and freight department,..		95,985 74
For salaries of Trustees, President, Directors,	}	21,326 75
Secretaries, Treasurer and Superintendent,..		
For printing, stationery and office expenses,....		
For law expenses,.....		
Other expenses in detail, as follows :		
Mail service,.....		1,363 30
Land damage,.....		156 00
Extra freight,...		1,470 96
Total,		<hr/> \$343,161 97

RECAPITULATION OF EXPENSES.

Maintaining roadway,	\$321,639 28
Repairs of machinery,	327,969 60
Operating,	343,161 97
Proportion of expenses due to passenger business,	
Proportion of expenses due to freight business, . .	
Total,	<u>\$991,770 85</u>

TABLE H.

EARNINGS, RECEIPTS AND PAYMENTS.]

Earnings and Receipts.

From passengers,	307,955 12
From freight,	417,590 96
From other sources, viz :	
Expresses,	9,317 64
Mails,	15,750 00

Rents,.....	7,473 81
Miscellaneous,.....	

Total..	\$758,087 53
---------	--------------

Payments other than for Construction.

For transportation expenses, viz :

For passenger business,.....	47,106 61
For freight business,.....	95,985 74
For other business, and what,.....	
For interest on funded debt,	
For interest on floating debt,.....	
For dividends,	
For carried to surplus fund,.....	
For amount of surplus fund,	

VALUE OF MATERIALS ON HAND.

Wood, cords of, 6731.	26,824 00
Coal, tons of,	
Oil, gallons of, 1668	3,336 57
Waste, pounds of,	
Iron rails, tons of, old, 100 $\frac{1}{4}$,	
Iron rails, tons of, new,.....	
Chairs, pounds of, 16,000	480 00
Spikes, pounds of, 1,050	67 50
Ties, number of, 4696	1,878 40
Iron and other metals, unwrought,	56,332 73
Iron and other metals, worked and partly worked,	
Lumber,	10,801 91

Other items specified as follows :

COST OF TRANSPORTATION.

Actual cost of transporting freight per ton, per mile,
 Actual cost of transporting passengers, per mile, ..

DETAILS OF EARNINGS FOR THE YEAR ENDING AUGUST 31, 1865.

SOURCE.	JULY, 1865.	AUG., 1865.	SEPT., 1864.	OCT., 1864.	NOV., 1864.	DEC., 1864.
Through passengers.....	17,936 16	25,042 11	17,333 15	13,650 91	12,641 59	10,023 70
Way passengers.....	22,012 91	19,986 05	13,902 93	10,461 53	9,352 79	9,061 64
Through freight...	28,004 25	28,895 96	31,728 94	33,942 65	34,845 63	30,011 56
Way freight.....	5,392 17	6,736 05	4,643 50	4,230 53	3,746 73	3,650 30
Express.....	855 88	855 88	750 00	750 00	750 00	750 00
Transport of mails.....	1,291 67	1,541 68	1,291 66	1,291 67	1,291 66	1,291 67
Use of Engines.....						
Use of cars.....						
Rent,.....	505 87	1,972 54	467 54	467 54	467 54	467 54
Other earnings specified in detail as follows :						
Total, ...	75,998 91	85,030 27	70,117 72	64,794 83	63,095 94	55,256 41

DETAILS OF EARNINGS (CONTINUED.)

SOURCE.	JAN., 1865.	FEB., 1865.	MARCH, 1865.	APRIL, 1865.	MAY, 1865.	JUNE, 1865.
Through passengers,.....	10,266 36	9,898 52	13,327 90	13,533 92	10,012 51	14,283 85
Way passengers,.....	8,592 11	8,392 17	9,770 81	8,629 16	7,767 14	12,075 20
Through freight,.....	27,632 00	26,312 91	31,266 87	37,905 05	24,927 25	26,662 10
Way freight,.....	2,919 87	2,911 64	4,273 44	4,987 96	5,648 97	6,314 63
Express,.....	750 00	750 00	750 00	750 00	750 00	855 88
Transport of mails,...	1,291 66	1,291 67	1,291 66	1,291 67	1,291 66	1,291 67
Use of Engines,.....						
Use of Cars,.....	467 54	467 54	472 54	472 54	472 54	772 54
Rents,						
Other earnings specified in detail as follows:						
Total,.....	51,919 54	50,024 45	61,153 22	67,570 30	50,870 07	62,255 87

TABLE I.

ACCIDENTS.

State the number of persons injured in life or limb, and the cause thereof, and whether passenger or persons employed.

State whether any such accidents have arisen from carelessness or negligence of any person in the employ of the corporation, and whether such person is retained in the service of the corporation.

	EMPLOYEES.		OTHERS.	
	Killed.	Injured.	Killed.	Injured.
Trains thrown from track,	1		1	12
Struck by bridge while on top of freight car,			1	
Run over while walking on track, .				
Injured at road crossing,			2	2
Total,	1		4	14

Total number of persons killed,

Total number of persons injured but not killed, . .

In addition to which must be given a statement of the date of each accident, the place where it occurred, the train, the cause and the extent of the injuries inflicted upon each person, and the name of such person.

Nov. 8th, 1864.—Martin Madden jumped from express train at Sutherland Falls, and was fatally injured.

Nov. 23d, 1864.—Night express train North was thrown from the track by a broken rail, three miles north of Pittsford, and the engineer, Orrin Todd, instantly killed, and the fireman, John Austin, seriously injured.

Feb. 14th, 1865.—An engine run into a sleigh which was driven by an unfrequented road from the Lake, on crossing near the Lake House in Burlington, fatally injuring Miss

Feb. 4, 1865.—Aaron Hinckley, a boy eight years old, was run over on crossing in yard at Rutland, and killed.

March 21st, 1865.—Michael Gleason was killed in yard at Rutland, while attempting to uncouple cars of a freight train.

June 27th, 1865.—Night express down, when within about two miles of Dummerston, was thrown from the track by a large slide of earth, washed down by a heavy rain, and landed in the river. One man, name unknown, who was in the sleeping car, was taken out dead, and twelve others more or less injured, but not seriously.

July 2d, 1865.—A soldier, name unknown, who persisted in riding on the top of a passenger car, was knocked off by a bridge, near Pittsford, and killed.

July 13th, 1865.—Edward VanDreason, employed as clerk at Bellows Falls Station, fell from a freight train in the yard at Bellows Falls—run over and fatally injured.

TABLE J.

NUMBER OF EMPLOYEES AND COMPENSATION.

7 Conductors of passenger trains.	
Amount of compensation, per month,.....	\$60, \$75
7 Conductors of freight trains.	
Amount of compensation, per month,... ..	55, 60
3 Conductors of wood and gravel trains.	
Amount of compensation, per month,.....	55
4 Master mechanics.	
Amount of compensation,.. ..	75, 83, 125
2 Road masters.	
Amount of compensation, per annum,.....	1,000
124 Men in repair shops.	
Amount of compensation, per day,..... ..	1 50, 3 50
6 Enginemen of passenger trains.	
Amount of compensation, per month,.....	75 00
7 Enginemen of freight trains.	
Amount of compensation, per month,... ..	70 00
3 Enginemen of wood and gravel trains.	
Amount of compensation, per month,... ..	70 00
22 Firemen.	
Amount of compensation, pr month,.....	45 50

4 Baggage-men.	
Amount of compensation, per month,.....	55 00
10 Switchmen.	
Amount of compensation, per day,.....	1 50, 2 00
21 Section men, (foremen.)	
Amount of compensation, per month,.....	55 00
176 Section hands.	
Amount of compensation, per day,.....	1 25, 1 50
16 Watchmen.	
Amount of compensation, per day,.....	1 50, 1 85
29 Station agents.	
Amount of compensation, per month,....	10 00, 104 00
294 Other Laborers.	
Amount of compensation, per day,.....	1 25, 1 50
3 Clerks connected with passenger business.	
Amount of compensation, per month.....	\$25, to \$65
3 Clerks connected with freight business.	
Amount of compensation, per month,.....	\$30 to \$90
Superintendent of bridges,—salary, per month...	\$125 00
Wood Agent—salary, per month.....	\$125 00
Other agents—how employed, and the salary of each, as follows :	

OFFICERS OF THE COMPANY.

E. A. BIRCHARD, } Trustees and Lessees.
JOHN B. PAGE, }

J. M. HAVEN, Treasurer.

GEO. A. MERRILL, Superintendent.

SALARIES.

Agent,.....	
Trustees,	
President,	
Superintendent, per annum,.....	
Treasurer, per annum,....	

STATE OF VERMONT, }

18th day of October, 1865.

RUTLAND COUNTY, ss. }

I, J. M. Haven, Treasurer of the Trustees Second Mortgage Rutland & Burlington Railroad Company, do solemnly swear that the above is a true statement of the condition of the finances of said Company, their Trustees, or assignees, or lessees, on the 31st day of August, 1865.

J. M. HAVEN,
Treasurer.

Subscribed and sworn to before me,

GEO. A. MERRILL,
Notary Public.

TABLE K.

Does any conductor or engineman in the services of your road indulge in intoxication ?

Not to the knowledge of the Managers.

Please answer the same question as applied to sectionmen, brakemen and switchmen.

No.

Does your road or any of its operatives knowingly furnish facilities for traffic in intoxicating liquors in violation of the Prohibitory law of this state ?

No.

Are the trains on your road allowed to stand across highway crossings to the unnecessary delay of the highway travel or business ?

No.

Do your stationmen allow boys without business to frequent and remain in or about the stations, platforms, and trains ?

No.

Are your road fences and cattle guards sufficiently constructed and supported, and kept in repair as the law demands ?

STATE OF VERMONT, }
RUTLAND COUNTY, ss. }

We, Edwin A. Birchard and John B. Page, depose and say that the facts set forth, and statements made in the foregoing report which has been signed by us, are true and correct, according to the best of our knowledge, information and belief.

E. A. BIRCHARD,
JOHN B. PAGE.

Signed, sworn to before me,

WALTER C. DUNTON,
Justice of the Peace.

The fiscal year of this road, ending August 31st, it is impossible to close the accounts of the year in season to be published in Commissioners Report, to be presented at the next October session.

STATE OF VERMONT, }
RUTLAND COUNTY, ss. }

I, Geo. A. Merrill, depose and say that the facts set forth, and statements made in the foregoing report which has been signed by me, are true and correct according to the best of my knowledge, information and belief.

Signed,

GEO. A. MERRILL,
Superintendent.

Subscribed and sworn before me this 18th day of September, A. D., 1866.

J. B. PAGE,
Notary Public.

ANNUAL REPORT
OF
THE BOARD OF DIRECTORS
OF THE
RUTLAND & WHITEHALL R. R. CO.,
FOR THE
YEAR ENDING JUNE 30, 1866.

The road of this Company extends from its eastern terminus in the village of Castleton, where it forms a connection with the road of the Rutland & Washington Railroad Company, westwardly through Hydeville and Fairhaven, to the west line of the State in Fairhaven, where it connects with the Saratoga and Whitehall Railroad.

The length of this road, including the branch at Hydeville, is seven miles and a fraction, and the same is leased perpetually to the Saratoga & Whitehall Railroad Company, at a rent equal to 6 per cent. upon its capital stock, payable quarter yearly. Its whole capital stock is \$255,700, including cost of road and branch and its rolling stock, and the stock is divided into shares of \$100 each, and the same is owned by 104 different persons, residing in the Eastern, Middle and Western States.

The quarterly rents have been promptly paid, and as promptly distributed to the stockholders during the past year.

This Company have upon the line of the road 4 Bridges, 3 Station Houses and 5 Turn-outs. The condition and state of

repairs of the road and bridges are believed to be good and safe for travelling.

By an arrangement betwixt the lessees of this Company and the Rensselaer & Saratoga Railroad Company, the control and direction of this road has passed into the hands of the latter Company since the 1st of May, 1865. At the same time, also, the same Company obtained, by contract, the control of the Rutland & Washington Railroad, extending from Rutland by way of Castleton and Poultney, to Eagle Bridge in the State of New York. The beneficial consequence of this arrangement, as well to the business and travelling public as to those interested pecuniarily, in the successful operation of this important thoroughfare of business and travel, continues to be more and more manifest.

The whole business operations upon this road, being carried on in connection with the business upon the other roads, operated by the Rensselaer & Saratoga R. R. Co., makes it impossible to present a specific statement of the business and earnings of this particular road.

This Company owe no debts, and receive no revenue from their road except the rent aforesaid.

The Board of Directors and officers for the current year are the following :

A. W. HYDE, Castleton,	} Directors.
WM. C. KITTREDGE, Fairhaven,	
ISRAEL DAVEY, Fairhaven,	
W. W. COOK, Whitehall,	
G. R. I. BOWDOIN, New York,	}

A. W. HYDE, *President.*

W. W. COOK, *Vice-President.*

WM. C. KITTREDGE, *Clerk, Treasurer and Transfer Agent.*

By order of the Board of Directors.

WM. C. KITTREDGE, *Director.*

To HON. D. E. NICHOLSON,

Railroad Commissioner.

STATE OF VERMONT, }
RUTLAND COUNTY, ss. }

At Fairhaven, in said County, this 27th day of September, 1866, personally appeared Wm. C. Kittredge, who signed the foregoing statement in my presence, and made oath that the same is true, according to his best knowledge, information and belief.

Before me,

Z. C. ELLIS, *Justice of the Peace.*

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TWELFTH ANNUAL REPORT

OF THE

RAILROAD COMMISSIONER

OF THE

STATE OF VERMONT,

TO THE

GENERAL ASSEMBLY,

OCTOBER SESSION, 1867.

RUTLAND, VT.,
TUTTLE & COMPANY, PRINTERS.
1867.

TWELFTH ANNUAL REPORT

TO THE

GENERAL ASSEMBLY.

OF THE

RAILROAD COMMISSIONER

OF THE

STATE OF VERMONT,

OCTOBER SESSION, 1867.

RUTLAND:
TUTTLE & CO., PRINTERS.
1867.

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V59

1866/67



REPORT.

RAILROAD COMMISSIONER'S OFFICE, }
Wallingford, Vt., Sept. 16th, 1867. }

To the General Assembly of the State of Vermont :

The undersigned respectfully reports upon the progress, condition and administration of the several Railroads within his jurisdiction, for the current official year, as follows :

For reasons of continuing force, I have copied closely my own administration of this department of the public service, so kindly approved by your predecessors of last year, and again report :

That in pursuance of my duties as defined by law, I furnished to the proper officers of all the Railroad corporations of this State (including such without the limits of Vermont as are immediately and by some proper interest connected with our lines,) tabular blank interrogatories, the sworn answers to which, so far as seasonably received, are herewith submitted for the use of the public through the sanction of your Honorable body.

I have spared no labor or care to keep myself and the proper officers of the roads informed of any and all suspected occasions of danger or inconvenience as to road bed, track, grade, slopes, curves, sub-structure, super-structure, connections, speed, sobriety, courtesy and responsibility, and find that all officers and functions involved in the operating of this right arm of our prosperity, are controlled by an enlightened apprehension of the advantages of meeting the public expectations and demands upon them.

It would be invidious to particularize, since all of the roads are making such outlays in current improvements ; and I, therefore, leave this topic by referring to their own aforesaid sworn reports.

I submit for the consideration of your Honorable body whether the corporations should be hurried to elevate their above grade bridges for the safety of brakemen on the freight trains, and whether further legislation is needed to guard a common danger at the various highway and farm crossings from strolling or other farm stock.

For Report of Rensselaer & Saratoga and the Troy & Boston Railroads, whose lines under current operation extend into this State, I make reference to extracts from the last Annual Report of the State Engineer and Surveyor of the State of New York.

All of which is respectfully submitted.

D. E. NICHOLSON,
Railroad Commissioner.

ANNUAL REPORT

OF THE

VT. CENTRAL RAILROAD CO.,

FOR THE

YEAR ENDING MAY 31, 1867.

TABLE A.

STOCK AND DEBTS.

One hundred thousand shares of stock issued :

First Mortgage Bonds issued,.....	\$3,000,000 00
Second Mortgage Bonds issued,.....	1,500,000 00
Equipment Loan Bonds issued,.....	1,000,000 00

TABLE B.

COST OF CONSTRUCTION.

Total cost of construction, as per last report, ..	\$8,402,054 92
Expended since last report, viz :	
For graduation and masonry,.....	
“ bridges,.....	
“ rails,.....	
“ chairs, spikes and ties,.....	
“ laying superstructure,.....	
“ buildings and fixtures,.....	

TABLE C.

EQUIPMENT.

Total cost of equipment as per last report,.....

Expended since last report,.....

Total cost of road and equipment,.....

TABLE D.

CHARACTERISTICS OF ROAD.

Length of road, (Vt. Central R. R.)..... 117 miles.

" " (Vt. & Canada R. R.) 65½ "

" " completed, (Vt. C. and Vt. & C.) 182½ "

" " branches,..... 2 "

" " side tracks,..... 27¼ "

Weight of rail per yard, 50 to 60 lbs.

All within the state.

CHARACTER AND LENGTH OF BRIDGING.

	No. of Structures.	No. of Spans.	Length of bridging in feet.
Trussle bridging,.....	8	.....	7990
Truss. bridging, 50 ft. span and under,.....	45	55	1595
Truss. do., from 50 to 100 feet span,.....	6	6	357
Truss. do., from 100 to 150 feet span,.....	19	30	3681
Truss. do., 150 ft. span and over,	13	27	4240
Draw bridges,... ..	2	..	339
Totals,	93	118	18202

VERMONT CENTRAL RAILROAD.

7

Number of road crossings at grade,.....	124
Number of road crossings above and below grade,	34
Number of cross ties per mile,.....	2200
Chairs, number per mile,.....	440 to 700
Whole number of switches on main track,.....	138

GRADIENTS AND ALIGNMENT.

Level number of miles,	unknown.
Maximum grade,.....	"
Amount of straight line, on Vt. Central,.....	83 miles.
Amount of curved lines, on Vt. Central,.....	34 "
Maximum radius, on Vt. Central,.....	11,460 feet.
Minimum radius, on Vt. Central,.....	1,146 "
Sum of ascents going in one direction, ..	unknown.
Sum of ascents going in opposite direction,.....	"
Height of termini and summit above tide water,.	"

BUILDINGS AND FIXTURES.

Passenger houses, Vt. C. and Vt. & Canada,....	36
Freight houses, " " ..	36
Engine houses, " "	6
Repair shops, " " ...	4
Water stations, " " . . .	28
Dwellings, " "	22
Wood sheds, " "	49
Turn tables, " "	5
Other buildings, as follows :	
Car houses, " "	2
Ice houses, " "	5

EQUIPMENT.

Number of locomotives owned by the Company on the 31st day of May, 1867 :

	Under 16 tons.	16 to 20.	20 to 25.	25 to 30.	30 tons and over.
In good repair,.....				5	46
Requiring slight repairs,.....					4
Requiring heavy repairs,.....					3
Worn out,.....				1	1

Number of cars owned by the company May 31st, 1867.

First class 8 wheel passenger cars in good repair,.	35
First class 8 wheel passenger cars wanting repairs,	7
2d class 8 wheel passenger cars in good repair,..	
2d class 8 wheel passenger cars wanting repair,..	
Baggage, express and mail cars in good repair,..	14
Baggage, express and mail cars wanting repair,..	
Covered freight and cattle 8 wheel cars in good repairs,	890
Covered freight and cattle 8 wheel cars wanting repairs,	25
Platform 8 wheel cars in good repair,.....	179
Other freight cars,	25
Gravel cars,	40
Average weight of passenger cars,	15 tons.
do do baggage do	10 tons.
do do box do	$7\frac{3}{4}$ tons.
do do platform do	$6\frac{1}{2}$ tons.
Snow plows,.....	5
Derrick cars,	5
Paymaster's car,.....	1

TABLE E.

BUSINESS OF THE YEAR.

Miles run by passenger trains,	433,471
Miles run by freight trains,	699,844
Miles run by gravel and construction trains,	49,552
Miles run by wood trains,	42,231
Number of through passengers carried in cars,	151,670
Number of way passengers,	204,023
Number of passengers having passes,	
Number of miles traveled by way passengers,	4,185,065
Average distance traveled by way passengers,	20½ miles.
Number of miles traveled by passengers having passes,	
Number of tons of through freight,	219,047
Number of tons of way freight,	213,063
Number of tons of way freight carried 1 mile,	15,815,129
Number of tons through freight moved towards market,	Do not know.
Number of tons through freight moved from market,	do
Number of tons of way through freight moved towards market,	do
Number of tons of way through freight moved from market,	do
Average rate of speed of ordinary passenger trains,	22 m. pr. hour
Average rate of speed of express trains,	27½ do
Average rate of speed of freight trains,	12 do
Rate of fare charged first class through passengers per mile,	3½ cents.
Rate of fare charged first class way passengers per mile,	4 cents.
Average rate of fare charged 2d class passengers per mile,	2 cents.

Rate per ton per mile charged 1st class thro'freig't,	Rates
Rate per ton per mile charged on 2d class through freight,	vary from
Rate per ton per mile charged on 3d class through freight,	one and
Rate per ton per mile charged on 4th class through freight,	a half to
Rate per ton per mile charg'd 1st class way freight,	ten cents
Rate per ton per mile charged on 2d class way freight,	a mile according
Rate per ton per mile charged on 3d class way freight,.....	to season
Rate per ton per mile charged on 4th class way freight,	and distance.
Total number of tons of freight carried one mile,.	41,455,549
Total number of passengers carried one mile,....	14,537,172

TABLE F.

EXPENSES OF MAINTAINING ROADWAY AND REAL ESTATE.

For the year ending May 31, 1857.

Ordinary repairs of road bed and superstructure,.	\$118,824	15
Extraordinary repairs of road bed,.....		
Cost of new rails used in repairs,...	107,630	28
Number and weight of chairs,.....		
Weight of spikes,.....		
Cost of repairs of rails,.....	29,951	35
Number of cross ties used for renewals, . . .		
Cost of same,.....	33,979	23
Cost of relaying rails and ties,.....	2,833	88
Insurance and taxes on real estate,.....	7,789	70
Repairs of bridges,	59,925	98
do stations,.....	64,088	47
do fences,	3,123	29
do depot furniture,....	7,365	14
Total,.....	\$435,511	47

COST OF REPAIRS OF MACHINERY.

Repairs of engines and tenders,.....	\$108,336 30
Repairs of passenger and baggage cars,.....	54,893 89
Repairs of freight cars,	135,548 68
Repairs of tools and machinery in shops,... ..	10,525 06
Oil used about workshops and stations,.....	2,876 57
Fuel used at shops, engine houses and stations,...	16,636 72
Waste used at shops,.....	84 80
Other items in detail as follows :	
Repairs of gravel, section and hand cars,.....	4,955 05
Repairs of snow plows,... ..	13 89
Repairs of road and track tools,.....	3,127 74
Total,.....	<u>\$336,998 70</u>

TABLE G.

COST OF OPERATING THE ROAD.

For the year ending May 31st, 1867.

Wood, including cost of preparing the same,.. .	\$263,088 52
Number of cords of wood used by locomotives,..	
“ “ tons of coal “ “ ..	
Number of cords of wood used at stations,.....	
“ “ tons of coal “ “	
Number of cords lost by fire,.....	
Number of gallons of oil,.....	
Number of pounds of waste,.....	
Cost of oil and waste for engines and tenders,....	25,177 70
Cost of oil and waste for passenger and baggage cars,	3,703 72
Cost of oil and waste for freight cars,... ..	13,950 11
Loss and damage of goods,.. ..	3,797 99
Loss and damage of baggage,.....	2,313 58
Damages for injuries to persons,... ..	4,312 62
Damages to property, including fire and animals	
killed on road,	1,185 25
Agents at stations, &c.,	29,794 76

VERMONT CENTRAL RAILROAD.

Clerks in general offices, &c.,.....	24,005 11
Labor, loading and unloading freight,	40,409 95
Porters and watchmen,.....	10,728 46
Switchmen,	12,197 75
Wood and water station attendance,.....	
Conductors, baggage-men and brakemen,.....	65,857 10
Enginemen and firemen,.....	61,313 88
For salaries of Trustees, President, Directors, Secretaries, Treasurer and Superintendent,..	16,281 86
For printing, stationery and office expenses,.....	8,335 00
For law expenses,.....	7,718 44
Other expenses in detail as follows :	.
Advertising,	1,394 86
U. S. Government taxes,.....	16,956 05
Interest,.....	29,901 13
General expense not included above,.....	23,786 16
Total,.....	\$666,207 00

RECAPITULATION OF EXPENSES.

Maintaining roadway,.....	\$435,511 47
Repairs of machinery,.....	336,998 70
Operating,	666,207 00
Proportion of expenses due to passenger business,	
Proportion of expenses due to freight business,..	
Total, ..	\$1,438,717 17

TABLE H.

EARNINGS, RECEIPTS AND PAYMENTS.

Earnings and Receipts.

From passengers,.....	\$ 533,739 55
From freight,.....	1,208,679 11
From other sources, viz :	.
Expresses,	10,078 93
Mails,	27,481 62
Rents,.....	4,012 02
Total,.....	\$1,783,991 23

Payments other than for Construction.

For transportation expenses, viz :

For passenger business,.....	
For freight business,.....	
For other business and what,.....	
For interest on funded debt,....	
For interest on floating debt,.....	
For dividends,.....	
For carried to surplus fund,.....	
For amount of surplus fund,.....	

VALUE OF MATERIAL ON HAND.

Wood, cords of,.....	68,874
Coal, tons of,	388
Oil, gallons of,.....	1,721
Waste, pounds of,	2,800
Iron rails, tons of, old,.....	
Iron rails, tons of, new,.....	
Chairs, pounds of,.....	
Spikes, pounds of,.....	
Ties, number of,.....	
Iron and other metals unwrought,	332,748 lbs.
Iron and other metals worked and partly worked,	216,365 lbs.
Lumber,.. ..	360,600 feet.

Other items specified as follows :

COST OF TRANSPORTATION.

Actual cost of transporting freight, per ton, per mile,	
Actual cost of transporting passengers per mile,..	

VERMONT CENTRAL RAILROAD.

DETAILS OF EARNINGS FOR THE YEAR ENDING MAY 31, 1867.

SOURCES.	JUNE, 1866.	JULY.	AUGUST.	SEPTEMBER.	OCTOBER.	NOVEMBER.
Through Passengers,.....	32,569 19	28,192 36	41,832 01	33,539 35	30,794 94	21,746 50
Way Passengers,.....	12,437 93	14,630 81	16,683 63	16,631 03	16,114 06	12,743 47
Through Freight,...	80,083 26	87,981 22	101,374 23	99,414 75	111,734 00	100,431 44
Way Freight,.....	6,970 24	7,792 15	7,982 41	8,844 56	9,834 53	8,802 84
Express, .....	.....	.....	.....	.....	.....	.....
Transport of Mails,.....	2,082 08	2,082 08	2,082 08	2,082 08	2,082 08	2,070 39
Use of Engines,...	.....	.....	.....	.....	.....	.....
Use of Cars,...	.....	.....	.....	.....	.....	.....
Rent, .....	.....	.....	.....	.....	.....	174 72
Total,.....	134,142 70	140,678 62	169,954 36	160,511 77	170,559 61	145,969 36

DETAILS OF EARNINGS, (Continued.)

SOURCES.	DEC., 1866.	JAN., 1867.	FEBRUARY.	MARCH.	APRIL.	MAY.
Through Passengers,.....	18,259 30	18,357 63	15,546 39	17,608 42	36,125 22	23,764 54
Way Passengers,.....	10,930 63	9,684 60	10,168 38	12,573 43	13,207 60	69,598 13
Through Freight,.....	84,059 08	70,018 00	63,016 79	61,758 85	73,675 39	88,387 97
Way Freight,.....	7,862 18	5,945 17	6,907 42	7,496 36	7,869 47	100,436 80
Express,.....	.....	.....	.....	.....	.....	10,078 93
Transport of Mails,.....	2,082 08	2,082 08	2,082 08	2,082 08	2,082 08	4,590 43
Use of Engines,.....	.....	.....	.....	.....	.....	.....
Use of Cars,.....	.....	.....	.....	.....	.....	.....
Rent,.....	.....	.....	.....	.....	.....	3,837 30
Total,.....	123,193 27	106,087 48	97,721 06	101,519 14	132,959 76	300,694 10

TABLE I.

ACCIDENTS.

State the number of persons injured in life or limb, and the cause thereof, and whether passengers or persons employed.

State whether any such accidents have arisen from carelessness or negligence of any person in the employ of the corporation, and whether such person is retained in the service of the corporation.

	EMPLOYEES.		OTHERS.	
	Killed.	Injured.	Killed.	Injured.
Trains thrown from track,.....	2	.....	.....	... ..
Struck by bridge while on top of freight car,.....	.....	.....	.....	.....
Run over while walking on track. ....	... ..	.....	2	1
Injured at road crossing,.....	... ..	.....	... ..	.....

Total number of persons killed,..... 4

Total number of persons injured but not killed, 1

In addition to which must be given a statement of the date of each accident, the place where it occurred, the train, the cause and the extent of the injuries inflicted upon each person, and the name of such person.

AUGUST 25th, 1866.—Samuel Decker, while walking on the track in Swanton, was run over by an Engine, and one leg so injured as to make its amputation necessary.

NOVEMBER 24th, 1866.—Silas Chamberlain, (Insane), was run over and killed, while walking on the track near Randolph Station.

NOVEMBER 24th, 1866.—An unknown man was found dead between Essex Junction and Colchester, supposed to have been killed by some passing train during the night.

DECEMBER 6th, 1866.—Charles S. Hayden, engineer, and

Vincent Chamberlin, fireman, were killed by the Engine on which they were being thrown from the track down the embankment between White River Junction and North Hartland. They were running the mail train North.

TABLE J.

NUMBER OF EMPLOYEES AND COMPENSATION ON VT. CENTRAL AND
VT. AND CANADA RAILROAD.

14 Conductors of passenger trains.	
Amount of compensation, (each per month,)	\$50 to 75
36 Conductors of freight trains.	
Amount of compensation, (each per month,)	45 to 60
3 Conductors of wood and gravel trains.	
Amount of compensation, (each per month,)	60
1 Master mechanic.	
Amount of compensation, (per year,)	4,000
2 Road Masters,	
Amount of compensation, (both per month,)	150
286 Men in repair shops.	
Amount of compensation, (per day each,)	1 to 4
13 Engine men of passenger trains.	
Amount of compensation, (each per day,)	3
35 Engine men of freight trains.	
Amount of compensation, (each per day,)	2.75 to 3
3 Engine men of wood and gravel trains.	
Amount of compensation, (each per day,)	3
47 Firemen.	
Amount of compensation, (each per day,)	1.75
14 Baggage-men.	
Amount of compensation, (each per month,)	26 to 45
26 Switchmen,	
Amount of compensation, (each per month,)	26 to 50
44 Section men (foreman,)	
Amount of compensation, (each per month,)	40 to 60

250 Section hands.	
Amount of compensation, (each per day,).....	1 50 to 1 60
32 Watchmen.	
Amount of compensation, (each per month,).....	10 to 45
35 Station agents.	
Amount of compensation, (each per year,).....	60 to 1,100
359 Other laborers.	
Amount of compensation, (each per year,)	1 to 3
3 Clerks connected with passenger business.	
Amount of compensation, (total per month,).....	200
11 Clerks connected with freight business.	
Amount of compensation, (total per month,).....	849 16
Superintendent of bridges—salary, (per month,)..	100
Wood agent, “ “	91 67
Other agents—how employed, and the salary of each as follows, viz :	

OFFICERS OF THE COMPANY—SALARIES.

Trustees,.....	...
President,.....	...
Superintendent, .....	...
Treasurer,.....	...

STATE OF VERMONT, }
COUNTY, SS. }

day of 186

I Treasurer of
the Railroad
Company, do solemnly swear that the above is a true statement
of the condition of the finances of said company, their Trustees,
or assignees, or lessees, on the 30th of June, 186

Treasurer.

Sworn before me,

Justice of the Peace.

TABLE K.

Does any conductor or engineman in the service of your road
indulge in intoxication?

The rules of the road expressly prohibit the use of intoxicating
drinks by any employee of the road, and the managers have no
knowledge of any violation of this rule.

Please answer the same question as applied to sectionmen,
brakemen and switchmen.

Same as above.

Does your road, or any of its operatives, knowingly furnish
facilities for traffic in intoxicating liquors in violation of the
prohibitory law of the State?

They do not.

Are the trains on your road allowed to stand across highway
crossings to the unnecessary delay of the highway travel or
business?

They are not.

Do your stationmen allow boys without business to frequent
and remain in or about the stations, platforms and trains?

The rules prohibit it.

Are your road fences and cattle guards sufficiently constructed
and supported, and kept in repair as the law demands?

They are supposed to be.

Are your trains jeopardized by neglect of persons to keep closed bars and gates at the various farm crossings? Answer in detail or with proximate particularity.

They are occasionally.

STATE OF VERMONT, }
FRANKLIN COUNTY, ss. }

We depose and say that the facts set forth, and statements made in the foregoing report which has been signed by us are true and correct according to the best of our knowledge, information and belief.

Signed,

L. BRAINERD,
JOSEPH CLARK,
J. GREGORY SMITH.

Subscribed and sworn before me this 31st day of August, 1867.

SAMUEL WILLIAMS,
Notary Public.

ANNUAL REPORT

OF THE

VT. & CANADA RAILROAD CO.,

FOR THE

YEAR ENDING JUNE 30, 1867.

TABLE A.

STOCK AND DEBTS.

Capital Stock of the Vermont and Canada R. R.,
on the 1st of June, 1867,..... \$2,500,000 00

TABLE B.

COST OF CONSTRUCTION.

Total cost of construction, as per last report,. . .
Expend ed since last report, viz :
For graduation and masonry,.....
“ bridges,.....
“ rails,.....
“ chairs, spikes and ties,.....
“ laying superstructure,.....
“ buildings and fixtures,.....

NOTE.—For information from and under this table (B) to and including table J., reference is respectfully made to the report of the Trustees and Managers of the Vt. Central and Vt. & Canada Railroads,—by whom the Vt. & Canada road is operated.
W. C. SMITH, Pres't Vt. & Canada R. R. Co.

TABLE C.

EQUIPMENT.

Total cost of equipment as per last report,.....
 Expended since last report,.....
 Total cost of road and equipment,.....

TABLE D.

CHARACTERISTICS OF ROAD.

Length of road, (Vt. Central R. R.).....
 “ “ (Vt. & Canada R. R.)
 “ “ completed, (Vt. C. and Vt. & C.)
 “ “ branches,.....
 “ “ side tracks,.....
 Weight of rail per yard,.....
 All within the state.

CHARACTER AND LENGTH OF BRIDGING.

	No. of Structures.	No. of Spans.	Length of bridging in feet.
Trussle bridging,.....
Truss. bridging, 50 ft. span and under,.....
Truss. do., from 50 to 100 feet span,.....
Truss. do., from 100 to 150 feet span,.....
Truss. do., 150 ft. span and over,
Draw bridges,...
Totals,

Number of road crossings at grade,.....
 Number of road crossings above and below grade,
 Number of cross ties per mile,.....
 Chairs, number per mile,.....
 Whole number of switches on main track,.....

GRADIENTS AND ALIGNMENT.

Level number of miles,
 Maximum grade,.....
 Amount of straight line, on Vt. Central,.....
 Amount of curved lines, on Vt. Central,.....
 Maximum radius, on Vt. Central,.....
 Minimum radius, on Vt. Central,.....
 Sum of ascents going in one direction, ..
 Sum of ascents going in opposite direction,.....
 Height of termini and summit above tide water,.

BUILDINGS AND FIXTURES.

Passenger houses, Vt. C. and Vt. & Canada,....			
Freight houses,	"	"	..
Engine houses,	"	"	....
Repair shops,	"	"	....
Water stations,	"	"	...
Dwellings,	"	"	....
Wood sheds,	"	"	....
Turn tables,	"	"	....
Other buildings, as follows :			
Car houses,	"	"	....
Ice houses,	"	"	....

EQUIPMENT.

Number of locomotives owned by the Company on the 30th day of June, 1867 :

	Under 16 tons.	16 to 20.	20 to 25.	25 to 30.	30 tons and over.
In good repair,
Requiring slight repairs,
Requiring heavy repairs,
Worn out,

Number of cars owned by the company June 30th, 1867.

First class 8 wheel passenger cars in good repair, .
 First class 8 wheel passenger cars wanting repairs, .
 2d class 8 wheel passenger cars in good repair, . .
 2d class 8 wheel passenger cars wanting repair, . .
 Baggage, express and mail cars in good repair, . .
 Baggage, express and mail cars wanting repair, . .
 Covered freight and cattle 8 wheel cars in good
 repairs,
 Covered freight and cattle 8 wheel cars wanting
 repairs,
 Platform 8 wheel cars in good repair,
 Other freight cars,
 Gravel cars,
 Average weight of passenger cars,
 do do baggage do
 do do box do
 do do platform do
 Snow plows,
 Derrick cars,
 Paymaster's car,

TABLE E.

BUSINESS OF THE YEAR.

Miles run by passenger trains,
Miles run by freight trains,
Miles run by gravel and construction trains,
Miles run by wood trains,
Number of through passengers carried in cars,
Number of way passengers,
Number of passengers having passes,
Number of miles traveled by way passengers,
Average distance traveled by way passengers,
Number of miles traveled by passengers having passes,
Number of tons of through freight,
Number of tons of way freight,
Number of tons of way freight carried 1 mile,
Number of tons through freight moved towards market,
Number of tons through freight moved from market,
Number of tons of way through freight moved towards market,
Number of tons of way through freight moved from market,
Average rate of speed of ordinary passenger trains,
Average rate of speed of express trains,
Average rate of speed of freight trains,
Rate of fare charged first class through passengers per mile,
Rate of fare charged first class way passengers per mile,
Average rate of fare charged 2d class passengers per mile,

Rate per ton per mile charged 1st class thro'freig't,	
Rate per ton per mile charged on 2d class through freight,	
Rate per ton per mile charged on 3d class through freight,	
Rate per ton per mile charged on 4th class through freight,	
Rate per ton per mile charg'd 1st class way freight,	
Rate per ton per mile charged on 2d class way freight,	
Rate per ton per mile charged on 3d class way freight,	
Rate per ton per mile charged on 4th class way freight,	
Total number of tons of freight carried one mile, .	
Total number of passengers carried one mile,	

TABLE F.

EXPENSES OF MAINTAINING ROADWAY AND REAL ESTATE.

For the year ending June 30th, 1857.

Ordinary repairs of road bed and superstructure, .	
Extraordinary repairs of road bed,	
Cost of new rails used in repairs,	
Number and weight of chairs,	
Weight of spikes,	
Cost of repairs of rails,	
Number of cross ties used for renewals,	
Cost of same,	
Cost of relaying rails and ties,	
Insurance and taxes on real estate,	
Repairs of bridges,	
do stations,	
do fences,	
do depot furniture,	
Total,	

COST OF REPAIRS OF MACHINERY.

Repairs of engines and tenders,	
Repairs of passenger and baggage cars,	
Repairs of freight cars,	
Repairs of tools and machinery in shops,	
Oil used about workshops and stations,	
Fuel used at shops, engine houses and stations, ...	
Waste used at shops,	
Other items in detail as follows :	
Repairs of gravel, section and hand cars,	
Repairs of snow plows,	
Repairs of road and track tools,	
Total,	

TABLE G.

COST OF OPERATING THE ROAD.

For the year ending June 30th, 1867.

Wood, including cost of preparing the same,	
Number of cords of wood used by locomotives, ..	
“ “ tons of coal “ “ ..	
Number of cords of wood used at stations,	
“ “ tons of coal “ “	
Number of cords lost by fire,	
Number of gallons of oil,	
Number of pounds of waste,	
Cost of oil and waste for engines and tenders,	
Cost of oil and waste for passenger and baggage cars,	
Cost of oil and waste for freight cars,	
Loss and damage of goods,	
Loss and damage of baggage,	
Damages for injuries to persons,	
Damages to property, including fire and animals	
killed on road,	
Agents at stations, &c.,	

Clerks in general offices, &c.,.....	
Labor, loading and unloading freight,	
Porters and watchmen,.....	
Switchmen,	
Wood and water station attendance,.....	
Conductors, baggage-men and brakemen,.....	
Enginemen and firemen,.....	
For salaries of Trustees, President, Directors, Secretaries, Treasurer and Superintendent,..	
For printing, stationery and office expenses,.....	
For law expenses,.....	
Other expenses in detail as follows :	
Advertising, ...	
U. S. Government taxes,.....	
Interest,.....	
General expense not included above,.....	
Total,.....	

RECAPITULATION OF EXPENSES.

Maintaining roadway,.....	
Repairs of machinery,.....	
Operating,	
Proportion of expenses due to passenger business,	
Proportion of expenses due to freight business,..	
Total,.....	

TABLE H.

EARNINGS, RECEIPTS AND PAYMENTS.

Earnings and Receipts.

From passengers,.....	
From freight,.....	
From other sources, viz :	
Expresses,	
Mails,	
Rents,.....	
Total,.....	

Payments other than for Construction.

For transportation expenses, viz :

For passenger business,.....	
For freight business,.....	
For other business and what,.....	
For interest on funded debt,....	
For interest on floating debt,.....	
For dividends,.....	
For carried to surplus fund,.....	
For amount of surplus fund,.....	

VALUE OF MATERIAL ON HAND.

Wood, cords of,.....	
Coal, tons of,	
Oil, gallons of,.....	
Waste, pounds of,	
Iron rails, tons of, old,.....	
Iron rails, tons of, new,.....	
Chairs, pounds of,.....	
Spikes, pounds of,.....	
Ties, number of,.....	
Iron and other metals unwrought,	
Iron and other metals worked and partly worked,.	
Lumber,..	

Other items specified as follows :

COST OF TRANSPORTATION.

Actual cost of transporting freight, per ton, per mile,	
Actual cost of transporting passengers per mile,..	

VERMONT AND CANADA RAILROAD.

DETAILS OF EARNINGS FOR THE YEAR ENDING JUNE 30, 1867.

SOURCES.	JULY.	AUGUST.	SEPTEMBER.	OCTOBER.	NOVEMBER.	DECEMBER.
Through Passengers,.....
Way Passengers,.....
Through Freight,.....
Way Freight,.....
Express,.....
Transport of Mails,.....
Use of Engines,.....
Use of Cars,.....
Rent,.....
Total,.....

DETAILS OF EARNINGS, (*Continued.*)

SOURCES.	JANUARY.	FEBRUARY.	MARCH.	APRIL.	MAY.	JUNE.
Through Passengers,.....
Way Passengers,.....
Through Freight,.....
Way Freight,.....
Express,.....
Transport of Mails,.....
Use of Engines,.....
Use of Cars,.....
Rent,.....
Total,.....

TABLE I.

ACCIDENTS.

State the number of persons injured in life or limb, and the cause thereof, and whether passengers or persons employed.

State whether any such accidents have arisen from carelessness or negligence of any person in the employ of the corporation, and whether such person is retained in the service of the corporation.

	EMPLOYEES.		OTHERS.	
	Killed.	Injured.	Killed.	Injured.
Trains thrown from track,.....
Struck by bridge while on top of freight car,.....
Run over while walking on track,.....
Injured at road crossing,.....

Total number of persons killed,.....

Total number of persons injured but not killed,

In addition to which must be given a statement of the date of each accident, the place where it occurred, the train, the cause and the extent of the injuries inflicted upon each person, and the name of such person.

TABLE J.

NUMBER OF EMPLOYEES AND COMPENSATION ON VT. CENTRAL AND
VT. AND CANADA RAILROAD.

14 Conductors of passenger trains.	
Amount of compensation, (each per month,)	
36 Conductors of freight trains.	
Amount of compensation, (each per month,)	
3 Conductors of wood and gravel trains.	
Amount of compensation, (each per month,)	
1 Master mechanic.	
Amount of compensation, (per year,)	
2 Road Masters,	
Amount of compensation, (both per month,)	
286 Men in repair shops.	
Amount of compensation, (per day each,)	
13 Engine men of passenger trains.	
Amount of compensation, (each per day,)	
35 Engine men of freight trains.	
Amount of compensation, (each per day,)	
3 Engine men of wood and gravel trains.	
Amount of compensation, (each per day,)	
47 Firemen.	
Amount of compensation, (each per day,)	
14 Baggage-men.	
Amount of compensation, (each per month,)	
26 Switchmen.	
Amount of compensation, (each per month,)	
44 Section men (foreman,)	
Amount of compensation, (each per month,)	
250 Section hands.	
Amount of compensation, (each per day,)	
32 Watchmen.	
Amount of compensation, (each per month,)	

35 Station agents.

Amount of compensation, (each per year,).....

359 Other laborers.

Amount of compensation, (each per year,)

3 Clerks connected with passenger business.

Amount of compensation, (total per month,).....

11 Clerks connected with freight business.

Amount of compensation, (total per month,).....

Superintendent of bridges—salary, (per month,).

Wood agent, “ “

Other agents—how employed, and the salary of
each as follows, viz :

OFFICERS OF THE COMPANY.

W. C. SMITH, *President.*

JOHN W. NEWTON, *Clerk and Treasurer.*

EDWARD BLAKE, *Transfer Agent.*

SALARIES.

Trustees,.....	...
President,	\$1,000 00
Superintendent, .....	...
Clerk, Treasurer and Transfer Agent,.....	3,000 00
Interest paid to and including June 1, 1867,.....	\$180,000 00

STATE OF VERMONT, }
 COUNTY, SS. }

30th day of June, 1867.

I, John W. Newton, Treasurer of the Vermont and Canada Railroad Company, do solemnly swear that the above is a true statement of the condition of the finances of said company, their Trustees, or assignees, or lessees, on the 30th day of June, 1867.

JOHN W. NEWTON, *Treasurer.*

Sworn before me,

GUY C. NOBLE, *Master in Chancery.*

TABLE K.

Does any conductor or engineman in the service of your road indulge in intoxication?

Please answer the same question as applied to sectionmen, brakemen and switchmen.

Does your road, or any of its operatives, knowingly furnish facilities for traffic in intoxicating liquors in violation of the prohibitory law of the State?

Are the trains on your road allowed to stand across highway crossings to the unnecessary delay of the highway travel or business?

Do your stationmen allow boys without business to frequent and remain in or about the stations, platforms and trains?

Are your road fences and cattle guards sufficiently constructed and supported, and kept in repair as the law demands?

Are your trains jeopardized by neglect of persons to keep closed bars and gates at the various farm crossings? Answer in detail or with proximate particularity.

STATE OF VERMONT, }
FRANKLIN COUNTY, ss. }

We depose and say that the facts set forth, and statements made in the foregoing report which has been signed by us are true and correct according to the best of our knowledge, information and belief.

Signed,

Subscribed and sworn before me this day of

ANNUAL REPORT

OF THE

ATLANTIC & ST. LAWRENCE R. R. CO.,

FOR THE

YEAR ENDING JUNE 30, 1867.

TABLE A.

STOCK AND DEBTS.

Capital Stock authorized by charter,.....	\$4,000,000 00
The amount paid in is.. .. .	2,494,900 00
Funded debt,... .. .	3,472,000 00
Floating debt,.....	
Interest at rate of six per cent.	
The coupons are paid by the Lessees.	

TABLE B.

COST OF CONSTRUCTION.

Total cost of construction, as per last report, ..	\$6,788,354 72
Expended since last report, viz :	
For graduation and masonry,.....	
“ bridges,.....	
“ rails,.....	
“ chairs, spikes and ties,.....	
“ laying superstructure,.....	
“ buildings and fixtures,.....	
Two thirds of the road was built at a stipulated price per mile —consequently cannot be apportioned.	

TABLE C.

EQUIPMENT.

Total cost of equipment as per last report,..... \$865,734 76
 Expended since last report,..... .

Total cost of road and equipment,..... 7,654,089 48

TABLE D.

CHARACTERISTICS OF ROAD.

Length of road, within state of Vermont,..... $30\frac{1}{2}$ miles.
 “ “ completed, in Vermont,..... $30\frac{1}{2}$ “
 “ “ branches, do
 “ “ side tracks, do about $1\frac{1}{2}$ miles
 Weight of rail per yard, do 63 lbs.

CHARACTER AND LENGTH OF BRIDGING.

	No. of Structures.	No. of Spans.	Length of bridging in feet.
Pile bridging,.....	1	.....	1,519
Truss. bridging, 50 ft. span and under,.....	10	10	400
Truss. do., from 50 to 100 feet span,.....	2	2	120
Truss. do., from 100 to 150 feet span,.....	14	14	1,750
Truss. do., 150 ft. span and over, .....	.....	.....	.....
Stone arch bridges,... ..	2	2	120
Totals,	29	28	3,909

NOTE.—In addition to above bridges, there are on the line 16 iron bridges—3 of which are 300 feet each in length, in spans of 75 feet; one over the Connecticut River, one over Wild River and the other over Presumpscot River—whole length of above named bridges 1,510 feet.

Number of road crossings at grade,.....	63
Number of road crossings above and below grade,	6
Number of cross ties per mile,.....	2,400
Chairs, number per mile,.....	about 600
Whole number of switches on main track,.....	do 100

GRADIENTS AND ALIGNMENT.

Level number of miles and grades to 20 feet,....	97.27
Maximum grade,.....	60 feet.
Amount of straight line, miles,.....	89½
Amount of curved lines, miles,.....	59½
Maximum radius,.....	5,730 feet.
Minimum radius,.....	955 do
Sum of ascents going in one direction, ..	1,929 do
Sum of ascents going in opposite direction,.....	757 do
Height of termini and summit above tide water,.	1,178 do

The charter of the Atlantic & St. Lawrence Railroad within the State of Vermont extends from the Connecticut River at Bloomfield to the boundary line in Norton. But the entire line from Portland to Montreal is worked in divisions, of which the point of junction is at Island Pond, in Brighton.

BUILDINGS AND FIXTURES.

Passenger houses,	29
Freight houses,.....	22
Engine houses,....	8
Repair shops, ...	6
Water stations,.....	17
Dwellings,.....	2
Wood sheds,	26
Turn tables,	10
Changes during the last year,	

Other buildings, as follows :

Two Hotels.

Two store houses on wharves in Portland for Ocean and Boston steamers.

EQUIPMENT.

Number of locomotives owned by the Company on the 30th day of June, 1867 :

	Under 16 tons.	16 to 20.	20 to 25.	25 to 30.	30 tons and over.
In good repair,.....	6	18	6
Requiring slight repairs,.....	2	9	1
Requiring heavy repairs,.....	1	2	.....
Worn out,.....

Number of cars appropriated by the company, June

30th, 1867, to Portland line,..... 565

First class 8 wheel passenger cars in good repair,. 16

First class 8 wheel passenger cars wanting repair,. 1

2d class 8 wheel passenger cars in good repair,..

2d class 8 wheel passenger cars wanting repair,..

Baggage, express and mail cars in good repair,..

Baggage, express and mail cars wanting repair,.. 7

Covered freight and cattle 8 wheel cars in good

repair,20 Five

Covered freight and cattle 8 wheel cars wanting hundred

repair, and

Platform 8 wheel cars in good repair,..... forty

Other freight cars, one.

Gravel cars,

Average weight of passenger cars,

do do baggage do

do do box do

do do platform do

TABLE E.

BUSINESS OF THE YEAR 1866.

Miles run by passenger trains,	146,396
Miles run by freight trains, gravel and construction trains, and wood trains,	768,167
No. of through and way passengers carried in cars,	205,630
Number of passengers having passes,	
Number of miles traveled by way passengers,	
Average distance traveled by way passengers,	
Number of miles traveled by passengers having passes,	
Number of tons of through and way freight,	514,367
Number of tons of way freight carried 1 mile,	
Number of tons through freight moved towards market,	
Number of tons through freight moved from market,	
Number of tons of way through freight moved towards market,	
Number of tons of way through freight moved from market,	
Average rate of speed of ordinary passenger trains, per hour,	19 miles.
Average rate of speed of express trains,	
Average rate of speed of freight trains, per hour,	11 miles.
Rate of fare charged first class through passengers per mile,	about $2\frac{3}{4}$ cents
Rate of fare charged first class way passengers per mile,	do 3 do
Average rate of fare charged 2d class passengers per mile,	
Rate per ton per mile charged 1st class thro'freig't,	
Rate per ton per mile charged on 2d class through freight,	
Rate per ton per mile charged on 3d class through freight,	

Rate per ton per mile charged on 4th class through freight,.....	.....
Rate per ton per mile charg'd 1st class way freight,	
Rate per ton per mile charged on 2d class way freight, ..	.....
Rate per ton per mile charged on 3d class way freight,.....	.....
Rate per ton per mile charged on 4th class way freight, .....	.....
Total number of tons of freight carried one mile,.	
Total number of passengers carried one mile,....	

TABLE F.

EXPENSES OF MAINTAINING ROADWAY AND REAL ESTATE.

For the year ending June 30th, 1867.

Ordinary repairs of road bed and superstructure,.	
Extraordinary repairs of road bed,.....	
Cost of new rails used in repairs,.....	
Number and weight of chairs,.....	.....
Weight of spikes,.....	.....
Cost of repairs of rails,.....	.....
Number of cross ties used for renewals, .....	.....
Cost of same,.....	.....
Cost of relaying rails and ties,.....	.....
Insurance and taxes on real estate,.....	.....
Repairs of bridges, .....	.....
do stations,.....	.....
do fences, .....	.....
do depot furniture,....	.....
Total,.....	.....

Books so kept cannot be answered in detail.

COST OF REPAIRS OF MACHINERY.

Repairs of engines and tenders,	
Repairs of passenger and baggage cars,	
Repairs of freight cars,	
Repairs of tools and machinery in shops,	
Oil used about workshops and stations,	
Fuel used at shops, engine houses and stations,	
Waste used at shops,	
Other items in detail as follows :	

Total,

Books so kept cannot be answered in detail.

TABLE G.

COST OF OPERATING THE ROAD.

For the year ending June 30th, 1867.

Wood, including cost of preparing the same,	
Number of cords of wood used by locomotives,	
“ “ tons of coal “ “	
Number of cords of wood used at stations,	
“ “ tons of coal “ “	
Number of cords lost by fire,	
Number of gallons of oil,	
Number of pounds of waste,	
Cost of oil and waste for engines and tenders,	
Cost of oil and waste for passenger and baggage cars,	
Cost of oil and waste for freight cars,	
Loss and damage of goods,	
Loss and damage of baggage,	
Damages for injuries to persons,	
Damages to property, including fire and animals killed on road,	
Agents at stations, &c.,	

Clerks in general offices, &c.,.....	
Labor, loading and unloading freight,	
Porters and watchmen,.....	
Switchmen,	
Wood and water station attendance,.....	
Conductors, baggage-men and brakemen,.....	
Enginemen and firemen,.....	
For salaries of Trustees, President, Directors, Secretaries, Treasurer and Superintendent,..	
For printing, stationery and office expenses,.....	
For law expenses,.....	
Other expenses in detail as follows :	

Total,.....

Books not classified to conform to above table, answers cannot be given.

RECAPITULATION OF EXPENSES, YEAR 1866.

Maintaining roadway,.....	
Repairs of machinery,.....	
Operating,	
Proportion of expenses due to passenger business,	
Proportion of expenses due to freight business,..	

Total for year 1866,... \$874,195 74

TABLE H.

EARNINGS, RECEIPTS AND PAYMENTS.

Earnings and Receipts, for Year 1866.

From passengers,.....	\$257,282 97
From freight,.....	768,805 03
From other sources, viz :	
Expresses and Mails,.....	29,690 75
Rents, Miscellaneous, &c.,...	24,970 48

Payments other than for Construction.

For transportation expenses, viz :

For passenger business,.....	
For freight business,.....	
For other business and what,.....	
For interest on funded debt,....	
For interest on floating debt,.....	
For dividends, 1st January and 1st July at rate of per cent. per annum less Government tax,.	
For carried to surplus fund,.....	
For amount of surplus fund,.....	

VALUE OF MATERIAL ON HAND.

Wood, cords of,.....	
Coal, tons of,	
Oil, gallons of,.....	
Waste, pounds of,	
Iron rails, tons of, old,.....	
Iron rails, tons of, new,.....	
Chairs, pounds of,.....	
Spikes, pounds of,.....	
Ties, number of,.....	
Iron and other metals unwrought,	
Iron and other metals worked and partly worked,.	
Lumber,..	

Other items specified as follows :

COST OF TRANSPORTATION.

Actual cost of transporting freight, per ton, per mile,	
Actual cost of transporting passengers per mile,.	

DETAILS OF EARNINGS FOR THE YEAR ENDING JUNE 30, 1867.

SOURCES.	JULY.	AUGUST.	SEPTEMBER.	OCTOBER.	NOVEMBER.	DECEMBER.
Through Passengers,.....
Way Passengers,.....
Through Freight,....
Way Freight,.....
Express,.....
Transport of Mails,.....
Use of Engines,.....
Use of Cars,....
Rent,
Total,.....

Cannot be answered in detail.

DETAILS OF EARNINGS, (*Continued.*)

SOURCES.	JANUARY.	FEBRUARY.	MARCH.	APRIL.	MAY.	JUNE.
Through Passengers,.....
Way Passengers,.....
Through Freight,.....
Way Freight,.....
Express,.....
Transport of Mails,.....
Use of Engines,.....
Use of Cars,.....
Rent,.....
Total,.....

Cannot be answered in detail.

TABLE I.

ACCIDENTS.

State the number of persons injured in life or limb, and the cause thereof, and whether passengers or persons employed.

State whether any such accidents have arisen from carelessness or negligence of any person in the employ of the corporation, and whether such person is retained in the service of the corporation.

	EMPLOYEES.		OTHERS.	
	Killed.	Injured.	Killed.	Injured.
Trains thrown from track,.....
Struck by bridge while on top of freight car,.....
Run over while walking on track,.....
Injured at road crossing,.....

Total number of persons killed,.....

Total number of persons injured but not killed,

In addition to which must be given a statement of the date of each accident, the place where it occurred, the train, the cause and the extent of the injuries inflicted upon each person, and the name of such person.

TABLE J.

NUMBER OF EMPLOYEES AND COMPENSATION.

Conductors of passenger trains.

Amount of compensation, (each per month,).....

Conductors of freight trains.

Amount of compensation, (each per month,).....

Conductors of wood and gravel trains.

Amount of compensation, (each per month,)... ..

Master mechanic.

Amount of compensation, (per year,)

Road Masters,

Amount of compensation, (both per month,)

Men in repair shops.

Amount of compensation, (per day each,)

Engine men of passenger trains.

Amount of compensation, (each per day,)

Engine men of freight trains.

Amount of compensation, (each per day,)

Engine men of wood and gravel trains.

Amount of compensation, (each per day,)

Firemen.

Amount of compensation, (each per day,)

Baggagemen.

Amount of compensation, (each per month,)

Switchmen.

Amount of compensation, (each per month,)

Section men (foreman,)

Amount of compensation, (each per month,)

Section hands.

Amount of compensation, (each per day,)

Watchmen.

Amount of compensation, (each per month,)

Station agents.

Amount of compensation, (each per year,)

Other laborers.

Amount of compensation, (each per year,)

Clerks connected with passenger business.

Amount of compensation, (total per month,)

Clerks connected with freight business.

Amount of compensation, (total per month,)

Superintendent of bridges—salary, (per month,) .

Wood agent,

“

“

Other agents—how employed, and the salary of
each as follows, viz:

The employees of the company, being employed upon two

divisions of the road, one South of Island Pond extending to Portland, the other North of Island Pond extending to the boundary line and Montreal, no specific statement can be made under the head of enquiry, as applied to the State of Vermont.

Lessees, Grand Trunk Railway Co. of Canada.

OFFICERS OF THE COMPANY.

ST. JOHN SMITH, *President*.

HENRY BAILEY, *Superintendent*.

CHAS. E. BARRETT, *Treasurer*.

SALARIES.

Trustees,
 President,
 Superintendent,
 Treasurer,
 Transfer Agent,

The earnings of the road are received by the Lessees. The Lessors have no interest in them while the rent is paid, and the accounts are so kept by the Grand Trunk Railway Co. as not to admit of an answer to the above as contemplated.

STATE OF MAINE, }

day of August, 1867.

CUMBERLAND COUNTY, SS. }

I, C. E. Barrett, Treasurer of the Atlantic and St. Lawrence Railroad Company, do solemnly swear that the above is a true statement of the condition of the finances of said company, their Trustees, or assignees, or lessees, on the 30th day of June, 186 .

Treasurer.

Sworn before me,

Justice of the Peace.

TABLE K.

Does any conductor or engineman in the service of your road indulge in intoxication?

Not to our knowledge.

Please answer the same question as applied to sectionmen, brakemen and switchmen.

There is a rule of the company subjecting an employee to discharge for intoxication.

Does your road, or any of its operatives, knowingly furnish facilities for traffic in intoxicating liquors in violation of the prohibitory law of the State?

Not to our knowledge.

Are the trains on your road allowed to stand across highway crossings to the unnecessary delay of the highway travel or business?

No.

Do your stationmen allow boys without business to frequent and remain in or about the stations, platforms and trains?

Not allowed, to our knowledge. There is but one station in Vermont which is much frequented by any persons.

Are your road fences and cattle guards sufficiently constructed and supported, and kept in repair as the law demands?

Fences are believed to be in usual good condition. Much the largest part of the line in Vermont is through forest land.

Are your trains jeopardized by neglect of persons to keep closed bars and gates at the various farm crossings? Answer in detail or with proximate particularity.

STATE OF MAINE, }
CUMBERLAND COUNTY, ss. } Portland, Aug. 5th, 1867.

The subscribers, a majority of the Directors of the Atlantic and St. Lawrence Railroad Company, depose and say that the facts set forth, and statements made in the foregoing report which has been signed by us, are true and correct according to the best of our knowledge, information and belief.

Signed,

J. B. BROWN,
BYRON GREENOUGH,
CHAS. E. BARRETT,
P. BARNES,
H. J. LIBBY.

Subscribed and sworn before me this day of

The Directors of the Atlantic and St. Lawrence Railroad Company, in presenting this their annual report, beg leave to offer a copy of their statement made to you the last year, which is as follows :—

The Directors of the Atlantic and St. Lawrence Railroad Company, in making their answers to the foregoing questions, beg to state, that, their road being under lease to the Grand Trunk Railway Company of Canada as set forth in a former report, and being under the exclusive management of that company, their information upon which this statement is based, is derived partly from their own records and files, but principally from the officers and agents of the company at Portland, and from their books and accounts there kept; and though they are not personally cognizant of many of the facts, and state them from information thus derived, and not as wholly within their own knowledge they have no doubt of the correctness of the statements made. As the books of the company have been from the first and are now kept in a method to conform to the requirements of another jurisdiction, very many of the questions in the tables cannot be answered; and such as are answered must apply to the whole line of road from Portland to the line of Vermont. Answers are, however, given as fully as the information in their possession will admit of.

The same course substantially has been adopted by the Directors in their report to the Vermont authorities during the time their road has been under lease, and they trust that in this case it will be satisfactory. They have no doubt, if more explicit answers are required upon any matter it will be readily furnished by the Lessees of the road, as far as it is in their power so to do, intimation to that effect being given to them, or to the authorized agents or officers on the line.

ANNUAL REPORT

OF THE

VERMONT & MASSACHUSETTS R. R. CO.

FOR THE

YEAR ENDING JUNE 30, 1867.

TABLE A.

STOCK AND DEBTS.

Capital Stock, including the Greenfield branch and Vermont portion,.....	\$3,200,000 00
Total amount of capital stock paid in,...	2,830,000 00
Number of shares of capital stock issued,.....	28,600
Funded debt, redeemable July 1st, 1883,...	550,000 00
Floating debt,.....	8,681 56
Rate of interest paid on debt, six per cent.	
Interest coupons have always been promptly paid when due.	

TABLE B.

COST OF CONSTRUCTION.

Total cost of construction, as per last report, ..	\$3,240,779 31
Expended since last report, viz :	
For graduation and masonry,.....	
“ bridges,.....	
“ rails,.....	
“ chairs, spikes and ties,.....	
“ laying superstructure,.....	
“ buildings and fixtures,.....	

TABLE C.

EQUIPMENT.

Total cost of equipment as per last report,..... \$225,650 20
 Expended since last report,..... .

Total cost of road and equipment,..... 3,466,429 51

TABLE D.

CHARACTERISTICS OF ROAD.

Length of road,..... 69 miles.
 “ “ completed,..... 69 “
 “ “ branches,..... 8 “
 “ “ side tracks,..... 5½ “
 Weight of rail per yard,..... 56 lbs.
 Within this state 10 miles and 1661 ft.; side tracks, 1 mile.

CHARACTER AND LENGTH OF BRIDGING.

	No. of Structures.	No. of Spans.	Length of bridging in feet.
Trustle bridging, .....	.....	.....	.....
Truss. bridging, 50 ft. span and under,	11	11	.....
Truss. do., from 50 to 100 feet span,	5	5	.....
Truss. do., from 100 to 150 feet span,	3	4	.....
Truss. do., 150 ft. span and over,	23	41	.....
Stone arch bridges, .....	.....	..	.....
Totals,	42	61	.....

Number of road crossings at grade,..... 62
 Number of road crossings above and below grade, .. 21
 Number of cross ties per mile,..... 2,350
 Chairs, number per mile,..... 530
 Whole number of switches on main track,..... 60

GRADIENTS AND ALIGNMENT.

Level number of miles,	
Maximum grade,	58 feet.
Amount of straight line, miles,	31 ¹³ ₂₀
Amount of curved lines, miles,	45 ⁷ ₁₀
Maximum radius,	
Minimum radius,	
Sum of ascents going in one direction,	
Sum of ascents going in opposite direction,	
Height of termini and summit above tide water,	

BUILDINGS AND FIXTURES.

Passenger houses,	19
Freight houses,	20
Engine houses,	5
Repair shops,	4
Water stations,	9
Dwellings,	8
Wood sheds,	19
Turn tables,	5
Changes during the last year,	
Other buildings, as follows :	
General office, Fitchburgh,	1

EQUIPMENT.

Number of locomotives owned by the Company on the 30th day of June, 1867 :

	Under 16 tons.	16 to 20.	20 to 25.	25 to 30.	30 tons and over.
In good repair,	9	.....	1
Requiring slight repairs,	2	.....	.....
Requiring heavy repairs,
Worn out,

Number of cars owned by the company, June 30th,

1867,	196
First class 8 wheel passenger cars in good repair, .	9
First class 8 wheel passenger cars wanting repair, .	1
2d class 8 wheel passenger cars in good repair, ..	0
2d class 8 wheel passenger cars wanting repair, ..	0
Baggage, express and mail cars in good repair, ..	5
Baggage, express and mail cars wanting repair, ..	1
Covered freight and cattle 8 wheel cars in good repair,	92
Covered freight and cattle 8 wheel cars wanting repair,	2
Platform 8 wheel cars in good repair,	70
Other freight cars, platform, wanting repair,	8
Gravel cars,	2
Snow plows cars,	6
Chair cars,	6
Average weight of passenger cars,	12½ tons.
do do baggage do	11½ "
do do box do	8 "
do do platform do	6½ "

TABLE E.

BUSINESS OF THE YEAR ENDING NOV. 30, 1886.

Miles run by passenger trains,	90,055
Miles run by freight trains,	63,101
Miles run by gravel and construction trains, and wood trains,	1,965
No. of through passengers carried in cars,	104,441
No. of way passengers,	88,055
Number of passengers having passes,	745
Number of miles traveled by way passengers,	1,531,876
Average distance traveled by way passengers,	17½ miles.
No. of miles traveled by passengers having passes, . .	19,790 “
Number of tons of through freight,	109,416 ⁶³³ / ₁₀₀₀
Number of tons of way freight,	36,528 ⁴⁹⁷ / ₁₀₀₀
Number of tons of way freight carried 1 mile,	700,494 ¹⁴⁹ / ₁₀₀₀
No. of tons through freight moved towards market, . .	80,489 ⁹³⁷ / ₁₀₀₀
Number of tons through freight moved from market, . .	28,926 ⁶³⁶ / ₁₀₀₀
Number of tons of way through freight moved towards and from market,	36,528 ⁴⁹⁷ / ₁₀₀₀
Average rate of speed of ordinary passenger trains, per hour,	22 miles.
Average rate of speed of express trains,	
Average rate of speed of freight trains, per hour, . .	10 miles.
Rate of fare charged first class through passengers per mile,	3½ cents.
Rate of fare charged first class way passengers per mile,	4 do
Average rate of fare charged 2d class passengers per mile,	
Rate per ton per mile charged 1st class thro'freig't, . .	7 cents.
Rate per ton per mile charged on 2d class through freight,	6 “
Rate per ton per mile charged on 3d class through freight,	5 “

Rate per ton per mile charged on 4th class through freight,	4 cents.
Rate per ton per mile charg'd 1st class way freight,	12 "
Rate per ton per mile charged on 2d class way freight,	9 "
Rate per ton per mile charged on 3d class way freight,	8 "
Rate per ton per mile charged on 4th class way freight,	5 "
Total number of tons of freight carried one mile,	2,755,329 ⁸¹⁰ / ₁₀₀₀
Total number of passengers carried one mile,	3,735,928

TABLE F.

EXPENSES OF MAINTAINING ROADWAY AND REAL ESTATE.

For the year ending June 30th, 186 .

Ordinary repairs of road bed and superstructure,	\$42,150 31
Extraordinary repairs of road bed,	
Cost of new rails used in repairs,	62,815 40
Number and weight of chairs,	In-
Weight of spikes,	
Cost of repairs of rails,	cluded
Number of cross ties used for renewals,	in
Cost of same,	
Cost of relaying rails and ties,	above.
Insurance and taxes on real estate,	2,788 29
Repairs of bridges,	5,665 87
do stations,	20,408 20
do fences,	1,814 74
do depot furniture,	
Total,	\$135,672 81

COST OF REPAIRS OF MACHINERY.

Repairs of engines and tenders,	\$42,515 41
Repairs of passenger and baggage cars,.....	21,076 09
Repairs of freight cars,	26,091 35
Repairs of tools and machinery in shops,... ..	5,340 02
Oil used about workshops,.....	Stated
Fuel,.....	else-
Waste,.....	where.
Other items in detail as follows :	
Repairs of gravel and hand cars,.....	771 01
Total,.....	<u>\$95,793 88</u>

TABLE G.

COST OF OPERATING THE ROAD.

For the year ending June 30th, 186 .

Wood, including cost of preparing the same,.. .	\$36,042 63
Number of cords of wood used by locomotives,..	
“ “ tons of coal “ “ ..	
Number of cords of wood used at stations,....	
“ “ tons of coal “ “	
Number of cords lost by fire,.....	0
Number of gallons of oil,.....	
Number of pounds of waste,.....	
Cost of oil and waste for engines and tenders, pas-	
senger and baggage cars, and freight cars,....	4,959 28
Loss and damage of goods, baggage, injuries to	
persons, property, including fire and animals	
killed on road,.....	1,137 01
Expenses of passenger department,....	23,104 01
Expenses of freight department,.....	31,806 49
Labor, loading and unloading freight,	
Porters, watchmen, and switchmen,.....	6,347 04
Wood and water station expense,.....	3,545 69

Conductors and baggage-men,.....	Included in
Brakemen,.....	freight and
Enginemen and firemen,.....	pass. dep'ts.
For salaries of President, Directors, Secretaries,	
Treasurer, Superintendent, and Clerks,... ..	7,841 66
For printing, stationery and advertising,.....	999 82
For law expenses,.....	1,318 80
Other expenses in detail as follows :	
Clearing snow,	832 87
Rent to Connecticut River Railroad Company,...	1,500 00
Incidental expenses,.....	6,512 79
U. S. and state taxes,.....	21,455 96
	<hr/>
Total,.....	\$147,404 05

RECAPITULATION OF EXPENSES.

Maintaining roadway,.....	\$135,672 81
Repairs of machinery,.....	95,793 88
Operating,	147,404 05
Proportion of expenses due to passenger business,	
Proportion of expenses due to freight business,...	
	<hr/>
Total,... ..	\$378,870 74

TABLE H.

EARNINGS, RECEIPTS AND PAYMENTS.

Earnings and Receipts.

From passengers,.....	\$154,875 17
From freight,	243,468 44
From other sources, viz :	
Tolls,.....	889 00
Expresses,.....	7,162 51
Mails,.....	6,000 00
Rents,.....	43,061 64
Miscellaneous,....	489 42
	<hr/>
	\$455,946 18

Payments other than for Construction.

For transportation expenses, viz :

For passenger, freight, and other business,.....	\$378,870 74
For interest on funded debt,....	28,887 54
For interest on floating debt,.....	
For dividends, Nos. 2 and 3 of $1\frac{1}{2}$ per cent. each, and U. S. tax,.....	90,315 80
For carried to surplus fund,.....	
For amount of surplus fund,.....	

VALUE OF MATERIAL ON HAND.

Wood, cords of,.....	\$27,976 25
Coal, tons of,	
Oil, gallons of, and waste, pounds of,.....	947 00
Iron rails, tons of, old, 300, at \$40 per ton,	12,000 00
Iron rails, tons of, new,.....	
Chairs, pounds of,.....	
Spikes, pounds of,.....	
Ties, number of,.....	
Iron and other metals unwrought, worked and partly worked, lumber,...	30,320 88
Other items specified as follows :	

COST OF TRANSPORTATION.

Actual cost of transporting freight, per ton, per mile,	
Actual cost of transporting passengers per mile,..	

DETAILS OF EARNINGS FOR THE YEAR ENDING JUNE 30, 1867.

SOURCES.	JULY.	AUGUST.	SEPTEMBER.	OCTOBER.	NOVEMBER.	DECEMBER.
Through Passengers,.....	13,824 12	17,238 68	24,346 88	14,334 46	12,103 30	10,207 66
Way Passengers,.....						
Through Freight,.....	16,978 55	20,043 64	18,951 97	22,755 65	21,297 02	17,544 85
Way Freight,.....						
Express,.....	499 62	470 62	458 37	461 52	510 55	576 90
Transport of Mails,.....	500 00	500 00	500 00	500 00	500 00	500 00
Tolls,.....	68 50	87 75	89 00	115 00	63 50	64 00
Miscellaneous,.....	52 80	19 12	34 05	39 51	27 40	22 86
Rent,.....	3,636 75	3,262 67	3,760 00	3,686 75	3,292 50	3,805 00
Total,.....	35,560 34	41,622 48	48,140 27	41,892 89	37,794 27	32,721 27

DETAILS OF EARNINGS, (Continued.)

SOURCES.	JANUARY.	FEBRUARY.	MARCH.	APRIL.	MAY.	JUNE.
Through Passengers,..... }	7,986 62	8,852 03	11,191 25	12,328 59	10,308 50	12,153 07
Way Passengers,..... }						
Through Freight,..... }	18,762 84	19,401 62	21,704 47	22,946 29	22,937 59	20,143 95
Way Freight,..... }						
Express,	519 93	534 61	765 39	765 00	800 00	800 00
Transport of Mails,.....	500 00	500 00	500 00	500 00	500 00	500 00
Tolls,	60 50	44 50	59 00	85 00	72 25	80 00
Miscellaneous,.....	46 00	36 18	78 66	49 29	55 23	28 32
Rent,	3,796 55	3,263 17	3,750 00	3,730 25	3,253 00	3,825 00
Total,.....	31,672 45	32,632 11	38,048 77	40,404 42	37,926 57	37,530 34

TABLE I.

ACCIDENTS.

State the number of persons injured in life or limb, and the cause thereof, and whether passengers or persons employed.

State whether any such accidents have arisen from carelessness or negligence of any person in the employ of the corporation, and whether such person is retained in the service of the corporation.

	EMPLOYEES.		OTHERS.	
	Killed.	Injured.	Killed.	Injured.
Trains thrown from track,.....
Struck by bridge while on top of freight car,.....
Run over while walking on track,.....
Injured at road crossing,.....

Total number of persons killed,..... 1

Total number of persons injured but not killed,

In addition to which must be given a statement of the date of each accident, the place where it occurred, the train, the cause and the extent of the injuries inflicted upon each person, and the name of such person.

OCTOBER 18th, 1866.—R. M. Huntley, switchman, was run over and killed by freight train of Cheshire Railroad backing down through the bridge in the yard at Fitchburg. No blame attached to train men.

TABLE J.

NUMBER OF EMPLOYEES AND COMPENSATION PER JUNE ROLL, '67.

4 Conductors of passenger trains.	
Amount of compensation, (each per month,)	\$65 to 75
4 Conductors of freight trains.	
Amount of compensation, (each per month,)	50 to 75
Conductors of wood and gravel trains.	
Amount of compensation, (each per month,)	
1 Master mechanic.	
Amount of compensation, (per year,)	1,380 00
Road Masters,	
Amount of compensation, (both per month,)	
48 Men in repair shops.	
Amount of compensation, (per day each,)	1 50 to 3 25
5 Engine men of passenger trains.	
Amount of compensation, (each per month,)	70 to 90
4 Engine men of freight trains.	
Amount of compensation, (each per month,)	55 to 75
Engine men of wood and gravel trains.	
Amount of compensation, (each per day,)	
10 Firemen.	
Amount of compensation, (each per month,)	45
4 Baggage men.	
Amount of compensation, (each per month,)	50 to 60
5 Switchmen.	
Amount of compensation, (each per month,)	40 to 50
9 Section men (foreman,)	
Amount of compensation, (each per day,)	2 00 to 2 25
64 Section hands.	
Amount of compensation, (each per day,)	1 50 to 1 60
6 Watchmen.	
Amount of compensation, (each per month,)	45 to 50
19 Station agents.	
Amount of compensation, (each per month,)	15 to 90

4 Other laborers.

Amount of compensation, (each per day,)..... 1 50

1 Clerk connected with passenger business.

Amount of compensation, (total per month,)..... 75

2 Clerks connected with freight business.

Amount of compensation, (total per month,)..... 60 to 120

Superintendent of bridges, wood agent and road
master,—one person. (per month,)... .. 150

Other agents—how employed, and the salary of
each as follows, viz:

1 Master carpenter, (per month,)... .. 80

5 Bridge and building carpenters, (per day,)..... 2 50 to 3 00

1 Porter, (per day,)..... 1 55

2 Flagmen, (per month,).. ... 30 to 42 50

8 Station agents, laborers and clerks, (per month,) 40 to 50

2 Telegraph operators, (per month,)..... 8 33 to 10 00

2 Freight receiptors, (per month,)..... 12 50 to 65 00

1 Car examiner, (per day,)..... 2 50

2 Brakemen on passenger trains, (per month,)... 45

1 Toll collector, (per month,)..... 20

11 Toll collectors freight trains, (per month,).... 45

OFFICERS OF THE COMPANY.

J. C. HOWE, WM. MINOT, JR., JAS. MCGREGOR, *Trustees.*

DANIEL S. RICHARDSON, *President.*

OTIS T. RUGGLES, *Superintendent.*

FRANKLIN N. POOR, *Treasurer.*

B. D. LOCKE, *Clerk.*

SALARIES.

Trustees,.....\$300, 100, 100

President, 1,500

Superintendent, 2,500

Treasurer, 2,500

Clerk, 50

Amount of net earnings for one year ending June	
30th, 1866,	\$140,924 50
Amount of net earnings for one year ending June	
30th, 1867,	77'075 44
Am't of funded and floating debt June 30th, 1867,	558,681 56
Amount of cash in Treasury,	\$4,810 69
Amount of Loans,	34,550 00
\$35,400 bonds Vt. & Mass. R. R. Co.,	33,719 66
	————— 33,080 35
Amount of interest paid during 12 months ending	
June 30, 1867, more than received for Loans, ..	28,887 54

COMMONWEALTH OF MASS.,)
 Suffolk County, ss.) 27th day of August, 1867.

I, Franklin N. Poor, Treasurer of the Vermont & Massachusetts Railroad Company, do solemnly swear that the above is a true statement of the condition of the finances of said company, their Trustees, or assignees, or lessees, on the 30th day of June, 1867.

FRANKLIN N. POOR, *Treasurer.*

Sworn before me,

P. E. TESCHEMACHER, *Justice of the Peace.*

TABLE K.

Does any conductor or engineman in the service of your road indulge in intoxication?

No.

Please answer the same question as applied to sectionmen, brakemen and switchmen.

No.

Does your road, or any of its operatives, knowingly furnish facilities for traffic in intoxicating liquors in violation of the prohibitory law of the State?

No.

Are the trains on your road allowed to stand across highway

crossings to the unnecessary delay of the highway travel or business?

No.

Do your stationmen allow boys without business to frequent and remain in or about the stations, platforms and trains?

No.

Are your road fences and cattle guards sufficiently constructed and supported, and kept in repair as the law demands?

Yes.

Are your trains jeopardized by neglect of persons to keep closed bars and gates at the various farm crossings? Answer in detail or with proximate particularity.

No.

COMMONWEALTH OF MASSACHUSETTS, }
Suffolk County, ss. }

We depose and say that the facts set forth, and statements made in the foregoing report which has been signed by us, are true and correct according to the best of our knowledge, information and belief.

Signed, DANIEL S. RICHARDSON, } Directors
JAS. A. DUPEE, } of the
GEO. F. FAY, } Vt. and Mass.
F. GOODHUE, } R. R. Co.

SUFFOLK, ss.—Subscribed and sworn before me this 27th day of August, 1867, by D. S. Richardson and Jas. A. Dupée.

F. A. BROOKS, *Justice of the Peace.*

WORCESTER, ss.—Subscribed and sworn before me this 28th day of August, 1867, by George F. Fay.

HENRY A. WILLIS, *Justice of the Peace.*

STATE OF VERMONT, } Brattleboro, Sept. 4th, 1867.

WINDHAM COUNTY, ss. } Personally appeared Francis Goodhue, and made oath that the foregoing report by him subscribed is true. Before me, R. W. CLARKE, *Notary Public.*

ANNUAL REPORT

OF THE

CONN. & PASSUMPSIC RIVERS R. R. CO.

FOR THE

YEAR ENDING MAY 31, 1867.

TABLE A.

STOCK AND DEBTS.

Preferred stock,.....	\$1,822,100 00
Old stock,.....	139,000 00
Special Stock,.....	700 00
Bonded Debt, 6 per cent.,.....	573,800 00
Coupon Notes, 7 per cent.,.....	200,000 00
Floating Indebtedness,.....	99,170 13
Coupons uncalled for,.....	1,601 50

TABLE B.

COST OF CONSTRUCTION.

Total cost of construction, as per last report, ..	\$2,845,733 41
Expended since last report, viz:.....	43,929 76
For graduation and masonry,.....	
“ bridges,.....	
“ rails,.....	
“ chairs, spikes and ties,.....	
“ laying superstructure,.....	
“ buildings and fixtures,.....	

Road completed to Derby (Canada Line) and commenced operating May 1st, 1867.

TABLE C.

EQUIPMENT.

Total cost of equipment as per last report,.....	\$289,040 52
Expended since last report,.....	52,160 00
Total cost of road and equipment,.....	341,200 52

TABLE D.

CHARACTERISTICS OF ROAD.

Length of road,.....	110 miles.
“ “ completed,.....	110 “
“ “ branches,.....	
“ “ side tracks,.....	11 “
Weight of rail per yard,.....	56 lbs.

CHARACTER AND LENGTH OF BRIDGING.

	No. of Structures.	No. of Spans.	Length of bridging in feet.
Trussle bridging,.....	11	.....	2,050
Truss. bridging, 50 ft. span and under,.....	13	18	769
Truss. do., from 50 to 100 feet span,.....	7	10	666
Truss. do., from 100 to 150 feet span,.....	5	5	580
Truss. do., 150 ft. span and over,	11	14	2,014
Stone arch bridges,...	.....	..	.....
Totals,	47	47	6,079

Number of road crossings at grade,.....	74
Number of road crossings above and below grade,	6
Number of cross ties per mile,.....	2,000
Chairs, number per mile, cast iron,...	680
Howe Chairs, per mile,.....	440
Whole number of switches on main track,.....	80

GRADIENTS AND ALIGNMENT.

Level number of miles,	
Maximum grade,	52 $\frac{8}{10}$ feet.
Amount of straight line, miles,	
Amount of curved lines, miles,	
Maximum radius,	
Minimum radius,	
Sum of ascents going in one direction,	
Sum of ascents going in opposite direction,	
Height of termini and summit above tide water, .	

BUILDINGS AND FIXTURES.

Passenger houses,	4
Freight houses,	20
Engine houses,	3
Repair shops,	2
Water stations,	13
Dwellings,	10
Wood sheds,	22
Turn tables,	2
Changes during the last year,	
Other buildings, as follows :	
Car Houses,	4
Building used as a store and general office,	1

EQUIPMENT.

Number of locomotives owned by the Company on the 31st day of May, 1867 :

	Under 16 tons.	16 to 20.	20 to 25.	25 to 30.	30 tons and over.
In good repair,.....	1	6	3	.....
Requiring slight repairs,.....
Requiring heavy repairs,.....	1	.....	.....
Worn out,.....

Number of cars owned by the company, May 31st,

1867,.....	332
First class 8 wheel passenger cars in good repair,.	8
First class 8 wheel passenger cars wanting repair,.	2
2d class 8 wheel passenger cars in good repair,..	
2d class 8 wheel passenger cars wanting repair,..	
Baggage, express and mail cars in good repair,..	4
Baggage, express and mail cars wanting repair,..	2
Covered freight and cattle 8 wheel cars in good repair,	148
Covered freight and cattle 8 wheel cars wanting repair,	52
Platform 8 wheel cars in good repair,.....	80
Other freight cars, hand cars,.....	26
Gravel cars,	10
Average weight of passenger cars,	24,000 lbs.
do do baggage do	17,000 lbs.
do do box do	14,000 lbs.
do do platform do	12,778 lbs.

TABLE E.

BUSINESS OF THE YEAR.

Miles run by passenger trains,	78,492
Miles run by freight trains,	71,432
Miles run by gravel and construction trains, and wood trains,	21,566
No. of through and way passengers carried in cars,	109,364
Number of passengers having passes,	
Number of miles traveled by way passengers,	
Average distance traveled by way passengers,	
No. of miles traveled by passengers having passes,	
Number of tons of through and way freight,	75,856
Number of tons of freight carried 1 mile,	3,823,306
No. of tons through freight moved towards market,	
Number of tons through freight moved from market,	
Number of tons of way through freight moved towards and from market,	
Average rate of speed of ordinary passenger trains, per hour,	23 miles.
Average rate of speed of express trains,	
Average rate of speed of freight trains, per hour,	12 miles.
Rate of fare charged first class through passengers per mile,	3 $\frac{1}{4}$ cents.
Rate of fare charged first class way passengers per mile,	4 do
Average rate of fare charged 2d class passengers per mile,	
Rate per ton per mile charged 1st class thro'freig't,	5 $\frac{2}{3}$ cents.
Rate per ton per mile charged on 2d class through freight,	4 $\frac{1}{2}$ "
Rate per ton per mile charged on 3d class through freight,	3 $\frac{7}{8}$ "
Rate per ton per mile charged on 4th class through freight,	3 $\frac{1}{3}$ "

Rate per ton per mile charg'd 1st class way freight,	8 $\frac{2}{3}$ cents.
Rate per ton per mile charged on 2d class way freight,	7 "
Rate per ton per mile charged on 3d class way freight,.....	
Rate per ton per mile charged on 4th class way freight,	
Total number of passengers carried one mile,....	3,486,989

TABLE F.

EXPENSES OF MAINTAINING ROADWAY AND REAL ESTATE.

For the year ending June 30th, 186 .

Ordinary repairs of road bed and superstructure,.	\$86,585 69
Extraordinary repairs of road bed,.....	Included
Cost of new rails used in repairs,.....	in above.
Number and weight of chairs,.....	
Weight of spikes,.....	
Cost of repairs of rails,.....	
Number of cross ties used for renewals,	43,161
Cost of same,.....	20 to 28c. each
Cost of relaying rails and ties,.....	included above
Insurance on real estate,	1,605 17
Repairs of bridges,	2,138 79
do stations,.....	5,622 98
do fences,	included above
do masonry,.....	do
Total,.....	\$95,952 63

COST OF REPAIRS OF MACHINERY.

Repairs of engines and tenders,.....	\$79,791 52
Repairs of passenger and baggage cars,.....	included above
Repairs of freight cars,.....	do
Repairs of tools and machinery in shops,... ..	27,395 38
Oil used about workshops,.....	
Fuel,.....	
Waste,.....	
Other items in detail as follows :	
Total,.....	\$107,186 90

TABLE G.

COST OF OPERATING THE ROAD.

For the year ending June 30th, 186 .

Wood, including cost of preparing the same,.. ..	\$38,498 01
Number of cords of wood used by locomotives,..	
“ “ tons of coal “ “ ..	
Number of cords of wood used at stations,.....	
“ “ tons of coal “ “ ..	
Number of cords lost by fire,.....	
Number of gallons of oil,.....	
Number of pounds of waste,.....	
Cost of oil and waste for engines and tenders, pas- senger and baggage cars, and freight cars,....	6,497 68
Loss and damage of goods, baggage, injuries to persons, property, including fire and animals killed on road,.....	2,974 11
Expenses of passenger department, labor, loading and unloading freight,	24,737 02
Porters, watchmen, and switchmen,.....	
Wood and water station expense,.....	
Conductors and baggage-men, brakemen, engine- men and firemen,.....	14,823 43

For salaries of Trustees, President, Directors, Secretaries, Treasurer, Superintendent, and Clerks,	18,383 52
For printing, stationery and office expenses,....included above	
For law expenses,.....	do
Other expenses in detail as follows :	

Total,.....	<u>\$105,913 77</u>
-------------	---------------------

RECAPITULATION OF EXPENSES.

Maintaining roadway,.....	\$95,952 63
Repairs of machinery,.....	107,186 90
Operating,	105,913 77
Proportion of expenses due to passenger business,	
Proportion of expenses due to freight business,..	
Total,... ..	<u>\$309,053 30</u>

TABLE H.

EARNINGS, RECEIPTS AND PAYMENTS.

Earnings and Receipts.

From passengers,.....	\$175,952 27
From freight,	301,533 76
From other sources, viz :	
Expresses,.....	4,200 00
Mails,.....	10,600 00
Rents,.....	1,419 83
Miscellaneous,....	

Payments other than for Construction.

For transportation expenses, viz :

For passenger business,.....	\$14,823 43
For freight business,.....	24,737 02
Fuel, repairs and general expenses,.....	269,492 85
For interest on funded debt,....	
For interest on floating debt,.....	
For dividends, ..	
For carried to surplus fund,.....	
For amount of surplus fund,.....	

VALUE OF MATERIAL ON HAND.

Wood, 11,023 cords of,.....	\$25,620 50
Coal, 154 tons of,	2,142 00
Oil, 614 gallons of,...	465 60
Waste, 290 pounds of,.....	39 35
Iron rails, 195 tons of, old,.....	9,675 00
Iron rails, 1½ tons of, new,.....	135 00
Chairs, 585 pounds of,.....	40 95
Spikes, 6,750 pounds of,.....	421 87
Ties, number of, 3,300.....	825 00
Iron and other metals unwrought, 57 tons,.....	6,991 31
Iron and other metals worked and partly worked, 18 tons,..	2,999 15
Lumber, 250 M.,.....	8,200 00
Other items specified as follows :—Sundries...	43,083 95

COST OF TRANSPORTATION.

Actual cost of transporting freight, per ton, per
mile,

Actual cost of transporting passengers per mile, ..

CONN. & PASSUMPSIC RIVERS RAILROAD.

DETAILS OF EARNINGS FOR THE YEAR ENDING JUNE 30, 186 .

SOURCES.	JULY.	AUGUST.	SEPTEMBER.	OCTOBER.	NOVEMBER.	DECEMBER.
Through Passengers,.....	22,020 76	25,731 03	23,912 02	17,648 81	14,694 81	12,762 05
Way Passengers,.....						
Through Freight,.....	23,714 94	28,068 90	26,065 98	33,957 61	32,040 67	27,459 20
Way Freight,.....						
Express,.....	350 00	350 00	350 00	350 00	350 00	350 00
Transport of Mails,.....	883 33	883 33	883 33	883 33	883 33	883 33
Use of Engines,.....						
Use of Cars,.....						
Rent,.....						
Total,.....						

DETAILS OF EARNINGS, (Continued.)

SOURCES.	JANUARY.	FEBRUARY.	MARCH.	APRIL.	MAY.	JUNE.
Through Passengers,..... }	10,288 86	10,275 76	12,813 54	14,832 91	12,379 64	13,392 08
Way Passengers,..... }						
Through Freight,..... }	21,684 15	17,574 01	20,417 58	21,964 85	25,220 53	23,464 14
Way Freight,..... }						
Express,	350 00	350 00	350 00	350 00	350 00	350 00
Transport of Mails,	883 33	883 33	883 33	883 33	883 33	883 33
Use of Engines, .....	.....	.....	.....	.....	.....	.....
Use of Cars, .....	.....	.....	.....	.....	.....	.....
Rent, .....	.....	.....	.....	.....	766 10	.....
Total, .....	.....	.....	.....	.....	.....	.....

TABLE I.

ACCIDENTS.

State the number of persons injured in life or limb, and the cause thereof, and whether passengers or persons employed.

State whether any such accidents have arisen from carelessness or negligence of any person in the employ of the corporation, and whether such person is retained in the service of the corporation.

	EMPLOYEES.		OTHERS.	
	Killed.	Injured.	Killed.	Injured.
Trains thrown from track,.....	1
Struck by bridge while on top of freight car,.....
Run over while walking on track,.....
Injured at road crossing,.....

Total number of persons killed,..... 1

Total number of persons injured but not killed,

In addition to which must be given a statement of the date of each accident, the place where it occurred, the train, the cause and the extent of the injuries inflicted upon each person, and the name of such person.

JULY 4, 1866. Evening passenger train going south was thrown from the track near McIndoe's Falls, by running into a large tree which had just fallen upon the track, and one passenger slightly injured.

NOVEMBER 14, 1866. Mr. Beard, express agent at Bradford, jumped from the train while in motion, and was so injured that he lived but two days.

NOVEMBER 15, 1866. Silas H. Fuller, brakeman on gravel train, while attempting to get upon the engine, fell under the wheels and was instantly killed.

FEBRUARY 26, 1867. Gilbert Williams, while lying near the track at Newport, intoxicated, was struck by the engine and so injured that he died the next day.

TABLE J.

NUMBER OF EMPLOYEES AND COMPENSATION.

2 Conductors of passenger trains.	
Amount of compensation, (each per month,).....	\$70 00
4 Conductors of freight trains.	
Amount of compensation, (each per month,)... .	60 00
1 Conductor of wood and gravel trains.	
Amount of compensation, (per day,).. . .	3 00
1 Master mechanic.	
Amount of compensation, (per month,)	125 00
2 Road Masters,	
Amount of compensation, (both per month,).....	75 00
50 Men in repair shops.	
Amount of compensation, (per day each,)... .	1 25 to 2 87
4 Engine men of passenger trains.	
Amount of compensation, (each per day,).....	3 00.
5 Engine men of freight, wood and gravel trains.	
Amount of compensation, (each per day,).....	2 50 to 3 00
8 Firemen.	
Amount of compensation, (each per day,).. . .	1 67
2 Baggage-men.	
Amount of compensation, (each per month,).....	55 00
Switchmen.	
Amount of compensation, (each per month,).....	
26 Section men (foreman,)	
Amount of compensation, (each per day,).....	1 50 to 2 00
50 Section hands.	
Amount of compensation, (each per day,).....	1 25 to 1 50
8 Watchmen.	
Amount of compensation, (each per day,).....	1 37
20 Station agents.	
Amount of compensation, (each per month,).....	20 to 58 33
30 Other laborers,	
Amount of compensation, (each per day,).....	1 25 to 1 50

1 Clerk connected with passenger business.	
Amount of compensation, (total per month,)	40
1 Clerk connected with freight business.	
Amount of compensation, (total per month,)	40
Superintendent of bridges,—salary, (per day,) . . .	3 00
Wood agent, do	
Other agents—how employed, and the salary of each as follows, viz:	
General ticket agent, cashier, and master of transportation,	2,800 00

SALARIES.

Agent,	\$1,200 00
Trustees,	
President,	
Superintendent,	2,000 00
Treasurer,	1,500 00

TRIAL BALANCE, JULY 18, 1867.

Construction,	\$2,889,663 17
Additional equipment,	104,775 00
New shops,	69,557 59
Wood lots,	7,691 14
Superintendent, including stock and materials on hand, also sundry unsettled accounts,	88,049 55
Notes receivable,	980 00
Finance com.,	4,495 89
Team account,	893 00
Agent for collections,	324 89
Cash,	5,320 32
Excise tax,	60 45
	<hr/>
	\$3,171,811 00

Lyndon lands,.....	\$	705 00
Preferred stock,.....	1,822,100 00	
Old stock,.....	139,000 00	
Special stock,.....	700 00	
Bonded debt,.....	573,800 00	
Notes payable,.....	299,170 13	
Trustees of sinking fund,.....	64,000 00	
Reserve,.....	24,000 00	
Partial payments on account subscription,	6,357 23	
Extension,.....	212,465 21	
Earnings,...	182 37	
Coupons uncalled for,.....	1,601 50	
Contingent,.....	24,852 56	
U. S. Government,.....	2,877 00	
		<hr/>
		\$3,171,811 00

N. P. LOVERING, JR., *Treasurer.*

STATE OF VERMONT, }
 CALEDONIA COUNTY, SS. }

7th day of September, 1867.

I, Nathaniel P. Lovering, Jr., Treasurer of the Conn. and Passumpsic Rivers Railroad Company, do solemnly swear that the above is a true statement of the condition of the finances of said company, their Trustees, or assignees, or lessees, on the 31st day of May, 1867.

N. P. LOVERING, JR., *Treasurer.*

Sworn before me,

HUBBARD HASTINGS.

Master in Chancery.

TABLE K.

Does any conductor or engineman in the service of your road indulge in intoxication?

Not an known.

Please answer the same question as applied to sectionmen, brakemen and switchmen.

No.

Does your road, or any of its operatives, knowingly furnish facilities for traffic in intoxicating liquors in violation of the prohibitory law of the State?

No.

Are the trains on your road allowed to stand across highway crossings to the unnecessary delay of the highway travel or business?

No.

Do your stationmen allow boys without business to frequent and remain in or about the stations, platforms and trains?

Are your road fences and cattle guards sufficiently constructed and supported, and kept in repair as the law demands?

Are your trains jeopardized by neglect of persons to keep closed bars and gates at the various farm crossings? Answer in detail or with proximate particularity.

STATE OF VERMONT,)

CALEDONIA COUNTY, SS. }

I, A. H. Perry depose and say that the facts set forth, and statements made in the foregoing report which has been signed by me, are true and correct according to the best of my knowledge, information and belief.

A. H. PERRY,

Superintendent.

Subscribed and sworn before me this 6th day of September, 1867.

HUBBARD HASTINGS,

Master in Chancery.

ANNUAL REPORT

OF THE

BENNINGTON & RUTLAND R. R. CO.,

FOR THE

YEAR ENDING JUNE 30, 186 .

TABLE A.

STOCK AND DEBTS.

Preferred stock,.....	...
Old stock, ...	...
Special stock, .....	...
Bonded debt, 6 per cent.,.....	...
Coupon notes, 7 per cent.,..	...
Floating indebtedness,.....	...
Coupons uncalled for,.....	...

TABLE B.

COST OF CONSTRUCTION.

Total cost of construction, as per last report,	
Expended since last report, viz : .....	...
For graduation and masonry, .....	...
“ bridges, .....	...
“ rails, .....	...
“ chairs, spikes and ties, .....	...
“ laying superstructure, .....	...
“ buildings and fixtures, ..	...

TABLE C.

EQUIPMENT.

Total cost of equipment as per last report,.....

Expended since last report,.....

Total cost of road and equipment,.....

TABLE D.

CHARACTERISTICS OF ROAD.

Length of road,..... 59 miles.

“ “ completed,..... 59 “

“ “ branches,.....

“ “ side tracks,..... 5 “

Weight of rail per yard,..... 56 lbs.

CHARACTER AND LENGTH OF BRIDGING.

	No. of Structures.	No. of Spans.	Length of bridging in feet.
Trussle bridging,.....			
Truss. bridging, 50 ft. span and under,.....	10	10	400
Truss. do., from 50 to 100 feet span,.....	8	8	520
Truss. do., from 100 to 150 feet span,.....	6	6	660
Truss. do., 150 ft. span and over,	3	3	480
Stone arch bridges,....	.....	.....	.....
Totals,	27	27	2,060

Number of road crossings at grade,..... 60

Number of road crossings above and below grade,

Number of cross ties per mile,..... 2,348

Chairs, number per mile,..... 440 to 704

Whole number of switches on main track,..... 40

GRADIENTS AND ALIGNMENT.

Level number of miles,.....	
Maximum grade,.....	
Amount of straight line, miles,.....	
Amount of curved lines, miles,.....	
Maximum radius,.....	
Minimum radius,.....	
Sum of ascents going in one direction, ..	
Sum of ascents going in opposite direction,.....	
Height of termini and summit above tide water,.	

BUILDINGS AND FIXTURES.

Passenger and freight houses,	13
Engine houses,.....	3
Repair shops, ...	1
Water stations,.....	5
Dwellings,.....	1
Wood sheds,	4
Turn tables,	3
Changes during the last year,	

Other buildings, as follows :

EQUIPMENT.

Number of locomotives owned by the Company on the 30th day of June, 1867.

	Under 16 tons.	16 to 26.	26 to 30.	30 to 35.	35 tons and over.
In good repair,.....				4	
Requiring slight repairs,.....					
Requiring heavy repairs,					
Worn out,					

Number of cars owned by the company June 30, 1867.

First class 8 wheel passenger cars in good repair,.	3
First class 8 wheel passenger cars wanting repair,	
2d class 8 wheel passenger cars in good repair,..	
2d class 8 wheel passenger cars wanting repair,..	
Baggage, express and mail cars in good repair,..	3
Baggage, express and mail cars wanting rapair,..	
Covered freight and cattle 8 wheel cars in good repair,	
Covered freight and cattle 8 wheel cars wanting repair,	
Platform 8 wheel cars in good repair,.....	
Other freight cars,	
Gravel cars,	
Average weight of passenger cars,.....	
do do baggage do	
do do box do	
do do platform do	

TABLE E.

BUSINESS OF FIVE AND ONE-HALF MONTHS.

Miles run by passenger and freight trains,.....	27,634
Miles run by gravel and construction trains,.....	
Miles run by wood trains,	
Number of through passengers carried in cars,...	448
Number of way passengers,	13,374
Number of passengers having passes,	
Number of miles traveled by way passengers,...	226,400
Average distance traveled by way passengers,....	18 miles.
No. of miles traveled by passengers having passes,	
Number of tons of through freight,	3,638
Number of tons of way freight,	11,769
Number of tons of way freight carried 1 mile,...	327,121
No. of tons through freight moved towards market,	
No. of tons through freight moved from market,	

Number of tons of way freight moved towards market,	
Number of tons of way freight moved from market,	
Average rate of speed of ordinary passenger trains, per hour,	20 miles.
Average rate of speed of express trains,	
Average rate of speed of freight trains, per hour, ..	12 "
Rate of fare charged first class through passengers per mile,	
Rate of fare charged first class way passengers per mile,	
Average rate of fare charged 2d class passengers per mile, ..	
Rate per ton per mile charged 1st class through freight,	
Rate per ton per mile charged on 2d class through freight,	
Rate per ton per mile charged on 3d class through freight,	
Rate per ton per mile charged on 4th class through freight,	
Rate per ton per mile charg'd 1st class way freight,	
Rate per ton per mile charged on 2d class way freight, ..	
Rate per ton per mile charged on 3d class way freight,	
Rate per ton per mile charged on 4th class way freight,	
Total number of tons of freight carried one mile, ..	492,614
Total number of passengers carried one mile,	242,580

TABLE F.

EXPENSES OF MAINTAINING ROADWAY AND REAL ESTATE.

For 5½ months, ending June 30th, 1867.

Ordinary repairs of road bed and superstructure, .. \$11,500 48

Extraordinary repairs of road bed,.....	
Cost of new rails used in repairs,.....	3,657 00
Number and weight of chairs,.....	
Weight of spikes,.....	
Cost of repairs of rails,.....	100 25
Number of cross ties used for renewals,	
Cost of same,.....	3,000 40
Cost of relaying rails and ties,.....	
Insurance and taxes on real estate,.....	856 87
Repairs of bridges,.	322 90
do stations,.....	187 28
do fences,	998 70
do masonry, bridges repaired,	21 65
Depot furniture,	
Total,.....	<hr/> \$20,645 53

COST OF REPAIRS OF MACHINERY.

*Repairs of engines and tenders,.....	\$60,505 81
*Repairs of passenger, baggage and freight cars, .	23,198 10
Repairs of tools and machinery in shops,....	140 50
Oil used about workshops,.....	81 45
Fuel,.....	
Waste,.....	
Other items in detail as follows :	
Snow plows,.....	150 00
Road tools,.....	416 04
Total,.....	<hr/> \$84,491 90

TABLE G.

COST OF OPERATING THE ROAD.

For the year ending June 30th, 1867.

Wood, including cost of preparing the same,.. .	\$4,544 40
Number of cords of wood used by locomotives,..	
“ “ tons of coal “ “ ..	

* Including 4 new locomotives and 6 new passenger and baggage cars.

Number of cords of wood used at stations,.....	
Number of cords lost by fire,.....	
Number of gallons of oil,.....	
Number of pounds of waste,.....	
Cost of oil and waste for engines and tenders, passenger and baggage cars, and freight cars,...	556 95
Loss and damage of goods, baggage, injuries to persons, property, including fire and animals killed on road,.....	38 91
Agents at stations, &c.,.....	2,665 40
Clerks in general offices, &c.,.....	416 66
Porters and watchmen,.....	631 00
Conductors, baggagemen and brakemen,....	1,193 80
Enginemmen and firemen,.....	1,340 65
For salaries of Trustees, President, Directors, Secretaries, Treasurer and Superintendent,. . .	916 64
For printing, stationery and office expenses,....	915 03
General expenses not included above,.....	3,576 96
Total,.....	\$16,796 49

RECAPITULATION OF EXPENSES.

Maintaining roadway,.....	20,645 53
Repairs of machinery,.....	84,491 85
Operating,..	16,796 49
Total,.....	\$121,933 87

TABLE H.

EARNINGS, RECEIPTS AND PAYMENTS.

From passengers,.....	12,396 38
From freight,.....	25,012 22
Expresses,.	2,612 50
Mails,.....	2,582 50
Rents,.....	62 35
Miscellaneous,.....	1,463 50
Total,..	\$44,129 45

BENNINGTON AND RUTLAND RAILROAD.

DETAILS OF EARNINGS FOR SIX MONTHS, ENDING JUNE 30, 1867.

SOURCES.	JANUARY.	FEBRUARY.	MARCH.	APRIL.	MAY.	JUNE.
Through Passengers,.....	7 80	97 80	143 00	140 48	111 65	187 80
Way Passengers,.....	732 25	1,797 92	2,242 61	2,004 71	2,002 84	2,927 52
Through Freight,.....	1,173 34	2,944 85	3,206 34	5,376 27	5,533 29	6,778 13
Way Freight,.....						
Express,	237 50	475 00	475 00	475 00	475 00	475 00
Transport of Mails,.....	237 50	475 00	475 00	475 00	475 00	475 00
Use of Engines, .....	.....	.....	.....	.....	.....	*1,463 50
Use of Cars,.....	.....	.....	.....	.....	.....	.....
Rent, .....	.....	.....	.....	.....	.....	*62 35
Total,.....	.....	.....	.....	.....	.....	.....

*Whole time.

TABLE J.

NUMBER OF EMPLOYEES AND COMPENSATION.

1 Conductor of passenger train,	
Amount of compensation, per month.....	\$60 00
1 Conductor of freight train,	
Amount of compensation, per month,....	50 00
1 Master mechanic,	
Amount of compensation, per day,.....	3 00
1 Road master,	
Amount of compensation, per year,	800 00
4 Men in repair shops,	
Amount of compensation, per day,.....	1 40 to 2 50
1 Engine man of passenger train,	
Amount of compensation, per month,.....	80 00
1 Engine man of freight train,	
Amount of compensation, per month,	70 00
2 Firemen,	
Amount of compensation, per month,.....	40 00
1 Baggage man,	
Amount of compensation, per month,.....	40 00
1 Switchman,	
Amount of compensation, per month,....	40 00
10 Section men (foreman),	
Amount of compensation, per month,	50 00
50 Sections hands,	
Amount of compensation, per day,.....	1 25 to 1 50
3 Watchmen,	
Amount of compensation, per day,.....	1 33 to 1 45
14 Station agents,	
Amount of compensation, per year,	120 to 850
1 Clerk connected with passenger and freight business,	
Amount of compensation, per year,	1,000 00
Superintendent, per year,.....	2,000 00

TABLE K.

Does any conductor or engineman in the service of your road indulge in intoxication?

Not to our knowledge.

Please answer the same question as applied to sectionmen, brakemen and switchmen.

Not to our knowledge.

Does your road, or any of its operatives, knowingly furnish facilities for traffic in intoxicating liquors in violation of the prohibitory law of this State?

Not to our knowledge.

Are the trains on your road allowed to stand across highway crossings to the unnecessary delay of the highway travel or business? No.

Do your stationmen allow boys without business to frequent and remain in or about the stations, platforms and trains?

Not to our knowledge.

Are your road fences and cattle guards sufficiently constructed and supported and kept in repair as the law demands?

They were left by the Lessees of the road, on the 16th of January, 1867, in bad condition,—and have not been fully repaired.

STATE OF VERMONT, }

BENNINGTON COUNTY, ss. }

Bennington, Sept. 24th, 1867.

I, T. W. Park, President, depose and say that the facts set forth, and statements made in the foregoing report which has been signed by me, are true and correct according to the best of my knowledge, information and belief.

Signed,

T. W. PARK.

ANNUAL REPORT

OF THE

RENSSELAER AND SARATOGA R. R. CO.

FOR THE YEAR 1866.

STATE OF NEW YORK, }
 RENSSELAER COUNTY, } ss.

HENRY C. LOCKWOOD, Treasurer, and ISAAC V. BAKER, acting Superintendent of operations of the Rensselaer and Saratoga Railroad Company, being severally sworn, each for himself, deposes and says, that the statements in the annexed report, which has been signed by each of them, at the end thereof, are true and correct, according to the best of his knowledge, information and belief.

(Signed)

HENRY C. LOCKWOOD.
 I. V. BAKER.

Subscribed and sworn to before me, the 28th day of November, 1866.

H. J. KING,
Commissioner of Deeds, Troy.

STOCK AND DEBTS.

Capital stock,.....	\$800,000 00
Amount of stock subscribed,.....	800,000 00
Amount paid in, as by last report,.....	800,000 00
Total amount now paid in of capital stock,.....	800,000 00
Funded debt, as by last report,.....	478,750 00
Total amount now of funded debt,.....	498,750 00
Total amount now of funded and floating debt,...	498,750 00
Average rate, per annum, of interest on funded debt,.....	7 per cent.

COST OF ROAD AND EQUIPMENT.

For graduation and masonry,.....	\$110,088 50	\$110,088 60
For bridges,.....	267,840 69	267,840 69
Superstructure, including iron,.....	250,691 83	290,691 83
Passenger and freight stations, build- ings and fixtures,.....	40,935 19	40,935 19
Engine and car-houses, machine shops, machinery and fixtures,...	30,980 24	30,980 24
Land, land damages and fences,...	42,854 17	43,735 17
Locomotives and fixtures, and snow plows,.....	66,000 00	132,476 00
Passenger and baggage cars,.....	53,200 00	81,267 33
Freight and other cars,.....	80,410 00	115,566 67
Engineering and agencies,	19,090 75	19,090 75
Total cost of road and equipment,...	\$962,091 37	\$1,132,672 37

CHARACTERISTICS OF ROAD.

Length of road,.....	25.22 miles.
Length of road laid,.....	25.22 “
Length of double track, including sidings,	5.50 “
Weight of rail, per yard, on main track,.....	58 to 62 lbs.
Number of engine-houses and shops,	8
Number of engines,.....	29
Number of 1st class passenger cars (rated as 8 wheel cars),.....	45
Number of 2d class and emigrant passenger cars (rated as 8 wheel cars),.....	5
Number of baggage, mail and express cars, (rated as 8 wheel cars),	17
Number of freight cars, (rated as 8 wheel cars),..	542
Length of main line of road from Troy to Ball- ston,.....	25.22 miles.

This report embraces the doings of the
 Rensselaer and Saratoga railroad,..... 25 miles,
 Saratoga and Schenectady railroad,.... 22 “

Albany and Vermont railroad,	12 miles.
Saratoga and Whitehall railroad,	47 “
Rutland and Whitehall railroad,	7 “
Troy, Salem and Rutland railroad, . . .	62 “

Total,	175 miles.
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DOINGS OF THE YEAR IN TRANSPORTATION, AND TOTAL MILES RUN.

Number of miles run by passenger trains,	262,979
Number of miles run by freight trains,	217,392
Number of miles run by wood and gravel trains,	28,792
	<hr/> 246,184
Number of passengers (all classes) carried in cars,	549,547
Number of miles traveled by passengers, or number of passengers carried one mile,	15,186,365
Number of tons, of 2,000 pounds, of freight carried in cars,	273,039
Total movement of freight, or number of tons carried one mile,	9,234,863
Average rate of speed adopted by ordinary pas- senger trains, including stops, (miles per hour),	25
Rate of speed of same, when in motion,	30
Average rate of speed adopted by express trains, including stops,	30
Rate of speed of same, when in motion,	35
Average rate of speed adopted by freight trains, including stops,	12
Rate of speed of same, when in motion,	15
Average weight, in tons, of passenger trains, exclusive of passengers and baggage,	86
Average weight, in tons, of freight trains, exclusive of freight,	145

The amount of freight, specifying the quantity in tons :

Of the products of the forest,.....	17,240
Of animals,.....	14,930
Of vegetable food,	24,727
Other agricultural products,.....	16,901
Manufactures,	58,625
Merchandise,	100,510
Other articles,	40,106
Total,.....	<hr/> 273,039

*The rate of fare for passengers, charged for the respective classes,
per mile, as follows :*

For first class through passengers,.....	3 to 4 cents.
For first class way passengers,.....	3 to 4 “
For second class through passengers,	2 to 3 “
For emigrant through passengers,.....	1.80 “

EXPENSES.

MAINTAINING THE ROAD, OR REAL ESTATE OF THE CORPORATION.	AMOUNT.	PASSENGER TRANSPORTA'T'N.	FREIGHT TRANSPORT'N.
Repairs of road-bed and railway, excepting cost of iron,	\$219,808 28	\$98,914 14	\$120,894 14
Cost of iron used in repairs,	86,716 79	39,023 40	47,693 39
Repairs of buildings,	40,425 25	18,191 50	22,233 75
Repairs of fences and gates,	8,283 04	3,726 30	4,556 74
Taxes on real estate,	30,955 67	13,930 05	17,025 62
Total,	\$386,189 03	\$173,785 39	\$212,403 64
REPAIRS OF MACHINERY.			
Repairs of engines and tenders,	\$78,040 41	\$35,118 21	\$42,922 20
Repairs of passenger and baggage cars,	31,401 42	31,401 42	.....
Repairs of freight cars,	22,317 12	.....	22,317 12
Repairs of tools and machinery in shops,	*		
Incidental expenses, including oil, fuel, clerks, watchmen, etc., about shops,	*		
Totals,	\$131,758 95	\$66,519 63	\$65,239 32

* Included in repairs.

EXPENSES,—(CONTINUED.)

OPERATING THE ROAD.	AMOUNT.	FREIGHT TRANSPORT'N.	FREIGHT TRANSPORT'N.
Office expenses, stationery, etc.,	\$4,676 03	\$2,104 22	\$2,571 81
Agents and clerks,	41,666 59	18,750 31	22,916 28
Labor: loading and unloading freight,	20,805 63	. . . . .	20,805 63
Porters, watchmen and switch tenders,	24,940 96	12,348 76	12,592 20
Wood and water station attendance,	1,623 65	730 65	893 00
Conductors, baggage and brakemen,	31,701 54	14,256 70	17,435 84
Enginemen and firemen,	36,288 36	16,329 96	19,958 40
Fuel, cost and labor of preparing for use, { Wood,	130,932 78	58,919 76	72,012 02
{ Coal,	16,213 13	7,295 92	8,917 21
Oil and waste for engines and tenders,	15,866 52	7,139 95	8,726 57
Oil and waste for freight cars,	2,004 93	. . . . .	2,004 93
Oil and waste for passenger and baggage cars,	2,617 17	2,617 17	. . . . .
Loss and damage of goods and baggage,	9,845 43	3,418 15	6,427 28
Damage for injuries of persons,	2,894 04	2,894 04	. . . . .
Damage to property, including damages by fire and cattle killed on road,	7,334 58	3,300 88	4,033 70
General superintendence,	12,750 00	5,737 50	7,012 50
Contingencies,	41,844 60	18,830 40	23,014 20
Totals,	\$104,005 94	\$174,683 37	\$229,322 57

EARNINGS AND CASH RECEIPTS, AND PAYMENTS.

1st Earnings:

From passengers,.....	\$569,488 80
From freight,.....	672,788 84
From other sources,.....	59,645 03

Total, \$1,301,922 67

3d Payments other than for Construction:

For transportation expenses,...	\$921,953 92
For interest, balance of account,.....	86,296 76
For dividends on stock, seven per cent.,.....	56,000 00
Lease account,.....	73,178 08
Special lease account,.....	60,666 66
Government tax,.....	38,710 93
To surplus,.....	65,116 32

Total,.. . . . \$1,301,922 67

Total amount of surplus,..... \$271,078 05

ACCIDENTS.

	Killed.	injured.
Employees,.....	4	1
Others,....	4	1
Total,.....	8	2

The following is a statement of the date of each accident, the place where it occurred, the train, the cause and the extent of the injuries inflicted upon each person, and the name of such person, as follows:

1865.

October 19. Aaron Carpenter, conductor of extra freight train, walked off a platform car on Van Schaick's Island, and falling, broke his neck, and died instantly.

1866.

January 11. Hubbell Allen, switchman at Waterford junc-

tion, while climbing to top of car to apply brakes, fell to the ground, was run over, had his leg crushed and amputated and subsequently died.

December 18. ——— Duellow stepped on to the track immediately ahead of the engine of wood train, half a mile east of Castleton, was run over and killed. Was intoxicated at the time ; had a bottle of liquor in his pocket.

January 29. An old man, named Wilson, drove nearly on to the track ahead of passenger train, No. 10, at Saratoga ; endeavored to turn his horse around, brought his cutter in contact with locomotive, was struck and instantly killed.

February 3. ——— Spicer, a drunken man, was run over by train No. 10 half a mile north of Ballston, and instantly killed. He had just been turned out of a dram-shop, drunk, and was warned of the danger of walking on the track.

March 31. Alfred Palmer, brakeman on freight train, fell from cars at West Rutland, and legs crushed ; both amputated. Has fully recovered.

April 18. John Rich, brakeman on freight train, fell from top of car while train was in motion near Ballston, was run over and killed.

May 18. George Dyke, fireman, while shackling cars at Eagle Bridge, was caught between two cars and injured so that he died on the 22d.

July 21. A boy, while playing in the yard at Green Island, was run over by an engine backing up ; leg amputated.

May 4. G. P. Haunum, was run over and instantly killed in the yard at Rutland, by the mail train from Saratoga. He was walking on the track in the same direction the train was moving. Several trains being in motion on various tracks, he apparently became confused and failed to get out of the way.

 NAMES AND RESIDENCES OF OFFICERS OF THE COMPANY.
Directors.

GEORGE H. CRAMER,.....	TROY, N. Y.
J. M. WARREN,	do
J. L. VAN SCHOONHOVEN,	do
DANIEL SOUTHWICK,.....	do
GEORGE DAUCHY,.....	do
THOMAS WHITE,.....	do
E. THOMPSON GALE,.....	do
JOHN A. GRISWOLD,	do
GEORGE M. TIBBETTS,	do
JAMES M. COOK,.....	SARATOGA, N. Y.
WILLIAM HOWARD HART,... ..	WATERVLIET, N. Y.
GEORGE B. WARREN,	TROY, N. Y.
BENJAMIN E. BATES,.....	BOSTON, MASS.
GEORGE H. CRAMER,.....	PRESIDENT.
H. C. LOCKWOOD,.....	TREASURER AND SEC'Y.
ISAAC V. BAKER,.....	SUPERINTENDENT.

Communications intended for this company should be addressed,
Troy, Rensselaer county, New York.

The undersigned have caused the foregoing statements to be prepared by the proper officers and agents of this company, from the books and records, and have examined them as far as practicable, and believe them to be correct.

(Signed,)

H. C. LOCKWOOD.

I. V. BAKER.

ANNUAL REPORT

OF THE

TROY & BOSTON RAILROAD COMPANY, FOR THE YEAR 1866.

STATE OF NEW YORK, }
RENSSELAER COUNTY, } ss.

DANIEL ROBINSON, Treasurer, and CHARLES W. MOSELEY, acting Superintendent of operations of the Troy and Boston Railroad Company, being severally sworn, each for himself, deposes and says, that the statements in the annexed report, which has been signed by each of them, at the end thereof, are true and correct, according to the best of his knowledge, information and belief.

(Signed,)

DAN. ROBINSON.
C. W. MOSELEY.

Subscribed and sworn to before me, the 19th day of March, 1867.

E. A. G. COLE,
Commissioner of Deeds, Troy, N. Y.

{ 5 cent I. R. }
Stamp
{ Canceled. }

STOCK AND DEBTS.

Capital stock as by charter,.....	\$1,000,000 00
Amount of stock subscribed,.....	654,670 00
Amount paid in, as by last report,.....	607,111 22
Total amount now paid in of capital stock,.....	607,111 22
Funded debt, as by last report,.....	1,452,000 00
Total amount now of funded debt,.....	1,497,000 00
Floating debt, as by last report,	282,216 18
The amount now of floating debt,.....	375,886 34
Total amount now of funded and floating debt,...	1,872,886 34
Average rate, per annum, of interest on funded debt,	7 per cent

COST OF ROAD AND EQUIPMENT.

For graduation and masonry,.....	\$783,467 97	\$816,185 31
For bridges,.....	33,825 90	33,825 90
Superstructure, including iron,.....	282,999 26	297,559 02
Passenger and freight stations, build- ings and fixtures,.....	40,357 79	55,593 18
Engine and car-houses, machine shops, machinery and fixtures,...	15,991 34	36,026 31
Land, land damages and fences,...	206,028 53	217,444 87
Locomotives and fixtures, and snow plows,.....	118,701 00	118,701 00
Passenger and baggage cars,.....	33,357 97	43,038 43
Freight and other cars,.....	149,884 67	179,659 67
Engineering and agencies,	360,774 46	375,024 46
Total cost of road and equipment,..	\$2,034,387 99	\$2,173,058 15

CHARACTERISTICS OF ROAD.

Length of road,.....	34.91 miles.
Length of road laid,.....	34.91 “
Length of double track, including sidings,	9.23 “
Weight of rail, per yard, on main track,.....	56 to 60 lbs.
Number of engine-houses and shops,	6
Number of engines,.....	13
Number of 1st class passenger cars (rated as 8 wheel cars),.....	12
Number of 2d class and emigrant passenger cars (rated as 8 wheel cars),.....	1
Number of baggage, mail and express cars, (rated as 8 wheel cars),	6
Number of freight cars, (rated as 8 wheel cars)..	218
Length of main line of road from Troy to State Line, Vermont,.....	34.91 miles.

This report embraces the doings of the

Troy and Boston railroad,..... 35 miles,

Troy and Bennington railroad,..... 5 “

Western Vermont railroad,	59 miles.
Southern Vermont railroad,	6 "
Troy and Greenfield railroad,	7 "

Total,	:	112 miles.
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DOINGS OF THE YEAR IN TRANSPORTATION, AND TOTAL MILES RUN.

Number of miles run by passenger trains,	188,642
Number of miles run by freight trains,	224,766
Number of passengers (all classes) carried in cars,	268,126
Number of miles traveled by passengers, or number of passengers carried one mile,	8,976,754
Number of tons, of 2,000 pounds, of freight carried in cars,	166,503
Total movement of freight, or number of tons carried one mile,	6,519,162
Average rate of speed adopted by ordinary passenger trains, including stops, (miles per hour),	23
Rate of speed of same, when in motion,	29
Average rate of speed adopted by express trains, including stops,	29
Rate of speed of same, when in motion,	34
Average rate of speed adopted by freight trains, including stops,	10
Rate of speed of same, when in motion,	12 to 15
Average weight, in tons, of passenger trains, exclusive of passengers and baggage,	68
Average weight, in tons, of freight trains, exclusive of freight,	112

The amount of freight, specifying the quantity in tons :

Of the products of the forest,	5,093
Of animals,	3,990
Of vegetable food,	5,036
Other agricultural products,	16,420

Manufactures,	39,032
Merchandise,	42,833
Other articles,	54,099
Total,.....	<u>\$166,503</u>

*The rate of fare for passengers, charged for the respective classes,
per mile, as follows :*

For first class through passengers,..... 3 to 4 cents. and tax.
For first class way passengers,..... 4 “ “

EXPENSES.

MAINTAINING THE ROAD, OR REAL ESTATE OF THE CORPORATION.	AMOUNT.	PASSENGER TRANSPORT'N.	FREIGHT TRANSPORT'N.
Repairs of road-bed and railway, excepting cost of iron,	\$83,075 90	\$41,537 95	\$41,537 95
Cost of iron used in repairs,	36,968 09	18,484 04	18,484 05
Repairs of buildings,	2,813 66	1,406 83	1,406 83
Repairs of fences and gates,	3,812 64	1,906 32	1,906 32
Taxes on real estate,	14,381 66	7,190 83	7,190 83
Total,	<u>\$141,051 95</u>	<u>\$70,525 97</u>	<u>\$70,525 98</u>
REPAIRS OF MACHINERY.			
Repairs of engines and tenders,	\$32,233 61	\$16,116 80	\$16,116 81
Repairs of passenger and baggage cars,	8,285 76	8,285 76	.....
Repairs of freight cars,	17,896 00	.....	17,896 00
Repairs of tools and machinery in shops,	2,003 75	1,001 87	1,001 88
Incidental expenses, including oil, fuel, clerks, watchmen, etc., about shops,	5,303 40	2,651 70	2,651 70
Totals,	<u>\$65,722 52</u>	<u>\$28,056 13</u>	<u>\$37,666 39</u>

TROY AND BOSTON RAILROAD.

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EXPENSES, — (CONTINUED.)

OPERATING THE ROAD.	AMOUNT.	FREIGHT TRANSPORT'N.	FREIGHT TRANSPORT'N.
Office expenses, stationery, etc.,.....	\$8,163 85	\$4,081 92	\$4,081 92
Agents and clerks,.....	27,898 16	13,949 08	13,949 08
Labor: loading and unloading freight,.....	13,525 08	. . . . .	13,525 08
Porters, watchmen and switch tenders,.....	8,445 31	4,222 65	4,222 66
Conductors, baggage and brakemen,.....	18,941 29	8,954 21	9,987 08
Enginemen and firemen,.....	21,649 60	11,715 25	9,934 35
Fuel, cost and labor of preparing for use,.....	70,778 55	35,389 27	35,389 28
Oil and waste for engines, tenders and cars,.....	8,241 25	3,813 92	4,427 33
Expenses of Union Railroad,.....	9,212 18	4,606 09	4,606 09
Loss and damage of goods and baggage,.....	3,930 37	234 20	3,696 17
Damage for injuries of persons,.....	1,080 95	540 47	540 48
Damage to property, including damages by fire and cattle killed on road,.....	1,376 00	688 00	688 00
General superintendence,.....	3,000 00	1,500 00	1,500 00
Contingencies,.....	6,973 58	3,486 79	3,486 79
Totals,.....	\$203,216 17	\$93,181 85	\$110,034 32

EARNINGS AND CASH RECEIPTS, AND PAYMENTS.

1st Earnings :

From passengers,.....	\$289,500	33
From freight,.....	326,923	72
From other sources,.....	25,062	50

Total, \$641,486 55

2d Receipts :

From passengers,.....	\$289,500	33
From freight,.....	326,923	72
Mails,.....	10,862	50
Express,.....	14,000	00
Newsman,.....	200	00

Total,..... . \$641,486 55

3d Payments other than for Construction :

For transportation expenses,.....	\$409,990	64
For interest and rents,.....	209,056	84
United States internal revenue,.....	20,322	44
To payments to surplus fund,.....	2,116	63

Total,..... . \$641,486 55

ACCIDENTS.

Passengers,.....	1
Employees,.....	6
Others,.....	3

The following is a statement of the date of each accident, the place where it occurred, the train, the cause and the extent of the injuries inflicted upon each person, and the name of such person, as follows :

1865.

October 8. De Witt Dean, a freight train brakeman, fell from a car at Eagle Bridge and received injuries from which he died same day.

November 21. John Billman, freight brakeman, fell from train, was run over and instantly killed.

December 20. Dennis Baxter, while attempting to cross the track, was struck by an express passenger train and received slight injuries.

1866.

January 3. George Godfrey, freight brakeman, caught between two cars while shackling at North Bennington, seriously injured.

February 2. Joseph P. Brandy, passenger, and Melvin Stevens, freight brakeman, slightly injured by portion of train being thrown from track, between Hoosick Falls and Hoosick.

February 17. Henry Waterman, a man whose hearing was greatly impaired, while attempting to cross the track at Moody's bridge, in the town of Williamstown, was struck by a freight train and received fatal injuries.

May 8. Timothy Mahoney, a deaf and dumb boy, about ten years of age, was run over and killed by a night express passenger train.

May 21. Edward Tyrrell, a track laborer in the employ of the company, was injured by a clay bank falling on him ; died three days after the accident.

June 27. A. H. Pinney, freight conductor, and Marshall Clapp, engineer, injured by freight train being thrown from track at Cemetery crossing, in the city of Troy.

August 3. John Lockwood, freight brakeman, while standing on top of train, was struck by a bridge near Clarendon station and instantly killed.

 NAMES AND RESIDENCES OF OFFICERS OF THE COMPANY.
Directors.

D. THOMAS VAIL,	TROY, N. Y.
DANIEL ROBINSON,...	do
JARED S. WEED,.....	do
JOHN H. WILLARD,.....	do
LYMAN BENNETT,...	do
JONAS C. HEARTT,.....	do
JOSEPH W. FULLER,.....	do
JOSEPH H. PARSONS,.....	do
HIRAM MILLER,.....	do
SAMUEL M. VAIL,.....	do
AMOS BRIGGS,.....	SCHAGHTICOKE, N.Y.
LYMAN WILDER,	HOOSICK FALLS, N.Y.
R H. WELLS,.....	NORTH ADAMS, MASS.
D. THOMAS VAIL,...	PRESIDENT.
DANIEL ROBINSON,.....	TREASURER.
JARED S. WEED,.....	SECRETARY.
CHAS. W. MOSELEY,.....	SUPERINTENDENT.

Communications intended for this company should be addressed,
Troy, Rensselaer county, New York.

The undersigned have caused the foregoing statements to be
prepared by the proper officers and agents of this company, from
the books and records, and have examined them as far as prac-
ticable, and believe them to be correct.

(Signed,)

DAN. ROBINSON.
C. W. MOSELEY.

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59
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REPORT

OF THE

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Railroad

Commissioner

OF THE



STATE OF VERMONT

FOR

1868.

MONTPELIER :

J. & J. M. POLANDS' STEAM PRINTING ESTABLISHMENT.

OCTOBER, 1868.

REPORT

OF THE

Railroad Commissioner

OF THE

STATE OF VERMONT

FOR

1868.

MONTPELIER :

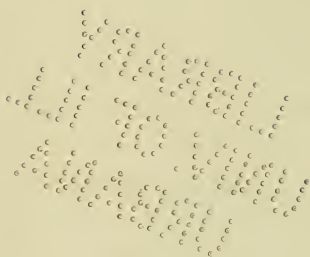
POLANDS' STEAM PRINTING ESTABLISHMENT.

1868.

385.3

V59

1867/68



REPORT.

To The General Assembly of the State of Vermont:

The undersigned has the honor to submit the following Report, upon the condition and management of the several Railroads in the State, for the year ending Aug. 31, 1868.

In compliance with the Statute, defining the duties of Railroad Commissioner, I furnished the officers and managers of Vermont Railroads with the form of a Report required of them, accompanied by a request that the same be perfected and returned to me for the use of the General Assembly at its present session. So far as I am in receipt of such reports they are submitted herewith.

The Railroads in Vermont are far from being perfect; but in passing over them from time to time,—which I have done, with the exception of the Atlantic & St. Lawrence Road,—I have noticed a marked improvement in the condition of road-beds and tracks, compared with what they were at the commencement of my official term.

I have not given as much time or attention to the examination of bridges as I should have done had I been an experienced bridge-builder. Railroad bridges are, as they should be, under the constant supervision of practical mechanics, upon the faithful discharge of whose duties, in time to come as in time past, the traveling public must rely for safe conveyance.

My attention was called at an early day to the existence of just cause for complaint, growing out of the want of proper notice of the departure of certain trains moving north from White River Junction. The annoyance had not

escaped the vigilance of Railroad managers. So far as I am informed at present, occasion for complaint no longer exists in that direction.

A partial connection has been effected at the State line, between the Bennington & Rutland Road and the road leading to Troy.

The work on the Lebanon Springs Road is being vigorously prosecuted. Its completion will secure an outlet to the business of Western Vermont, the want of which has been a source of great inconvenience and loss to the large and important manufacturing interests in that section of the State.

The Woodstock Railroad, incorporated in 1863, is under contract, and will without doubt be completed within a year.

An organization of the Rutland & Woodstock Road has been had within a few weeks. In the event of its construction, an important link will have been supplied between the Great West and our eastern seaboard.

Until within the past year, Vermont roads have never contributed to the catalogue of railroad disasters which from time to time have shocked the entire country. The catastrophe which occurred at Northfield on the 11th day of December, 1867, was of that character. Legislation cannot now prevent it. The like of it, without legislation, will never occur again. It has become a part of the history of our Railroads ; there it must remain, an ever present admonition to railroad managers and operatives, of the unyielding necessity—in the management of railroads—to exercise at all times every possible precaution against the recurrence of similar calamities.

Respectfully submitted.

WILLIAM ROUNDS,

Railroad Commissioner.

CHESTER, Sept. 5th, 1868.

ANNUAL REPORT
OF THE
VT. CENTRAL RAILROAD CO.,
FOR THE
YEAR ENDING MAY 31, 1868.

TABLE A.

STOCK AND DEBTS.

TABLE B.

COST OF CONSTRUCTION.

Total cost of construction as per last report, \$8,402,054 92

Expended since last report, viz :

For graduation and masonry,

“ bridges,

“ rails,

“ chairs, spikes and ties,

“ laying superstructure,

“ buildings and fixtures,

TABLE C.

EQUIPMENT.

Total cost of equipment as per last report,

Expended since last report,

Total cost of road and equipment,

TABLE D.

CHARACTERISTICS OF ROAD.

Length of road, (Vt. Central,)	117 miles.
“ “ (Vt. & Canada,)	65½ “
“ “ completed,	182½ “
“ “ branches,	2 “
“ “ side track,	30¾ “
Weight of rail per yard,	57 to 63 lbs.

CHARACTER AND LENGTH OF BRIDGING.

	No. of structures.	No. of spans.	Length of bridging in feet.
Trestle bridging,	8		8085
Truss bridging, 50 feet span and under,	45	55	1595
Truss do., from 50 to 100 feet span,	6	6	357
Truss do., from 100 to 150 feet span,	19	30	3681
Truss do., 150 feet span and over,	13	27	4240
Draw bridges,	2		244
Totals,	93	118	18,202

Number road crossings at grade,	125
Number of road crossings above and below grade,	34
Number of cross ties per mile,	2500
Chairs, number per mile,	440 to 600
Whole number of switches on main track,	145

GRADIENTS AND ALIGNMENT.

Level, number of miles,	
Maximum grade,	
Amount of straight line, on Vt. Central,	83 miles.
Amount of curved lines, on Vt. Central,	34 “

Maximum radius, on Vt. Central,	11,460 feet.
Minimum radius, on Vt. Central,	1,146 “
Sum of ascents going in one direction,	
Sum of ascents going in opposite direction,	
Height of termini and summit above tide water,	

BUILDINGS AND FIXTURES.

Passenger houses,	36
Freight houses,	35
Engine houses,	5
Repair shops,	3
Water stations,	28
Dwellings,	22
Wood sheds,	51
Turn-Tables,	5

Other buildings, as follows :

EQUIPMENT.

Number of locomotives owned by the Company on the 31st day of May, 1868 :

	Under 18 tons.	16 to 20.	20 to 25.	25 to 30.	30 tons and over.
In good repair, |					
Requiring slight repairs,					
Requiring heavy repairs,					
Worn out,					

Number of cars owned by the Company, May 31, 1868.

First class 8 wheel passenger cars in good repair,	34
First class 8 wheel passenger cars wanting repair,	8
Second class 8 wheel passenger cars in good repair,	0
“ “ “ wanting repairs,	0

Baggage, express and mail cars in good repair,	15
Baggage, express and mail cars wanting repair,	1
Covered freight and cattle 8 wheel cars, in good repair,	908
Covered freight and 8 wheel cars, wanting repair,	38
Platform 8 wheel cars, in good repair,	187
Other freight cars,	21
Gravel cars,	42
Average weight of passenger cars,	15 tons.
do do baggage do	10 do
do do box do	$7\frac{3}{4}$ do
do do platform do	$6\frac{3}{4}$ do

TABLE E.

BUSINESS OF THE YEAR.

Miles run by passenger trains,	434,785
Miles run by freight trains,	707,318
Miles run by gravel and construction trains,	33,345
Miles run by wood trains,	32,861
Number of through passengers carried in cars,	139,156 $\frac{1}{2}$
Number of way passengers,	221,038 $\frac{1}{2}$
Number of passengers having passes,	
Number of miles traveled by way passengers,	4,479,950
Average distance traveled by way passengers,	20 $\frac{1}{4}$
Number of miles traveled by passengers having <i>passes</i> ,	
Number of tons of through freight,	232,085
Number of tons of way freight,	245,523
Number of tons of way freight carried 1 mile,	17,065,262
Number of tons of through freight moved toward market,	
Number of tons of through freight moved from market,	
Number of tons of way freight moved towards market,	

Number of tons of way freight moved from market,	
Average rate of speed of ordinary passenger trains,	22 m. pr. hour.
Average rate of speed of express trains,	27 do
Average rate of speed of freight trains,	12 do
Rate of fare charged first class through passengers per mile,	3 cents.
Rate of fare charged first class way passengers, per mile,	4 do
Average rate of fare charged second class passengers, per mile,	2 do
Rate per ton per mile charged on 1st class through freight,	Rates vary
Rate per ton per mile charged on 2d class through freight,	from
Rate per ton per mile charged on 3d class through freight,	one and
Rate per ton per mile charged on 4th class through freight,	a quarter to
Rate per ton per mile charged on 1st class way freight,	ten cents per mile
Rate per ton per mile charged on 2d class way freight,	according to
Rate per ton per mile charged on 3d class way freight,	season, grade
Rate per ton per mile charged on 4th class way freight,	and distances.
Total number of tons of freight carried one mile,	47,067,328
Total number of passengers carried one mile,	14,520,100

TABLE F.

EXPENSES OF MAINTAINING ROADWAY AND REAL ESTATE.

For the year ending May 31, 1868.

Ordinary repairs of road bed and superstructure,	\$159,790 17
Extraordinary repairs of road bed,	
Cost of new rails used in repairs,	103,889 09
Number and weight of chairs,	
Weight of spikes,	
Cost of repairs of rails,	31,918 13
Number of cross ties used for renewals,	
Cost of same,	27,646 64
Cost of relaying rails and ties,	5,012 18
Insurance and taxes on real estate,	8,662 79
Repairs of bridges,	55,653 54
do stations,	18,301 16
do fences,	6,465 36
do depot furniture,	6,633 92
Total,	<u>\$423,972 98</u>
COST OF REPAIRS OF MACHINERY.	
Repairs of engines and tenders,	\$ 99,412 00
Repairs of passenger and baggage cars,	60,529 91
Repairs of freight cars,	114,507 62
Repairs of tools and machinery in work shops,	8,933 48
Oil used about workshops,	2,955 37
Fuel,	13,175 60
Waste,	369 65
Repairs of gravel, section, and hand cars, snow ploughs and road tools,	5,795 09
Other items in detail as follows.	
Total,	<u>\$305,678 72</u>

TABLE G.

COST OF OPERATING THE ROAD.

For the year ending May 31, 1868.

Wood, including the cost of preparing the same,	\$195,076 47
Number of cords of wood used by locomotives,	
do tons of coal do	
Number of cords of wood used at stations,	
do tons of coal, do	
Number of cords lost by fire,	
Number of gallons of oil,	
Number of pounds of waste,	
Cost of oil and waste for engines and tenders,	17,614 05
do do pass. and bag. cars,	2,834 13
do do freight cars,	6,293 32
Loss and damage of goods,	5,319 45
Loss and damage of baggage,	1,151 94
Damages for injuries to persons,	808 11
Damages to property, including fire, and animals killed on road,	1,756 43
Clerks in general offices, &c.,	20,687 34
Agents at stations,	36,196 88
Labor loading and unloading freight,	37,321 65
Watchmen,	10,500 41
Switchmen,	12,726 03
Advertising,	1,372 95
Conductors, baggagemen and brakemen,	70,884 98
Enginemen and firemen,	62,643 95
For salaries of trustees, president, directors, secretaries, treasurer and superintendent,	20,127 48

For printing and stationery,	6,351 85
For law expenses,	5,005 72
Other expenses, not included above,	30,775 49
	<hr/>
Total,	\$545,448 63

RECAPITULATION OF EXPENSES.

Maintaining roadway,	423,972 98
Repairs of machinery,	305,678 72
Operating,	545,448 63
Proportion of expenses due to passenger business,	
Proportion of expenses due to freight busi- ness,	
	<hr/>
Total,	\$1,275,100 33

TABLE H.

EARNINGS, RECEIPTS AND PAYMENTS.

Earnings and Receipts.

From passengers,	\$536,200 69
From freight,	1,209,851 17
From other sources, viz :	
Expresses,	12,270 02
Mails,	27,135 00
Rents,	2,624 19
Miscellaneous,	325 28
	<hr/>
Total,	\$1,788,406 35

Payments other than for Construction.

For transportation expenses, viz :

For passenger business,	}	Bro't forward, \$1,275,100 33
For freight business,		
For other business, and what,		

Balance,	\$513,306 02
----------	--------------

Of which sum there has been expended for
for building new draw at Rouse's Point,
together with interest, government taxes
and other items, the sum of

251,461 86

VALUE OF MATERIALS ON HAND.

Wood, cords of, 67,533, valued at		\$263,700 11
Coal, tons of,	“	1,209 92
Oil, gallons of,	“	2,079 00
Waste, pounds of,	“	250 00
Iron rails, tons of, old,		
Iron rails, tons of, new,		
Chairs, pounds of,		
Spikes, pounds of,		
Ties, number of,		
Iron and other metals, unwrought,	} Valued	70,687 17
Iron and other metals, worked and partly worked,		
Lumber, valued at		13,640 74

Other items specified as follows:

COST OF TRANSPORTATION.

Actual cost of transporting freight per ton,
per mile,

Actual cost of transporting passengers, per
mile,

DETAILS OF EARNINGS FOR THE YEAR ENDING MAY 31, 1868.

SOURCE.	JUNE, 1867.	JULY.	AUGUST.	SEPTEMBER.	OCTOBER.	NOVEMBER.
Through passengers,	21,708 30	37,344 21	38,924 89	36,844 05	42,161 33	38,448 44
Way passengers,	14,302 12	18,654 13	20,251 46	19,968 96	18,681 01	14,737 99
Through freight,	78,703 39	95,153 19	100,712 87	114,747 92	129,167 75	113,171 78
Way freight,	12,918 68	11,537 24	12,646 00	12,350 91	14,349 95	18,306 75
Express,	968 33	968 33	968 33	968 33	968 33	968 33
Transport of mails,	2,261 26	2,261 25	2,261 25	2,261 25	2,261 25	2,261 26
Use of engines,						
Use of cars,						
Rent,						858 63
Other earnings specified in details, as follows:	14 00		91 66	117 93	33 13	
Total,	130,786 08	165,918 35	175,856 46	187,259 35	207,622 75	188,753 18

DETAILS OF EARNINGS CONTINUED.

SOURCE.	DEC. 1867.	JAN. 1868.	FEBRUARY.	MARCH.	APRIL.	MAY.
Through passengers,	24,076 64	18,383 40	18,458 25	22,944 96	29,250 25	29,384 49
Way passengers,	11,642 92	10,427 66	9,644 12	13,623 53	14,423 69	11,913 89
Through freight,	61,764 14	58,914 88	53,658 23	62,164 13	89,475 84	100,052 46
Way freight,	6,745 83	7,179 30	7,375 36	11,509 89	12,715 77	24,528 91
Express,	1,076 71	1,076 67	1,076 67	1,076 67	1,076 66	1,076 66
Transport of mails,	2,261 26	2,261 25	2,261 25	2,261 25	2,261 25	2,261 22
Use of engines,						
Use of cars,						
Rent,				739 56	706 19	319 81
Other earnings specified in detail, as follows:	68 56					
Total,	107,636 06	98,243 16	92,473 88	114,319 99	149,909 65	169,537 44

TABLE I.

ACCIDENTS.

State the number of persons injured in life or limb, and the cause thereof, and whether passengers or persons employed.

State whether any such accidents have arisen from carelessness or negligence of any persons in the employ of the corporation, and whether such person is retained in the service of the corporation.

	EMPLOYEES.		OTHERS.	
	Killed.	Injured.	Killed.	Injured.
Trains thrown from track, Struck by bridge while on top of freight cars, Run over while walking on track, Injured at road crossing, Fell from train while in motion,	16	39		
	3		4	
Total,	19	39	4	

Total number of persons killed, 23

Total number of persons injured but not killed, 39

In addition to which must be given a statement of the date of each accident, the place where it occurred, the train, the cause and the extent of the injuries inflicted upon each person, and the name of such person.

TABLE J.

NUMBER OF EMPLOYEES AND COMPENSATION.

13 Conductors of passenger trains,	
Amount of compensation, (each per month),	\$50 to 75
31 Conductors of freight trains,	
Amount of compensation, (each per month),	50 to 65
2 Conductors of wood and gravel trains,	
Amount of compensation, (both per month),	52
2 Master mechanics,	
Amount of compensation, (both per year),	3,000
2 Road masters,	
Amount of compensation, (both per year),	1,800
278 Men in repair shops,	
Amount of compensation, (each per day),	1 to 4.50
14 Enginemen of passenger trains,	
Amount of compensation, (each per day),	3 00
33 Enginemen of freight trains,	
Amount of compensation, (each per day),	2 50 to 3 00
2 Enginemen of wood and gravel trains,	
Amount of compensation, (each per day),	3 00
53 Firemen,	
Amount of compensation, (each per day),	1 75
9 Baggage men,	
Amount of compensation, (each per month),	45 to 50
26 Switchmen,	
Amount of compensation, (each per month),	26 to 52
43 Section men, (foremen),	
Amount of compensation, (each per day),	2 00
247 Section hands,	
Amount of compensation, (each per day),	1 50 to 1 60

STATE OF VERMONT, }
FRANKLIN COUNTY, SS. }

St. Albans, Aug. 21, 1868.

WE depose and say that the facts set forth, and statements made in the foregoing report, which has been signed by us, are true and correct according to the best of our knowledge, information and belief.

Signed,

J. GREGORY SMITH,
ROBERT F. TAYLOR.

Trustees and Managers.

Subscribed and sworn to before me this 21st day of August, 1868.

SAMUEL WILLIAMS,

Notary Public.

ANNUAL REPORT

OF THE

VT. AND CANADA RAILROAD CO.,

FOR THE

YEAR ENDING JUNE 30, 1868.

TABLE A.

STOCK AND DEBTS.

Capital Stock on the 1st of June, 1868,	\$2,500,000 00
---	----------------

TABLE B.

COST OF CONSTRUCTION.

Total cost of construction as per last report,

Expended since last report, viz :

For graduation and masonry,

“ bridges,

“ rails,

“ chairs, spikes and ties,

“ laying superstructure,

“ buildings and fixtures,

NOTE,—For information from and under this table [B] to and including table [J], reference is respectfully made to the report of the Trustees and Managers of the Vt. Central & Vt. & Canada Railroads, by whom the Vt. & Canada road is operated.

JOHN W. NEWTON,
Treasurer Vt. & Canada R. R. Co.

TABLE C.

EQUIPMENT.

Total cost of equipment as per last report,

Expended since last report,

Total cost of road and equipment,

TABLE D.

CHARACTERISTICS OF ROAD.

Length of road,

“ “ completed,

“ “ branches,

“ “ side track,

Weight of rail per yard,

CHARACTER AND LENGTH OF BRIDGING.

	No. of structures.	No. of spans.	Length of bridging in feet.
Trestle bridging,			
Truss bridging, 50 feet span and under,			
Truss do., from 50 to 100 feet span,			
Truss do., from 100 to 150 feet span,			
Truss do., 150 feet span and over,			
Draw bridges,			
Totals,			

Number road crossings at grade,

Number of road crossings above and below grade,

Number of cross ties per mile,

Chairs, number per mile,

Whole number of switches on main track,

GRADIENTS AND ALIGNMENT.

Level, number of miles,
 Maximum grade,
 Amount of straight line, miles,
 Amount of curved line, miles,
 Maximum radius,
 Minimum radius,
 Sum of ascents going in one direction,
 Sum of ascents going in opposite direction,
 Height of termini and summit above tide water,

BUILDINGS AND FIXTURES.

Passenger houses,
 Freight houses,
 Engine houses,
 Repair shops,
 Water stations,
 Dwellings,
 Wood sheds,
 Turn-Tables,

Other buildings, as follows :

EQUIPMENT.

Number of locomotives owned by the Company on the
day of 186

	Under 18 tons.	16 to 20.	20 to 25.	25 to 30.	30 tons and over.
In good repair,					
Requiring slight repairs,					
Requiring heavy repairs,					
Worn out,					

Number of cars owned by the Company,	186
First class 8 wheel passenger cars in good repair,	
First class 8 wheel passenger cars wanting repair,	
Second class 8 wheel passenger cars in good repair,	
“ “ “ wanting repairs,	
Baggage, express and mail cars in good repair,	
Baggage, express and mail cars wanting repair,	
Covered freight and cattle 8 wheel cars, in good repair,	
Covered freight and 8 wheel cars, wanting repair,	
Platform 8 wheel cars, in good repair,	
Other freight cars,	
Gravel cars,	
Average weight of passenger cars,	
do do baggage do	
do do box do	
do do platform do	

TABLE E.

BUSINESS OF THE YEAR.

Miles run by passenger trains,
Miles run by freight trains,
Miles run by gravel and construction trains,
Miles run by wood trains,
Number of through passengers carried in cars,
Number of way passengers,
Number of passengers having passes,
Number of miles traveled by way passengers,
Average distance traveled by way passengers,
Number of miles traveled by passengers having <i>passes</i> ,
Number of tons of through freight,
Number of tons of way freight,
Number of tons of way freight carried 1 mile,

Number of tons of through freight moved toward market,

Number of tons of through freight moved from market,

Number of tons of way freight moved towards market,

Number of tons of way freight moved from market,

Average rate of speed of ordinary passenger trains,

Average rate of speed of express trains,

Average rate of speed of freight trains,

Rate of fare charged first class through passengers per mile,

Rate of fare charged first class way passengers, per mile,

Average rate of fare charged second class passengers, per mile,

Rate per ton per mile charged on 1st class through freight,

Rate per ton per mile charged on 2d class through freight,

Rate per ton per mile charged on 3d class through freight,

Rate per ton per mile charged on 4th class through freight,

Rate per ton per mile charged on 1st class way freight,

Rate per ton per mile charged on 2d class way freight,

Rate per ton per mile charged on 3d class way freight.

Rate per ton per mile charged on 4th class
way freight,

Total number of tons of freight carried one
mile,

Total number of passengers carried one mile,

TABLE F.

EXPENSES OF MAINTAINING ROADWAY AND REAL ESTATE.

	For the year ending	186 .
Ordinary repairs of road bed and superstructure,		\$
Extraordinary repairs of road bed,		
Cost of new rails used in repairs,		
Number and weight of chairs,		
Weight of spikes,		
Cost of repairs of rails,		
Number of cross ties used for renewals,		
Cost of same,		
Cost of relaying rails and ties,		
Insurance and taxes on real estate,		
Repairs of bridges,		
do stations,		
do fences,		
do masonry,		
Total,		

COST OF REPAIRS OF MACHINERY.

Repairs of engines and tenders,
Repairs of passenger and baggage cars,
Repairs of freight cars,
Repairs of tools and machinery in work shops,

Oil used about workshops,
 Fuel,
 Waste,
 Other items in detail as follows.

Total,

TABLE G.

COST OF OPERATING THE ROAD.

	For the year ending	186 .
Wood, including the cost of preparing the same,		
Number of cords of wood used by locomotives,		
do tons of coal do		
Number of cords of wood used at stations,		
do tons of coal, do		
Number of cords lost by fire,		
Number of gallons of oil,		
Number of pounds of waste,		
Cost of oil and waste for engines and tenders,		
do do pass. and bag. cars,		
do do freight cars,		
Loss and damage of goods,		
Loss and damage of baggage,		
Damages for injuries to persons,		
Damages to property, including fire, and ani-		
mals killed on road,		
Office expenses and stationery,		
Number of agents,		
Number of clerks,		
Labor loading and unloading freight,		
Porters and watchmen,		
Switchmen,		

Wood and water station attendance,
 Conductors, baggagemen,
 Brakemen,
 Enginemen and firemen,
 For salaries of trustees, president, directors,
 secretaries, treasurer and superintendent,
 For printing, stationery and office expenses,
 For law expenses,
 Other expenses in detail, as follows :

Total,

RECAPITULATION OF EXPENSES.

Maintaining roadway,
 Repairs of machinery,
 Operating,
 Proportion of expenses due to passenger
 business,
 Proportion of expenses due to freight busi-
 ness,

Total,

TABLE H.

EARNINGS, RECEIPTS AND PAYMENTS.

Earnings and Receipts.

From passengers,
 From freight,
 From other sources, viz :
 Expresses,

Mails,
Rents,
Miscellaneous,

Total,

Payments other than for Construction.

For transportation expenses, viz :

For passenger business,
For freight business,
For other business, and what,
For interest on funded debt,
For interest on floating debt,
For dividends,
For carried to surplus fund,
For amount of surplus fund,

VALUE OF MATERIALS ON HAND.

Wood, cords of,
Coal, tons of,
Oil, gallons of,
Waste, pounds of,
Iron rails, tons of, old,
Iron rails, tons of, new,
Chairs, pounds of,
Spikes, pounds of,
Ties, number of,
Iron and other metals, unwrought,
Iron and other metals, worked and partly worked,
Lumber,

Other items specified as follows :

COST OF TRANSPORTATION.

Actual cost of transporting freight per ton,
per mile,

Actual cost of transporting passengers, per
mile,

DETAILS OF EARNINGS FOR THE YEAR ENDING 186 .

SOURCE.	JULY.	AUGUST.	SEPTEMBER.	OCTOBER.	NOVEMBER.	DECEMBER.
Through passengers, Way passengers, Through freight, Way freight, Express, Transport of mails, Use of engines, Use of cars, Rent, Other earnings specified in details, as follows:						
Total,						

DETAILS OF EARNINGS CONTINUED.

SOURCE.	JANUARY.	FEBRUARY.	MARCH.	APRIL.	MAY.	JUNE.
Through passengers, Way passengers, Through freight, Way freight, Express, Transport of mails, Use of engines, Use of cars, Rent, Other earnings specified in detail, as follows:						
Total,						

TABLE I.

ACCIDENTS.

State the number of persons injured in life or limb, and the cause thereof, and whether passengers or persons employed.

State whether any such accidents have arisen from carelessness or negligence of any persons in the employ of the corporation, and whether such person is retained in the service of the corporation.

	EMPLOYEES.		OTHERS.	
	Killed.	Injured.	Killed.	Injured.
Trains thrown from track, Struck by bridge while on top of freight cars, Run over while walking on track, Injured at road crossing,				
Total,				

Total number of persons killed,

Total number of persons injured but not killed,

In addition to which must be given a statement of the date of each accident, the place where it occurred, the train, the cause and the extent of the injuries inflicted upon each person, and the name of such person.

TABLE J.

NUMBER OF EMPLOYEES AND COMPENSATION.

Conductors of passenger trains,
Amount of compensation,
Conductors of freight trains,
Amount of compensation,
Conductors of wood and gravel trains,
Amount of compensation,
Master mechanics,
Amount of compensation,
Road masters,
Amount of compensation,
Men in repair shops,
Amount of compensation,
Enginemen of passenger trains,
Amount of compensation,
Enginemen of freight trains,
Amount of compensation,
Enginemen of wood and gravel trains,
Amount of compensation,
Firemen,
Amount of compensation,
Baggagemen,
Amount of compensation,
Switchmen,
Amount of compensation,
Section men, (foremen),
Amount of compensation,
Section hands,
Amount of compensation,

Watchmen,
 Amount of compensation,
 Station agents,
 Amount of compensation,
 Other laborers,
 Amount of compensation,
 Clerks connected with passenger business,
 Amount of compensation,
 Clerks connected with freight business,
 Amount of compensation,
 Superintendent of bridges—salary,
 Wood agent,
 Other agents—how employed—and the salary of each, as follows, viz :

OFFICERS OF THE COMPANY.

W. C. SMITH, *President*,
 JOHN W. NEWTON, *Clerk and Treasurer*,
 EDWARD BLAKE, *Transfer Agent*.

SALARIES.

President,	\$1,000 00
Treasurer, Clerk and Transfer Agent,	3,000 00
Interest paid to and including June 1st, 1868,	\$200,000 00

STATE OF VERMONT, }
 FRANKLIN COUNTY, SS. } 21st day of July, 1868.

I, John W. Newton, Treasurer of the Vermont & Canada Railroad Company, do solemnly swear that the above is a true statement of the condition of the finances of said company, their Trustees, or assignees or lessees, on the 30th day of June, 1868.

JOHN W. NEWTON, *Treasurer*.

Sworn before me,

EUGENE PUTNAM, *Notary Public*.

STATE OF VERMONT, }
COUNTY, SS. }

depose and say that the facts set forth, and statements made in the foregoing report, which has been signed by are true and correct according to the best of knowledge, information and belief.

Signed,

Subscribed and sworn to before me this day of
186 .

ANNUAL REPORT

OF THE

ATLANTIC & ST. LAWRENCE R. R. CO.

FOR THE

YEAR ENDING JUNE 30, 1868.

TABLE A.

STOCK AND DEBTS.

Capital Stock authorized by Charter,	\$4,000,000 00
The amount paid in is,	2,494,900 00
Funded debt,	3,472,000 00
Floating debt,	

Interest at rate of six per cent.

The coupons are paid by the Lessees.

TABLE B.

COST OF CONSTRUCTION.

Total cost of construction as per last report, \$6,788,354 72

Expended since last report, viz :

For graduation and masonry,

“ bridges,

“ rails,

“ chairs, spikes and ties,

“ laying superstructure,

“ buildings and fixtures,

NOTE.—Two-thirds of this road was built at a stipulated price per mile, consequently, cannot be apportioned.

TABLE C.

EQUIPMENT.

Total cost of equipment as per last report,	\$865,734 76
Expended since last report,	
Total cost of road and equipment,	<u>\$7,654,089 48</u>

TABLE D.

CHARACTERISTICS OF ROAD.

Length of road, within State of Vermont,	30½ miles.
“ “ completed, do	30½ miles.
“ “ branches, do	
“ “ side track, do	about 1½ miles.
Weight of rail per yard,	63 lbs.

CHARACTER AND LENGTH OF BRIDGING.

	No. of structures.	No. of spans.	Length of bridging in feet.
Pile bridging,	1		1,519 one draw.
Truss bridging, 50 feet span and under,	10	10	400
Truss do., from 50 to 100 feet span,	2	2	120
Truss do., from 100 to 150 feet span,	14	14	1,750
Truss do., 150 feet span and over,			
Stone arch bridges,	2	2	120
Totals,	29	28	3,909

NOTE.—In addition to the above Bridges, there are on the line 16 Iron Bridges, 3 of which are three hundred feet each in length, in spans of 75 feet; one over the Connecticut River, one over Wild River, and the other over the Presumpscot River. Whole length of above named bridges, 1,510 feet.

Number road crossings at grade,	63
Number of road crossings above and below grade,	6
Number of cross ties per mile,	2,400
Chairs, number per mile,	about 600
Whole number of switches on main track,	do 100

GRADIENTS AND ALIGNMENT.

Level, number of miles and grades to 20 feet,	97.27
Maximum grade,	60 feet
Amount of straight line, miles,	89½ "
Amount of curved line, miles,	59½ "
Maximum radius,	5,730 "
Minimum radius,	955 "
Sum of ascents going in one direction,	1,929 "
Sum of ascents going in opposite direction,	757 "
Height of termini and summit above tide water,	1,178 "

BUILDINGS AND FIXTURES.

Passenger houses,	29
Freight houses,	22
Engine houses,	8
Repair shops,	6
Water stations,	17
Dwellings,	2
Wood sheds,	26
Turn-Tables,	10
Other buildings, as follows :	
Hotels,	2
Store houses on wharves at Portland for Bos-	
ton and ocean steamers,	10.

EQUIPMENT.

Number of locomotives owned by the Company on the
day of 186

	Under 18 tons.	16 to 20.	20 to 25.	25 to 30.	30 tons and over.
In good repair,			4	28	2
Requiring slight repairs,				3	
Requiring heavy repairs,			2	6	
Worn out,					

Number of cars appropriated by the Company,	
June 30th, 1868, to the Portland line,	565
First class 8 wheel passenger cars in good repair,	16
First class 8 wheel passenger cars wanting repair,	1
Second class 8 wheel passenger cars in good repair,	
“ “ “ wanting repairs,	
Baggage, express and mail cars in good repair,	
Baggage, express and mail cars wanting repair,	
Covered freight and cattle 8 wheel cars in	
good repair, covered freight and cattle 8	
wheel cars wanting repair, platform 8 wheel	
cars in good repair,	548
Other freight cars,	
Gravel cars,	
Average weight of passenger cars,	
do do baggage do	
do do box do	
do do platform do	

NOTE.—The Charter of the Atlantic & St. Lawrence Railroad within the State of Vermont, extends from the Connecticut River at Bloomfield to the boundary line in Norton. But the entire line from Portland to Montreal is worked in divisions, of which the point of junction is at Island Pond in Brighton.

TABLE E.

BUSINESS OF THE YEAR ENDING DEC. 31, 1867.

Miles run by passenger trains,	142,399
Miles run by freight trains,	532,637
Miles run by gravel and construction trains, }	48,276
Miles run by wood trains, }	
Number of through passengers carried in cars,	28,808
Number of way passengers,	98,465
Number of passengers having passes,	
Number of miles traveled by way passengers,	
Average distance traveled by way passengers,	
Number of miles traveled by passengers having <i>passes</i> ,	
Number of tons of through freight,	
Number of tons of way freight,	
Number of tons of way freight carried 1 mile,	
Number of tons of through freight moved toward market,	
Number of tons of through freight moved from market,	
Number of tons of way freight moved towards market,	
Number of tons of way freight moved from market,	
Average rate of speed of ordinary passenger trains,	20 m. pr. hour
Average rate of speed of express trains,	“ “
Average rate of speed of freight trains,	11 “ “
Rate of fare charged first class through pas- sengers, per mile,	
Rate of fare charged first class way passen- gers, per mile,	

Average rate of fare charged second class
 passengers, per mile,
 Rate per ton per mile charged on 1st class
 through freight,
 Rate per ton per mile charged on 2d class
 through freight,
 Rate per ton per mile charged on 3d class
 through freight,
 Rate per ton per mile charged on 4th class
 through freight,
 Rate per ton per mile charged on 1st class
 way freight,
 Rate per ton per mile charged on 2d class
 way freight,
 Rate per ton per mile charged on 3d class
 way freight,
 Rate per ton per mile charged on 4th class
 way freight,
 Total number of tons of freight carried one
 mile,
 Total number of passengers carried one mile,

TABLE F.

EXPENSES OF MAINTAINING ROADWAY AND REAL ESTATE.

For the year ending	186 .
---------------------	-------

Ordinary repairs of road bed and superstruc- ture,	\$
Extraordinary repairs of road bed,	
Cost of new rails used in repairs,	
Number and weight of chairs,	
Weight of spikes,	
Cost of repairs of rails,	

Number of cross ties used for renewals,
 Cost of same,
 Cost of relaying rails and ties,
 Insurance and taxes on real estate,
 Repairs of bridges,
 do stations,
 do fences,
 do masonry,
 Total,
 Books so kept cannot be answered in detail.

COST OF REPAIRS OF MACHINERY.

Repairs of engines and tenders,
 Repairs of passenger and baggage cars,
 Repairs of freight cars,
 Repairs of tools and machinery in work
 shops,
 Oil used about workshops,
 Fuel,
 Waste,
 Other items in detail as follows.

 Total,
 Books not classified to conform to above table,—answer
 cannot be given.

TABLE G.

COST OF OPERATING THE ROAD.

	For the year ending	186 .
Wood, including the cost of preparing the same,		
Number of cords of wood used by locomotives,		
do tons of coal do		

Number of cords of wood used at stations,
 do tons of coal, do
 Number of cords lost by fire,
 Number of gallons of oil,
 Number of pounds of waste,
 Cost of oil and waste for engines and tenders,
 do do pass. and bag. cars,
 do do freight cars,
 Loss and damage of goods,
 Loss and damage of baggage,
 Damages for injuries to persons,
 Damages to property, including fire, and ani-
 mals killed on road,
 Office expenses and stationery,
 Number of agents,
 Number of clerks,
 Labor loading and unloading freight,
 Porters and watchmen,
 Switchmen,
 Wood and water station attendance,
 Conductors and baggagemen,
 Brakemen,
 Enginemen and firemen,
 For salaries of trustees, president, directors,
 secretaries, treasurer and superintendent,
 For printing, stationery and office expenses,
 For law expenses,
 Other expenses in detail, as follows:

Total,

 RECAPITULATION OF EXPENSES.

Maintaining roadway,
 Repairs of machinery,
 Operating,
 Proportion of expenses due to passenger
 business,
 Proportion of expenses due to freight busi-
 ness,

Total,—year ending Dec. 31, 1867,	\$930,609 05
-----------------------------------	--------------

TABLE H.

EARNINGS, RECEIPTS AND PAYMENTS.

Earnings and Receipts year ending Dec. 31, 1867.

From passengers,	\$235,735 31
From freight,	745,360 22
From other sources, viz :	
Expresses, }	
Mails, }	30,198 61
Rents, }	
Miscellaneous, }	15,193 86

Payments other than for Construction.

For transportation expenses, viz :
 For passenger business,
 For freight business,
 For other business, and what,
 For interest on funded debt,
 For interest on floating debt,
 For dividends, 1st January and 1st July at

rate of 4 per cent. per annum less government tax,

For carried to surplus fund,

For amount of surplus fund,

The statement of receipts and expenses apply to the Portland Division, extending from Island Pond to Portland.

VALUE OF MATERIALS ON HAND.

Wood, cords of,

Coal, tons of,

Oil, gallons of,

Waste, pounds of,

Iron rails, tons of, old,

Iron rails, tons of, new,

Chairs, pounds of,

Spikes, pounds of,

Ties, number of,

Iron and other metals, unwrought,

Iron and other metals, worked and partly worked,

Lumber,

Other items specified as follows :

COST OF TRANSPORTATION.

Actual cost of transporting freight per ton,
per mile,

Actual cost of transporting passengers, per
mile,

DETAILS OF EARNINGS FOR THE YEAR ENDING 186 .

SOURCE.	JULY.	AUGUST.	SEPTEMBER.	OCTOBER.	NOVEMBER.	DECEMBER.
Through passengers, Way passengers, Through freight, Way freight, Express, Transport of mails, Use of engines, Use of cars, Rent, Other earnings specified in details, as follows:						
Total,						

Cannot be answered in detail.

DETAILS OF EARNINGS CONTINUED.

SOURCE.	JANUARY.	FEBRUARY.	MARCH.	APRIL.	MAY.	JUNE.
Through passengers, Way passengers, Through freight, Way freight, Express, Transport of mails, Use of engines, Use of cars, Rent, Other earnings specified in detail, as follows:						
Total,						

Cannot be answered in detail.

TABLE I.

ACCIDENTS.

State the number of persons injured in life or limb, and the cause thereof, and whether passengers or persons employed.

State whether any such accidents have arisen from carelessness or negligence of any persons in the employ of the corporation, and whether such person is retained in the service of the corporation.

	EMPLOYEES.		OTHERS.	
	Killed.	Injured.	Killed.	Injured.
Trains thrown from track, Struck by bridge while on top of freight cars, Run over while walking on track, Injured at road crossing,				
Total,				

Total number of persons killed,

Total number of persons injured but not killed,

In addition to which must be given a statement of the date of each accident, the place where it occurred, the train, the cause and the extent of the injuries inflicted upon each person, and the name of such person.

TABLE J.

NUMBER OF EMPLOYEES AND COMPENSATION.

Conductors of passenger trains,	
Amount of compensation,	
Conductors of freight trains,	
Amount of compensation,	
Conductors of wood and gravel trains,	
Amount of compensation,	
Master mechanics,	
Amount of compensation,	
Road masters,	
Amount of compensation,	
Men in repair shops,	
Amount of compensation,	
Enginemen of passenger trains,	
Amount of compensation,	
Enginemen of freight trains,	
Amount of compensation,	
Enginemen of wood and gravel trains,	
Amount of compensation,	
Firemen,	
Amount of compensation,	
Baggagemen,	
Amount of compensation,	
Switchmen,	
Amount of compensation,	
Section men, (foremen),	
Amount of compensation,	
Section hands,	
Amount of compensation,	

Watchmen,
 Amount of compensation,
 Station agents,
 Amount of compensation,
 Other laborers,
 Amount of compensation,
 Clerks connected with passenger business,
 Amount of compensation,
 Clerks connected with freight business,
 Amount of compensation,
 Superintendent of bridges—salary,
 Wood agent, “
 Other agents—how employed—and the salary of each, as follows, viz :

The Employees of the Company being employed upon two divisions of the Road,—one South of Island Pond extending to Portland, the other North of Island Pond extending to the Boundary Line and Montreal, no specific statement can be made under the head of enquiry as applied to the State of Vermont.

OFFICERS OF THE COMPANY.

SALARIES.

Grand Trunk Railway Co. of Canada, Lessees.

ST. JOHN SMITH, *President*,

HENRY BAILEY, *Superintendent*,

CHAS. E. BARRETT, *Treasurer*,

P. BARNES, *Vice President of Board of Directors*
Atlantic & St. Lawrence R. R. Co.

The earnings of the Road are received by the Lessees. The Lessors have no interest in them while the rent is paid, and the accounts are so kept by the Grand Trunk Railroad Company, as not to admit of an answer to the above as contemplated.

STATE OF MAINE, }
CUMBERLAND COUNTY, SS. } 6th day of August, 1868.

I, Chas. E. Barrett, Treasurer of the Atlantic and St. Lawrence Railroad Company, do solemnly swear that the above is a true statement of the condition of the finances of said company on the 30th day of June, 1868.

CHAS. E. BARRETT, *Treasurer.*

Sworn before me,

P. BARNES, *Justice of the Peace.*

STATE OF VERMONT, }
COUNTY, SS. }

depose and say that the facts set forth, and statements made in the foregoing report, which has been signed by are true and correct according to the best of knowledge, information and belief.

Signed,

Subscribed and sworn to before me this

day of

186 .

ANNUAL REPORT

OF THE

VERMONT & MASSACHUSETTS R. R. CO.

FOR THE

YEAR ENDING JUNE 30, 1868.

TABLE A.

STOCK AND DEBTS.

Capital Stock, including the Greenfield branch and Vermont portion,	\$3,200,000 00
Total amount of capital stock paid in,	2,860,000 00
Number of shares of capital stock issued,	28,600 00
Funded debt, redeemable July 1, 1883,	\$550,000 00
Floating debt, (old bonds and fractions),	345 00
Rate of interest paid on debt, six per cent.	
Interest coupons have always been promptly paid when due.	

TABLE B.

COST OF CONSTRUCTION.

Total cost of construction as per last report,	\$3,240,779 31
Expended since last report, viz :	
For graduation and masonry,	
“ bridges,	
“ rails,	
“ chairs, spikes and ties,	
“ laying superstructure,	
“ buildings and fixtures,	

TABLE C.

EQUIPMENT.

Total cost of equipment as per last report,	\$225,650 20
Expended since last report,	
Total cost of road and equipment,	\$3,466,429 51

TABLE D.

CHARACTERISTICS OF ROAD.

Length of road,	69 miles.
“ “ completed,	69 miles.
“ “ branches,	8 miles.
“ “ side track,	5½ miles.
Weight of rail per yard,	56 lbs.
Within this State, 10 miles and 1661 feet; side track, 1 mile.	

CHARACTER AND LENGTH OF BRIDGING.

	No. of structures.	No. of spans.	Length of bridging in feet.
Trestle bridging,			
Truss bridging, 50 feet span and under,	11	11	
Truss do., from 50 to 100 feet span,	5	5	
Truss do., from 100 to 150 feet span,	3	4	
Truss do., 150 feet span and over,	23	41	
Draw bridges,			
Totals,	42	61	

Number road crossings at grade,	60
Number of road crossings above and below grade,	21
Number of cross ties per mile,	2,350
Chairs, number per mile,	530
Whole number of switches on main track,	60

GRADIENTS AND ALIGNMENT.

Level, number of miles,	
Maximum grade,	58 feet
Amount of straight line, miles,	31 13-20 "
Amount of curved line, miles,	45 7-10 "
Maximum radius,	
Minimum radius,	
Sum of ascents going in one direction,	
Sum of ascents going in opposite direction,	
Height of termini and summit above tide water,	

BUILDINGS AND FIXTURES.

Passenger houses,	19
Freight houses,	20
Engine houses,	5
Repair shops,	4
Water stations,	9
Dwellings,	8
Wood sheds,	19
Turn-Tables,	5
General Office, Fitchburg,	1
Other buildings, as follows :	

EQUIPMENT.

Number of locomotives owned by the Company on the 30th day of June, 1868,	(twelve) 12
--	-------------

	Under 18 tons.	16 to 20.	20 to 25.	25 to 30.	30 tons and over.
In good repair,			8		1
Requiring slight repairs,			2		
Requiring heavy repairs,					
Worn out,					

Number of cars owned by the Company, June 30th, 1868,	199
First class 8 wheel passenger cars in good repair,	11
First class 8 wheel passenger cars wanting repair,	1
Second class 8 wheel passenger cars in good repair,	
“ “ “ wanting repairs,	
Baggage, express and mail cars in good repair,	6
Baggage, express and mail cars wanting repair,	1
Covered freight and cattle 8 wheel cars in good repair,	89
Covered freight and cattle 8 wheel cars, wanting repair,	4
Platform 8 wheel cars in good repair,	74
Other freight cars, 1 drovers' saloon and 2 snowplow cars,	3
5 chair cars, and 5 platform cars, wanting repairs,	10
Gravel cars,	
Average weight of passenger cars,	13½ tons.
do do baggage do	12 do
do do box do	8 do
do do platform do	6½ do

TABLE E.

BUSINESS OF THE YEAR ENDING NOV. 30, 1867.

Miles run by passenger trains,	117,167
Miles run by freight trains,	76,527

Miles run by gravel and construction trains,	}	5,432
Miles run by wood trains,		
Number of through passengers carried in cars,		104,438
Number of way passengers,		101,498
Number of passengers having passes,		841
Number of miles traveled by way passengers,		1,586,113
Average distance traveled by way passengers,		15½ miles.
Number of miles traveled by passengers having <i>passes</i> ,		16,574
Number of tons of through freight,		127,516 ⁸⁸ / ₁₀₀₀
Number of tons of way freight,		46,398 ¹³⁰ / ₁₀₀₀
Number of tons of way freight carried 1 mile,		812,106 ⁶⁷⁷ / ₁₀₀₀
Number of tons of through freight moved toward market, number of tons of through freight moved from market,		90,461 ⁹⁷² / ₁₀₀₀
Number of tons of way freight moved towards market, number of tons of way freight moved from market,		37,054 ¹¹⁶ / ₁₀₀₀
Average rate of speed of ordinary passenger trains,		22 m. pr. hour
Average rate of speed of express trains,		
Average rate of speed of freight trains,	10	“
Rate of fare charged first class through pas- sengers, per mile,		3½ cents.
Rate of fare charged first class way passen- gers, per mile,	4	“
Average rate of fare charged second class passengers, per mile,		
Rate per ton per mile charged on 1st class through freight,	7	“
Rate per ton per mile charged on 2d class through freight,	6	“

Rate per ton per mile charged on 3d class through freight,	5 cents.
Rate per ton per mile charged on 4th class through freight,	4 "
Rate per ton per mile charged on 1st class way freight,	12 "
Rate per ton per mile charged on 2d class way freight,	9 "
Rate per ton per mile charged on 3d class way freight,	8 "
Rate per ton per mile charged on 4th class way freight,	5 "
Total number of tons of freight carried one mile,	3,281,427 ⁸⁰⁸ / ₁₀₀₀
Total number of passengers carried one mile,	3,837,794

TABLE F.

EXPENSES OF MAINTAINING ROADWAY AND REAL ESTATE.

For the year ending June 30th, 1868.

Ordinary repairs of road bed and superstructure,	}	\$83,510 54
Extraordinary repairs of road bed,		
Cost of new rails used in repairs,		26,873 36
Number and weight of chairs,		In-
Weight of spikes,		
Cost of repairs of rails,		cluded
Number of cross ties used for renewals,		
Cost of same,		in
Cost of relaying rails and ties,		above.
Insurance and taxes on real estate,		5,788 48

Repairs of bridges,	8,332 12
do stations,	16,002 05
do fences,	1,871 76
Total,	<u>\$142,378 31</u>

COST OF REPAIRS OF MACHINERY.

Repairs of engines and tenders,	\$26,954 05
Repairs of passenger and baggage cars,	22,278 86
Repairs of freight cars,	17,488 78
Repairs of tools and machinery in work shops,	3,435 52
Oil used about workshops, }	} stated elsewhere.
Fuel, }	
Waste, }	
Repairs of gravel and hand cars,	312 50
Other items in detail as follows,	
Total,	<u>\$70,469 71</u>

TABLE G.

COST OF OPERATING THE ROAD.

For the year ending June 30, 1868.

Wood, including the cost of preparing the same,	\$29,949 94
Number of cords of wood used by locomotives,	5,912
Number of cords of wood used at stations,	200
Number of cords lost by fire,	
Cost of oil and waste for engines and tenders, }	\$3,415 71
do do pass. and bag. cars, }	
do do freight cars, }	
Loss and damage of goods, }	2,685 15
Loss and damage of baggage, }	

Damages for injuries to persons,	} included in last amount.	
Damages to property, including fire, and animals killed on road,		
Office expenses and stationery,		
Expense of passenger department,		\$31,052 07
Expense of freight do		35,137 57
Porters and watchmen, }		7,951 74
Switchmen, }		
Wood and water station expenses,		1,405 60
Conductors and baggagemen, }	} included in ex- penses charged freight and pas- senger dept's.	
Brakemen,		
Enginemen and firemen,		
For salaries of trustees, president, directors, secretaries, treasurer, superintendent and clerks,		9,308 32
For printing, stationery and advertising,		787 28
For law expenses,		1,003 00
Other expenses in detail, as follows :		
Clearing snow,		3,212 12
Rent to Connecticut River Railroad Co.,		1,500 00
Rent for use of Troy and Greenfield Railroad,		5,000 00
Incidental expenses,		6,281 07
U. S. tax,		3,889 98
State tax,		20,837 70
Total,		<u>\$163,417 25</u>

RECAPITULATION OF EXPENSES.

Maintaining roadway,	\$142,378 31
Repairs of machinery,	70,469 71
Operating,	163,417 25
Total,	<u>\$376,265 27</u>

TABLE H.

EARNINGS, RECEIPTS AND PAYMENTS.

Earnings and Receipts.

From passengers,	\$156,651 88
From freight,	257,345 23
From other sources, viz :	
Tolls,	900 75
Expresses,	9,900 00
Mails,	6,000 00
Rents,	46,051 75
Miscellaneous,	486 67

Payments other than for Construction.

For transportation expenses, viz :

For passenger business, }	376,265 27
For freight business, }	
For interest on funded debt,	29,368 30
For interest on floating debt,	
For dividend, No. 4 of 1½ pr cent. and U. S. Tax,	45,157 90
For carried to surplus fund,	
For amount of surplus fund,	

VALUE OF MATERIALS ON HAND.

Wood, cords of,	\$29,719 75
Coal, tons of,	
Oil, gallons of, }	596 30
Waste, pounds of, }	
Iron rails, tons of, old, 300 at \$45 00	13,500 00
Iron rails, tons of, new,	

Chairs, pounds of,	}	\$32,527 21
Spikes, pounds of,		
Ties, number of,		
Iron and other metals, unwrought,		
Iron and other metals, worked and partly worked,		
Lumber,	}	

Other items specified as follows :

COST OF TRANSPORTATION.

Actual cost of transporting freight per ton,
per mile,

Actual cost of transporting passengers, per
mile,

DETAILS OF EARNINGS FOR THE YEAR ENDING JUNE 30th, 1868.

SOURCE.	JULY, 1867.	AUGUST.	SEPTEMBER.	OCTOBER.	NOVEMBER.	DECEMBER.
Through passengers, }	14,874 34	18,461 88	18,018 19	13,998 59	13,581 99	9,710 01
Way passengers, }						
Through freight, }	19,263 31	20,233 12	21,974 77	23,786 87	19,463 31	17,354 76
Way freight, }						
Express,	800 00	800 00	800 00	800 00	800 00	800 00
Transport of mails,	500 00	500 00	500 00	500 00	500 00	500 00
Tolls,	92 00	103 00	98 00	62 25	69 00	68 00
Miscellaneous,	86 36	17 65	32 02	37 25	42 90	28 90
Rent,	3,861 33	3,250 00	3,788 75	3,726 83	5,064 17	3,902 08
Total,	39,477 34	43,365 65	45,211 73	42,911 79	39,521 37	32,363 75

DETAILS OF EARNINGS CONTINUED.

SOURCE.	JAN. 1868.	FEBRUARY.	MARCH.	APRIL.	MAY.	JUNE.
Through passengers, }	9,982 33	9,196 07	11,553 81	12,751 03	11,317 88	13,205 76
Way passengers, }						
Through freight, }	18,182 94	18,877 29	22,630 03	25,780 46	25,521 94	24,276 43
Way freight,						
Express,	800 00	900 00	850 00	850 00	850 00	850 00
Transport of mails,	500 00	500 00	500 00	500 00	500 00	500 00
Tolls,	48 25	30 50	64 25	90 00	117 00	58 50
Miscellaneous,	87 03	51 25	23 96	15 80	37 39	26 16
Rent,	3,924 09	3,250 00	4,125 00	3,747 00	3,287 50	4,125 00
Total,	33,524 64	32,805 11	39,747 05	43,734 29	41,631 71	43,041 85

TABLE I.

ACCIDENTS.

State the number of persons injured in life or limb, and the cause thereof, and whether passengers or persons employed.

State whether any such accidents have arisen from carelessness or negligence of any persons in the employ of the corporation, and whether such person is retained in the service of the corporation.

	EMPLOYEES.		OTHERS.	
	Killed.	Injured.	Killed.	Injured.
Trains thrown from track, Struck by bridge while on top of freight cars, Run over while walking on track, Injured at road crossing,				
Total,				

Total number of persons killed,

3

Total number of persons injured but not killed,

In addition to which must be given a statement of the date of each accident, the place where it occurred, the train, the cause and the extent of the injuries inflicted upon each person, and the name of such person.

AUGUST 6, 1867.—Honnora Leary, aged 4 years, was run over and killed at Grouts', in attempting to go between engine and baggage car, while train was moving slowly on side track to get wood.

DECEMBER 10, 1867.—Between Montague and Greenfield, passenger train run over and killed John Donevan while he was lying on the track. Coroner's Jury rendered verdict, "that they exonerate the employees from all blame."

APRIL 28, 1868.—Charles Atwood, aged $8\frac{1}{2}$ years, injured at Brattleboro, by getting caught between freight cars at depot, while moved by hand. He died April 30th.

TABLE J.

NUMBER OF EMPLOYEES AND COMPENSATION,

As per June Pay-Roll, 1868.

4 Conductors of passenger trains,	
Amount of compensation,	\$75 per month.
3 Conductors of freight trains,	
Amount of compensation,	\$50 to \$65 do.
1 Conductor of wood and gravel trains,	
Amount of compensation,	\$3.00 per day.
1 Master mechanic,	
Amount of compensation,	\$115 per month.
2 Road and bridge masters,	
Amount of compensation,	\$75 to \$150. do.
56 Men in repair shops,	
Amount of compensation,	\$1.50 to \$3.50 per day.
4 Enginemen of passenger trains,	
Amount of compensation,	\$75 and \$90 per month.
4 Enginemen of freight trains,	
Amount of compensation,	\$60 to \$75 do.
1 Enginemen of wood and gravel trains,	
Amount of compensation,	\$70 do.
9 Firemen,	
Amount of compensation,	\$40 to \$50 do.

4 Baggage men,	
Amount of compensation,	\$50 to \$60 per month.
5 Switchmen,	
Amount of compensation,	45 to \$54.50 do.
12 Section men, (foremen),	
Amount of compensation,	\$2. to 2.25 per day.
76 Section hands,	
Amount of compensation,	\$1.50 do.
6 Watchmen,	
Amount of compensation,	\$45 to \$50 per month.
20 Station agents,	
Amount of compensation,	\$15 to \$90 do.
6 Other laborers,	
Amount of compensation,	\$40 to \$45 do.
1 Clerk connected with passenger business,	
Amount of compensation,	\$75 do.
2 Clerks connected with freight business,	
Amount of compensation,	\$60 to \$120 do.
2 Freight receiptors,	\$12.50 to \$45 do.
1 Car examiner,	\$2,50 per day.
Other agents—how employed—and the salary of each, as follows, viz :	
3 Flagman,	\$30 and \$42.50 per month.
4 Telegraph operators,	\$10 do.
4 Bridge carpenters,	\$2.25 to \$3.00 per day.
1 Toll collector,	\$20 per month.
3 Brakeman on passenger trains,	\$45 do.
12 do. freight do.	\$45 do.
1 Master carpenter,	\$80 do.
75 Gravel train men,	\$1.25 and \$2.00 per day.

OFFICERS OF THE COMPANY.

J. C. HOWE, WM. MINOT, JR., J. MCGREGOR, *Trustees.*
 DANIEL S. RICHARDSON, *President.*
 OTIS T. RUGGLES, *Superintendent,*
 FRANKLIN N. POOR, *Treasurer,*
 B. D. LOCKE, *Clerk.*

SALARIES.

Trustees,	\$300, \$100, \$600.
President,	2,000 per annum.
Superintendent,	4,000 do
Treasurer,	3,000 do
Clerk,	50 do

Amount of net earnings for one year ending
 June 30, 1867, \$ 77,075 44

Amount of net earnings for one year ending
 June 30, 1868, 101,071 01

Amount of funded and floating debt June 30, '68, 550,345 00

Amount of cash in Treasury, \$ 4,338 30

do Loans, 48,500 00

2,600 Vt. & Mass. R. R. Co. Bonds, 24,473 49

————— \$77,311 79

Amount of interest paid during twelve months
 ending June 30, 1868, (more than received
 for loans), 29,368 30

COMMONWEALTH OF MASS., }
 Suffolk County, ss. } 26th day of August, 1868.

I, Franklin N. Poor, Treasurer of the Vermont and Massachusetts Railroad Company, do solemnly swear that the above

is a true statement of the condition of the finances of said company, their trustees, or assignees or lessees, on the 30th day of June, 1868.

FRANKLIN N. POOR, *Treasurer.*

Sworn before me,

P. E. TESCHEMACHER, *Justice of the Peace.*

COMMONWEALTH OF MASS., }

Suffolk County, ss. }

We depose and say that the facts set forth, and statements made in the foregoing report, which has been signed by us, are true and correct according to the best of our knowledge, information and belief.

Signed,

DANIEL S. RICHARDSON,	} <i>Directors of the Vt. and Mass. R. R. Co.</i>
WM. H. HILL,	
F. GOODHUE,	
GEORGE F. FAY,	

SUFFOLK, SS.—Subscribed and sworn to by Daniel S. Richardson and William H. Hill, before me this 27th day of August, A. D. 1868.

P. E. TESCHEMACHER,

Justice of the Peace.

ANNUAL REPORT

OF THE

CONN. & PASSUMPSIC RIVERS R. R. CO.

FOR THE

YEAR ENDING MAY 31, 1868.

TABLE A.

STOCK AND DEBTS.

Preferred stock,	\$1,822,100 00
Old stock,	138,900 00
Special stock,	900 00
Bonded debt, six per cent.,	553,800 00
Coupon notes, seven per cent.,	300,000 00
Floating indebtedness,	94,000 00
Coupons uncalled for,	1,817 00

TABLE B.

COST OF CONSTRUCTION.

Total cost of construction as per last report, \$2,889,663 17

Expended since last report, viz :

For graduation and masonry,	}	\$65,748 32
“ bridges,		
“ rails,		
“ chairs, spikes and ties,		
“ laying superstructure,		
“ buildings and fixtures,		

TABLE C.

EQUIPMENT.

Total cost of equipment as per last report,	\$341,200 52
Expended since last report,	24,050 00
Total cost of road and equipment,	

TABLE D.

CHARACTERISTICS OF ROAD.

Length of road,	110 miles.
“ “ completed,	“
“ “ branches,	
“ “ side track,	12 miles.
Weight of rail per yard,	56 lbs.

CHARACTER AND LENGTH OF BRIDGING.

	No. of structures.	No. of spans.	Length of bridging in feet.
Trestle bridging,	11		2,050
Truss bridging, 50 feet span and under,	13	18	769
Truss do., from 50 to 100 feet span,	7	10	666
Truss do., from 100 to 150 feet span,	5	5	580
Truss do., 150 feet span and over,	11	14	2,014
Draw bridges,			
Totals,	47	47	6,079

Number road crossings at grade,	74
Number of road crossings above and below grade,	6
Number of cross ties per mile,	2,000
Chairs, number per mile, cast iron, 680, Howe chair 440.	
Whole number of switches on main track,	80

GRADIENTS AND ALIGNMENT.

Level, number of miles,	
Maximum grade,	52 8-10 feet
Amount of straight line, miles,	
Amount of curved line, miles,	
Maximum radius,	
Minimum radius,	
Sum of ascents going in one direction,	
Sum of ascents going in opposite direction,	
Height of termini and summit above tide water,	

BUILDINGS AND FIXTURES.

Passenger houses,	5
Freight houses,	20
Engine houses,	3
Repair shops,	4
Water stations,	13
Dwellings,	19
Wood sheds,	23
Turn-Tables,	1
Car houses,	3
Other buildings, as follows :	
Store,	1

EQUIPMENT.

Number of locomotives owned by the Company on the
31st day of May, 1868,

	Under 18 tons.	16 to 20.	20 to 25.	25 to 30.	30 tons and over.
In good repair,		1	7	3	1
Requiring slight repairs,					
Requiring heavy repairs,					
Worn out,					

Number of cars owned by the Company, May 31, 1868,

First class 8 wheel passenger cars in good repair,	8
First class 8 wheel passenger cars wanting repair,	1
Smoking second class 8 wheel passenger cars in good repair,	1
“ “ “ “ wanting repair,	1
Baggage, express and mail cars in good repair,	3
Baggage, express and mail cars wanting repair,	3
Covered freight and cattle 8 wheel cars in good repair,	191
Covered freight and cattle 8 wheel cars, wanting repair,	10
Platform 8 wheel cars in good repair,	91
1 Drivers' saloon	
Gravel cars,	10
Average weight of passenger cars,	24,000
do do baggage do	17,000
do do box do	14,000
do do platform do	12,778

TABLE E.

BUSINESS OF THE YEAR.

Miles run by passenger trains,	85,696
Miles run by freight trains,	73,017
Miles run by gravel and construction trains, }	26,679
Miles run by wood trains, }	

Number of through passengers carried in cars, }	112,817
Number of way passengers, }	
Number of passengers having passes,	
Number of miles traveled by way passengers,	
Average distance traveled by way passengers,	
Number of miles traveled by passengers having <i>passes</i> ,	
Number of tons of through freight, }	74,757
Number of tons of way freight, }	
Number of tons of way freight carried 1 mile,	3,712,070
Number of tons of through freight moved toward market,	
Number of tons of through freight moved from market,	
Number of tons of way freight moved towards market,	
Number of tons of way freight moved from market,	
Average rate of speed of ordinary passenger trains,	23 m. pr. hour
Average rate of speed of express trains,	
Average rate of speed of freight trains,	12 " "
Rate of fare charged first class through pas- sengers, per mile,	3 $\frac{1}{4}$ cents.
Rate of fare charged first class way passen- gers, per mile,	4 "
Average rate of fare charged second class passengers, per mile,	
Rate per ton per mile charged on 1st class through freight,	5 $\frac{2}{3}$ "
Rate per ton per mile charged on 2d class through freight,	4 $\frac{1}{2}$ "

Rate per ton per mile charged on 3d class through freight,	37 ⁷ / ₈ cents.
Rate per ton per mile charged on 4th class through freight,	31 ¹ / ₂ "
Rate per ton per mile charged on 1st class way freight,	8 ² / ₃ "
Rate per ton per mile charged on 2d class way freight,	7 "
Rate per ton per mile charged on 3d class way freight,	
Rate per ton per mile charged on 4th class way freight,	
Total number of tons of freight carried one mile,	3,712,070
Total number of passengers carried one mile,	3,518,057

TABLE F.

EXPENSES OF MAINTAINING ROADWAY AND REAL ESTATE.

For the year ending May 31, 1868.

Ordinary repairs of road bed and superstruc- ture,	\$86,956 52
Extraordinary repairs of road bed,	
Cost of new rails used in repairs, (included above),	11,700 00
Number and weight of chairs,	
Weight of spikes,	
Cost of repairs of rails,	
Number of cross ties used for renewals,	36,848
Cost of same, 20 to 28 cents each.	
Cost of relaying rails and ties,	
Insurance on real estate,	\$2,654 61

Repairs of bridges,	7,376 25
do stations,	3,208 65
do fences,	
do masonry,	
Total,	<u>\$100,196 03</u>

COST OF REPAIRS OF MACHINERY.

Repairs of engines and tenders,	}	\$88,389 31
Repairs of passenger and baggage cars,		
Repairs of freight cars,		
Repairs of tools and machinery in work shops,		7,910 24
Oil used about workshops,		
Fuel,		
Waste,		
Other items in detail as follows,		
Total,		<u>\$96,299 55</u>

TABLE G.

COST OF OPERATING THE ROAD.

For the year ending May 31, 1868.

Wood, including the cost of preparing the same,	\$37,466 38
Number of cords of wood used by locomotives,	
do tons of coal, do	
Number of cords of wood used at stations,	
do tons of coal do	
Number of cords lost by fire,	
Number of gallons of oil,	
Number of pounds of waste,	

Cost of oil and waste for engines and tenders,	}	
do do pass. and bag. cars,		4,521 74
do do freight cars,		
Loss and damage of goods,	}	
Loss and damage of baggage,		
Damages for injuries to persons,		4,053 27
Damages to property, including fire, and animals killed on road,		
Office expenses and stationery,		
Number of agents,		
Number of clerks,		
Labor loading and unloading freight,		
Porters and watchmen,		
Expense of passenger department,		\$18,315 88
Wood and water station expenses,		
Conductors and baggagemen,		
Expense of freight department,		27,141 25
Enginemen and firemen,		
For salaries of trustees, president, directors, secretaries, treasurer, superintendent and general expenses,		31,900 36
For printing, stationery and office expenses,		
For law expenses,		1,003 00
Other expenses in detail, as follows :		

Total,	<u>\$123,398 88</u>
--------	---------------------

RECAPITULATION OF EXPENSES.

Maintaining roadway,	\$100,196 03
Repairs of machinery,	96,299 55
Operating,	123,398 88
Proportion of expenses due to passenger business,	
Proportion of expenses due to freight busi- ness,	
Total,	<u>\$319,894 46</u>

TABLE H.

EARNINGS, RECEIPTS AND PAYMENTS.

Earnings and Receipts.

From passengers,	\$170,172 10
From freight,	302,418 55
From other sources, viz :	
Expresses,	4,950 00
Mails,	11,160 09
Rents,	3,107 96
Miscellaneous,	

Payments other than for Construction.

For transportation expenses, viz :	
For passenger business,	18,315 88
For freight business,	27,141 25
Fuel, repairs and general expenses,	274,437 33
For interest on funded debt,	
For interest on floating debt,	
For dividends,	115,080 00
For carried to surplus fund,	
For amount of surplus fund,	

VALUE OF MATERIALS ON HAND.

Wood, 25,138 cords of,	\$70,531 73
Coal, 175 tons of,	2,386 15
Oil, 2,786 gallons of,	1,685 56
Waste, 3,575 pounds of,	485 63
Iron rails, 111 tons of, old,	5,615 00
Iron rails, tons of, new,	
Chairs, 8,129 pounds of,	406 45
Spikes, 2,100 pounds of,	120 74
Ties, 1,600 number of,	4,160 00

Iron and other metals, unwrought,	\$7,069 33
Iron and other metals, worked and partly worked,	4,500 38
Lumber, 670 M. feet,	21,730 03
Car trimmings, paint and sundry articles for repairs,	32,427 20

COST OF TRANSPORTATION.

Actual cost of transporting freight per ton, per mile,
Actual cost of transporting passengers, per mile,

DETAILS OF EARNINGS FOR THE YEAR ENDING MAY 31, 1868.

SOURCE.	JULY, 1867.	AUGUST.	SEPTEMBER.	OCTOBER.	NOVEMBER.	DECEMBER.
Passengers,	22,243 34	25,529 39	23,782 47	18,319 67	14,538 99	11,762 21
Way passengers,						
Freight,	21,854 16	24,376 75	28,185 32	39,182 49	29,885 67	21,967 20
Way freight,						
Express,						
Transport of mails,						
Use of engines,						
Use of cars,						
Rent,					1,298 48	
Other earnings specified in details as follows:						
Total,						

DETAILS OF EARNINGS CONTINUED.

SOURCE.	JAN. 1868.	FEBRUARY.	MARCH.	APRIL.	MAY.	JUNE.
Passengers,	9,494 89	10,054 00	12,752 15	12,446 21	11,963 15	13,396 01
Way passengers,						
Freight,	20,839 07	18,518 89	20,957 53	24,223 44	31,066 40	21,361 63
Way freight,						
Express,	412 50	pr. month. }	included in	passenger	earnings.	
Transport of mails,	926 42	pr. month. }				
Use of engines,						
Use of cars,						
Rent,					1,809 48	
Other earnings specified in detail, as follows:						
Total,						

TABLE I.

ACCIDENTS.

State the number of persons injured in life or limb, and the cause thereof, and whether passengers or persons employed.

State whether any such accidents have arisen from carelessness or negligence of any persons in the employ of the corporation, and whether such person is retained in the service of the corporation.

	EMPLOYEES.		OTHERS.	
	Killed.	Injured.	Killed.	Injured.
Trains thrown from track, Struck by bridge while on top of freight cars, Run over while walking on track, Injured at road crossing,				
Total,				

No fatal accidents, or any resulting in permanent disability.

Total number of persons killed,

Total number of persons injured but not killed,

In addition to which must be given a statement of the date of each accident, the place where it occurred, the train, the cause and the extent of the injuries inflicted upon each person, and the name of such person.

TABLE J.

NUMBER OF EMPLOYEES AND COMPENSATION.

3 Conductors of passenger trains,	
Amount of compensation,	\$60 to \$70 per month.
4 Conductors of freight trains,	
Amount of compensation,	\$60 do.
Conductors of wood and gravel trains,	
Amount of compensation,	
1 Master mechanic,	
Amount of compensation,	\$150 per month.
2 Road masters,	
Amount of compensation, (both),	\$138 do.
Men in repair shops,	
Amount of compensation,	\$1.40 to \$3.50 per day.
4 Enginemen of passenger trains,	
Amount of compensation,	\$3.00 do.
4 Enginemen of freight trains,	
Amount of compensation,	\$2.75 to \$3.00 do.
1 Enginemen of wood and gravel trains,	
Amount of compensation,	\$3.00 do.
9 Firemen,	
Amount of compensation,	\$1.67 do.
2 Baggage-men,	
Amount of compensation,	\$55 per month.
Switchmen,	
Amount of compensation,	
24 Section men, (foremen),	
Amount of compensation,	\$1.50 to \$2.25 per day.
Section hands,	
Amount of compensation,	\$1.35 to \$1.50 do.
4 Watchmen,	
Amount of compensation,	\$1.37½ do.

25 Station agents,	
Amount of compensation,	\$20 to \$66.67 per month.
Other laborers,	
Amount of compensation,	\$1.40 to \$1.50 per day.
1 Clerk connected with passenger business,	
Amount of compensation,	\$40 per month.
1 Clerk connected with freight business,	
Amount of compensation,	\$1.25 per day.
Superintendent of bridges—salary,	\$3.00 do.
Wood agent,	
Other agents—how employed—and the salary of each, as follows, viz :	

OFFICERS OF THE COMPANY.

SALARIES.

Trustees,	
Agent,	\$1,200 00
Superintendent,	2,000 00
Treasurer and Transfer Agent,	1,500 00

Trial Balance, July 14, 1868.

Construction,	\$2,995,411 49
Wood lots,	6,378 14
Superintendent, including stock and materials on hand, also sundry accounts unsettled,	130,632 33
Excise tax,	106 35
Notes receivable,	2,188 00
Finance committee,	368 32
Agent for collections,	544 89
Cash,	13,876 55
	<hr/>
	\$3,109,506 07

Dividends uncalled for,	\$2,469 34	
Coupons uncalled for, (1,817),	1,817 00	
Lyndon Lands,	10,920 07	
Preferred stock,	1,822,100 00	
Old stock,	138,900 00	
Special stock,	700 00	
Agent for construction,	248 02	
Rent account,	1,684 65	
Trustees of sinking fund,	70,200 00	
Reserve,	24,000 00	
Contingent,	24,852 56	
Bonded debt,	553,800 00	
Notes payable,	394,000 00	
Partial payments account sub-		
scription,	4,998 23	
Earnings,	55,939 20	
U. S. Government,	2,877 00	
	<hr/>	\$3,109,506 07

[E. E.] N. P. LOVERING, JR., *Treasurer.*

STATE OF VERMONT, }
 } 1st day of August, 1868.
 CALEDONIA COUNTY, SS.

I, N. P. Lovering, Jr., Treasurer of the Connecticut and Passumpsic Rivers Railroad Company, do solemnly swear that the above is a true statement of the condition of the finances of said company, their trustees, or assignees or lessees, on the 14th day of July, 1868.

N. P. LOVERING, JR., *Treasurer.*

Sworn before me,

HUBBARD HASTINGS,

Master in Chancery.

STATE OF VERMONT, }
CALEDONIA COUNTY, SS. }

I, A. H. PERRY, depose and say that the facts set forth, and statements made in the foregoing report, which has been signed by me, are true and correct according to the best of my knowledge, information and belief.

Signed, A. H. PERRY, *Superintendent.*

Subscribed and sworn to before me this 3d day of August
1868.

HUBBARD HASTINGS,
Master in Chancery.

ANNUAL REPORT
OF THE
BENNINGTON & RUTLAND R. R. CO.,
FOR THE
YEAR ENDING JUNE, 1868.

TABLE A.

STOCK AND DEBTS.

TABLE B.

COST OF CONSTRUCTION.

Total cost of construction as per last report,

Expended since last report, viz :

For graduation and masonry,

“ bridges,

“ rails,

“ chairs, spikes and ties,

“ laying superstructure,

“ buildings and fixtures,

TABLE C.

EQUIPMENT.

Total cost of equipment as per last report,

Expended since last report,

Total cost of road and equipment,

TABLE D.

CHARACTERISTICS OF ROAD.

Length of road,	59 miles.
“ “ completed,	“
“ “ branches,	
“ “ side track,	5 miles.
Weight of rail per yard,	56 lbs.

CHARACTER AND LENGTH OF BRIDGING.

	No. of structures.	No. of spans.	Length of bridging in feet.
Trestle bridging,			
Truss bridging, 50 feet span and under,	10	10	400
Truss do., from 50 to 100 feet span,	8	8	520
Truss do., from 100 to 150 feet span,	6	6	660
Truss do., 150 feet span and over,	3	3	480
Draw bridges,			
Totals,	27	27	2,060

Number road crossings at grade,	60
Number of road crossings above and below grade,	.
Number of cross ties per mile,	2,348
Chairs, number per mile,	440-704
Whole number of switches on main track,	40

GRADIENTS AND ALIGNMENT.

Level, number of miles,	.
Maximum grade,	
Amount of straight line, miles,	
Amount of curved line, miles,	
Maximum radius,	

Minimum radius,
 Sum of ascents going in one direction,
 Sum of ascents going in opposite direction,
 Height of termini and summit above tide water,

BUILDINGS AND FIXTURES.

Passenger houses,	13
Freight houses,	
Engine houses,	3
Repair shops,	1
Water stations,	5
Dwellings,	1
Wood sheds,	4
Turn-Tables,	3

Other buildings, as follows :

EQUIPMENT.

Number of locomotives owned by the Company on the
 30th day of June, 1868,

	Under 18 tons.	16 to 20.	20 to 25.	25 to 30.	30 tons and over.
In good repair,				3	
Requiring slight repairs,				1	
Requiring heavy repairs,					
Worn out,					

Number of cars owned by the Company, June 30, 1868,

First class 8 wheel passenger cars in good repair,	3
First class 8 wheel passenger cars wanting repair,	
Second class 8 wheel passenger cars in good repair,	
“ “ “ “ wanting repair,	

Baggage, express and mail cars in good repair,	3
Baggage, express and mail cars wanting repair,	
Covered freight and cattle 8 wheel cars in good repair,	15
Covered freight and cattle 8 wheel cars, wanting repair,	
Platform 8 wheel cars in good repair,	25
Other freight cars,	
Gravel cars,	
Average weight of passenger cars,	
do do baggage do	
do do box do	
do do platform do	

TABLE E.

BUSINESS OF THE YEAR.

Miles run by passenger trains,	
Miles run by freight trains,	
Miles run by gravel and construction trains,	
Miles run by wood trains,	
Number of through passengers carried in cars,*	1,541
Number of way passengers,	39,821
Number of passengers having passes,	
Number of miles traveled by way passengers,	657,968
Average distance traveled by way passengers,	
Number of miles traveled by passengers having <i>passes,</i>	
Number of tons of through freight,	8,677
Number of tons of way freight,	29,081
Number of tons of way freight carried 1 mile,	833,088
Number of tons of through freight moved toward market,	
Number of tons of through freight moved from market,	

* This 1,541 is the number of passengers having Joint Tickets to and from stations on this road.

Number of tons of way freight moved towards market,	
Number of tons of way freight moved from market,	
Average rate of speed of ordinary passenger trains,	20 m. pr. hour
Average rate of speed of express trains,	
Average rate of speed of freight trains,	12 " "
Rate of fare charged first class through passengers, per mile,	
Rate of fare charged first class way passengers, per mile,	
Average rate of fare charged second class passengers, per mile,	
Rate per ton per mile charged on 1st class through freight,	
Rate per ton per mile charged on 2d class through freight,	
Rate per ton per mile charged on 3d class through freight,	
Rate per ton per mile charged on 4th class through freight,	
Rate per ton per mile charged on 1st class way freight,	
Rate per ton per mile charged on 2d class way freight,	
Rate per ton per mile charged on 3d class way freight,	
Rate per ton per mile charged on 4th class way freight,	
Total number of tons of freight carried one mile,	1,171,224
Total number of passengers carried one mile,	719,674

TABLE F.

EXPENSES OF MAINTAINING ROADWAY AND REAL ESTATE.

For the year ending June 30, 1868.

Ordinary repairs of road bed and superstructure,	\$26,862 52
Extraordinary repairs of road bed,	
Cost of new rails used in repairs,	20,442 47
Number and weight of chairs, } cost,	2,824 64
Weight of spikes, }	
Cost of repairs of rails,	2,404 60
Number of cross ties used for renewals,	
Cost of same,	5,910 19
Cost of relaying rails and ties,	
Insurance and taxes on real estate,	856 87
Repairs of bridges,	2,769 09
do stations and furniture,	1,233 97
do fences,	1,765 84
do masonry, including road repairs,	
Total,	\$65,070 19

COST OF REPAIRS OF MACHINERY.

Repairs of engines and tenders,	\$3,297 69
Repairs of passenger and baggage cars, }	*35,335 04
Repairs of freight cars, }	
Repairs of tools and machinery in work shops,	472 96
Oil used about workshops, }	159 94
Waste, }	
Other items in detail as follows,	
Section cars and tools,	1,396 94
Total,	\$40,662 57

*Including 40 new freight cars.

TABLE G.

COST OF OPERATING THE ROAD.

For the year ending	186 .
Wood, including the cost of preparing the same,	\$14,812 84
Number of cords of wood used by locomotives,	
do tons of coal, do	
Number of cords of wood used at stations,	
do tons of coal do	
Number of cords lost by fire,	
Number of gallons of oil,	
Number of pounds of waste,	
Cost of oil and waste for engines and tenders,	} 1,550 17
do do pass. and bag. cars,	
do do freight cars & stations,	
Loss and damage of goods,	79 29
Loss and damage of baggage,	10 00
Damages for injuries to persons,	
Damages to property, including fire,	
and animals killed on road,	20 00
Office expenses and stationery,	
Agents at stations,	5,478 66
U. S. government taxes,	582 22
Labor loading and unloading freight,	
Porters and watchmen,	1,551 15
Switchmen,	499 96
Wood and water station expenses,	
Conductors and baggagemen, }	3,451 75
Brakemen, }	
Enginemen and firemen,	3,778 49
For salaries of trustees, president, directors,	
secretaries, treasurer, superintendent and	
clerks,	3,000 00

For printing, stationery,	583 38
For expenses, not mentioned above,	6,599 96
Other expenses in detail, as follows :	
Total,	<u>\$41,997 87</u>

RECAPITULATION OF EXPENSES.

Maintaining roadway,	\$65,070 19
Repairs of machinery,	40,662 57
Operating,	41,997 87
Proportion of expenses due to passenger business,	
Proportion of expenses due to freight business,	
Total,	<u>\$147,730 63</u>

TABLE H.

EARNINGS, RECEIPTS AND PAYMENTS.

Earnings and Receipts.

From passengers,	\$37,406 03
From freight,	74,765 45
From other sources, viz :	
Expresses,	5,700 00
Mails,	5,677 86
Rents,	136 00
Miscellaneous,	1,755 00
	<u>\$125,440 34</u>

Payments other than for Construction.

For transportation expenses, viz :	
For passenger business,	
For freight business,	
For other business, and what,	

For interest on funded debt,
For interest on floating debt,
For dividends,
For carried to surplus fund,
For amount of surplus fund,

VALUE OF MATERIALS ON HAND.

Wood, cords of,
Coal, tons of,
Oil, gallons of,
Waste, pounds of,
Iron rails, tons of, old,
Iron rails, tons of, new,
Chairs, pounds of,
Spikes, pounds of,
Ties, number of,
Iron and other metals, unwrought,
Iron and other metals, worked and partly
worked,
Lumber,

Other items specified as follows :

COST OF TRANSPORTATION.

Actual cost of transporting freight per ton,
per mile,
Actual cost of transporting passengers, per
mile,

DETAILS OF EARNINGS FOR THE YEAR ENDING 186

SOURCE.	JULY.	AUGUST.	SEPTEMBER.	OCTOBER.	NOVEMBER.	DECEMBER.
Through passengers, {	4,081 46	4,509 84	4,083 84	3,114 47	2,931 93	2,684 15
Way passengers, {						
Through freight, {	7,602 42	7,543 61	7,230 19	1,847 11	7,263 76	4,107 79
Way freight, {						
Express,	475 00	475 00	475 00	475 00	475 00	475 00
Transport of mails,	475 00	475 00	475 00	475 00	475 00	475 00
Use of engines,						
Use of cars,						
Rent,						
Other earnings specified in details as follows:						
Total,						

DETAILS OF EARNINGS CONTINUED.

SOURCE.	JANUARY.	FEBRUARY.	MARCH.	APRIL.	MAY.	JUNE.
Through Passengers, }	1,822 90	1,604 85	2,497 08	2,719 01	3,200 89	4,150 61
Way passengers, }						
Through freight, }	3,089 70	3,150 70	3,659 09	7,993 21	7,848 35	8,429 52
Way freight, }						
Express,	475 00	475 00	475 00	475 00	475 00	475 00
Transport of mails,	475 00	475 00	475 00	484 52	491 67	491 67
Use of engines,						
Use of cars,						
Rent,						
Other earnings specified in detail, as follows:						
Total,						

TABLE I.

ACCIDENTS.

State the number of persons injured in life or limb, and the cause thereof, and whether passengers or persons employed.

State whether any such accidents have arisen from carelessness or negligence of any persons in the employ of the corporation, and whether such person is retained in the service of the corporation.

	EMPLOYEES.		OTHERS.	
	Killed.	Injured.	Killed.	Injured.
Trains thrown from track, Struck by bridge while on top of freight cars, Run over while walking on track, Injured at road crossing,				
Total,				

Total number of persons killed,

Total number of persons injured but not killed,

In addition to which must be given a statement of the date of each accident, the place where it occurred, the train, the cause and the extent of the injuries inflicted upon each person, and the name of such person.

TABLE J.

NUMBER OF EMPLOYEES AND COMPENSATION.

1 Conductor of passenger trains,

Amount of compensation,

\$60 per month.

1 Conductor of mixed trains,	
Amount of compensation,	\$50 per month.
1 Conductor of wood and gravel trains,	
(part of the time),	
Amount of compensation,	\$50 do.
1 Master mechanic,	
Amount of compensation,	\$3.00 per day.
1 Road master,	
Amount of compensation,	\$800 per year.
Men in repair shops,	
Amount of compensation,	
1 Engineman of passenger trains,	
Amount of compensation,	\$80 per month.
2 Enginemen of freight trains,	
Amount of compensation,	\$50 to \$70 do.
1 Engineman of wood and gravel trains,	
Amount of compensation,	\$50 do.
4 Firemen,	
Amount of compensation,	\$40 do.
1 Baggage man,	
Amount of compensation,	\$50 do.
2 to 5 Switchmen,	
Amount of compensation,	\$40 do.
10 Section men, (foremen),	
Amount of compensation,	\$50 per month.
40 to 50 Section hands,	
Amount of compensation,	\$1.50 per day.
3 Watchmen,	
Amount of compensation,	\$1.33-\$1.45 do.
14 Station agents,	
Amount of compensation,	\$120-\$850 per year.
Other laborers,	
Amount of compensation,	

1 Clerk connected with passenger business,	}	\$1,000 pr. yr.
Amount of compensation,		
1 Clerk connected with freight business,		
Amount of compensation,		
Superintendent of bridges—salary,		
Wood agent,		
Other agents—how employed—and the salary of each, as follows, viz :		

OFFICERS OF THE COMPANY.

SALARIES.

Trustees,	
Agent,	
Superintendent,	\$2,000 per year.
Treasurer,	

STATE OF VERMONT, }
 BENNINGTON COUNTY, SS. } 17th day of September, 1868.

I, H. M. Stanton, Asst. Treasurer of the Bennington and Rutland Railroad Company, do solemnly swear that the above is a true statement of the condition of the finances of said company, their trustees, or assignees or lessees, on the 30th day of June, 1868.

H. M. STANTON, *Asst. Treasurer.*

Sworn before me,

C. E. HOUGHTON,
Notary Public.

STATE OF VERMONT, }
 BENNINGTON COUNTY, SS. }

I, T. W. PARK, depose and say that the facts set forth,

and statements made in the foregoing report, which has been signed by H. M. Stanton, are true and correct according to the best of my knowledge, information and belief.

Signed,

T. W. PARK.

Subscribed and sworn to before me this 17th day of Sept.
1868.

C. E. HOUGHTON,

Notary Public.

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8/69

ANNUAL REPORT

OF THE

Railroad Commissioner

OF THE

STATE OF VERMONT

FOR

1869.



MONTPELIER:
FREEMAN STEAM PRINTING HOUSE AND BINDERY.
1869.

ANNUAL REPORT

OF THE

Railroad Commissioner

OF THE

STATE OF VERMONT

FOR

1869.



MONTPELIER:
FREEMAN STEAM PRINTING HOUSE AND BINDERY,
1869.

3863

V59

1867/69



REPORT.

To the General Assembly of the State of Vermont:

The undersigned respectfully submits his second Annual Report upon the condition and management of Vermont Railroads :

Near the close of the last session of the General Assembly I was directed by joint resolution to "report forthwith the actual condition" of the Atlantic & St. Lawrence Railroad; and, although a copy of the resolution was forwarded to me immediately upon its adoption, by Hon. George Nichols, Secretary of State, it was not received seasonably to admit of my making an examination of the road and report to the Legislature before its final adjournment.

I examined the road above referred to, as early as practicable the present season, and found much of the road-bed and a large proportion of the track in very bad condition.

With as little delay as possible, I called the attention of the Managing Director of the Grand Trunk Railroad—which last named company has a lease of the road in question—to the necessity of giving the Atlantic & St. Lawrence road immediate attention; and was informed, at once, by him that the road would undergo thorough repairs this season.

On the first day of September I made a second examination of the road above named, and was very much gratified to find a marked improvement in its condition; defective portions of the road-bed have been materially improved, new ties have taken the place of old ones, and, as I am informed, nearly six hundred tons of new iron has been laid within the past four months, and a large per cent. of the old iron re-

paired and relaid, so that at this time the track, with the exception of that part in the vicinity of stations, is in fair condition. It is obvious, however, that there is room for further improvement, and I was assured at the time of my second examination of the road, by the engineer in charge of the eastern division of the Grand Trunk Road, that the work of repairs on that part of their line in Vermont would be vigorously prosecuted during the remainder of the season.

I will simply add that having passed over the Grand Trunk Road from Montreal to the boundary line at Norton, and thence over the Atlantic & St. Lawrence Road to the eastern boundary of Vermont, the last named road will compare favorably with that part of the Grand Trunk Road in Canada, over which I passed.

From the examination which I have made of the other Vermont railroads—and I have passed over the principal roads several times—I am able to report them in good condition; and that, with the exception of interruptions occasioned by the deep snows of the past winter, and the swollen streams of last spring,—which in many instances submerged railroad tracks to the depth of several feet—trains have been run over Vermont roads with great regularity and almost uniform safety to passengers.

On or about the 15th day of July, I furnished the managers of the several Vermont railroads with the form of a report required of them. So far as I am in receipt of such reports from the several roads, they are submitted herewith.

WILLIAM ROUNDS,
Railroad Commissioner.

ANNUAL REPORT OF THE CONNECTICUT & PASSUMPSIC RIVERS R. R. CO.

For the Year ending May 31, 1869.

TABLE B.

COST OF CONSTRUCTION.

Total cost of construction as per last report, \$2,955,411 49

Expended since last report, viz :

Equipment,	\$39,894 00
Roadway,	875 50

TABLE C.

EQUIPMENT.

Total cost of equipment as per last report, \$365,250 52

Expended since last report, 39,894 00

TABLE D.

CHARACTERISTICS OF ROAD.

Length of road,	110 miles.
“ “ completed,	“
“ “ branches,	none.
“ “ side tracks,	12 miles.
Weight of rail per yard,	65 lbs.

CHARACTER AND LENGTH OF BRIDGING.

	No. of structures.	No. of spans.	Length of bridging in feet.
Trestle bridging,	11		2050
Truss bridging, 50 feet span and under,	13	18	796
Truss do., from 50 to 100 feet span,	7	10	666
Truss do., from 100 to 150 feet span,	5	5	580
Truss do., 150 feet span and over,	11	14	2014
Total,	47	47	6076

Number of road crossings at grade,	74
Number of railroad crossings above and below grade,	7
Number of cross ties per mile,	2000
Chairs, number per mile,	440 to 680
Whole number of switches on main track, about	80

GRADIENTS AND ALIGNMENT.

Maximum grade,	52 8-10 feet.
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BUILDINGS AND FIXTURES.

Passenger houses,	6
Freight houses,	21
Engine houses,	3
Repair shops,	5
Water stations,	12
Dwellings,	19
Wood sheds,	25
Turn tables,	2
Other buildings, as follows :	
Store building,	1

EQUIPMENT.

Number of locomotives owned by the company on the 31st day of May, 1869,

	Under 18 tons	16 to 20 tons.	20 to 25 tons.	25 to 30 tons.	30 tons and over
In good repair,		2	3	2	4
Requiring slight repairs,			2		
Requiring heavy repairs,					
Worn out,					

Number of cars owned by the company, May 31, 1869 :

First class 8 wheel passenger cars in good repair,	8
First class 8 wheel passenger car wanting repair,	1
Smoking and mail cars in good repair,	2

Baggage, express and mail cars in good repair,	5
Baggage, express and mail cars wanting repair,	2
Covered freight and cattle 8 wheel cars in good repair,	202
Platform 8 wheel cars in good repair,	114
Wood cars,	12
Coal cars,	13
Freight saloons,	4
Gravel cars,	20
Hand cars,	28
Drovers' saloon,	1
Average weight of passenger cars, about	24,000
do do baggage “	17,000
do do box “	14,000
do do platform “	12,778

TABLE E.

BUSINESS OF THE YEAR.

Miles run by passenger trains,	89,302
Miles run by freight trains,	84,829
Miles run by gravel and construction trains, }	16,278
Miles run by wood trains, }	
Number of through passengers carried in cars, }	118,019
Number of way passengers, }	
Number of tons of through freight, }	87,222
Number of tons of way freight, }	
Average rate of speed of express trains, }	23 miles.
Average rate of speed of ordinary passenger trains,	
do do freight trains,	13 “
Rate of fare charged first class through passengers per mile,	3 $\frac{1}{4}$ cents.
Rate of fare charged first class way passengers, per mile,	4 “
Rate per ton per mile charged on 1st class through freight,	5 $\frac{2}{3}$ “
Rate per ton per mile charged on 2d class through freight,	4 $\frac{1}{2}$ “

Rate per ton per mile charged on 3d class through freight,	3 $\frac{7}{8}$ "
Rate per ton per mile charged on 4th class through freight,	3 $\frac{1}{3}$ "
Rate per ton per mile charged on 1st class way freight,	8 $\frac{2}{3}$ "
Rate per ton per mile charged on 2d class way freight,	7 "
Total number of tons of freight carried one mile,	4,799,290
Total number of passengers carried one mile,	3,747,271

TABLE F.

EXPENSES OF MAINTAINING ROADWAY AND REAL ESTATE.

For the year ending May 31, 1869.

Ordinary repairs of road bed and super-structure,	}	\$99,603 44
Extraordinary repairs of road bed,		
Cost of new rails used in repairs,		
Number and weight of chairs,		
Weight of spikes,		
Cost of repairs of rails,	}	29,996
Number of cross ties used for renewal.,		
Cost of same, average, 27 cents each.		
Insurance on real estate,		\$5,587 55
Repairs of bridges,		6,638 53
“ stations, and new buildings,		18,200 96
“ fences,	} included in repairs of road.	
“ masonry,		

COST OF REPAIRS OF MACHINERY.

Repairs of engines and tenders,		\$24,922 09
“ passenger and baggage cars,	}	45,958 37
“ freight cars,		
Repairs of tools and machinery in work shops,		4,350 91

TABLE G.

COST OF OPERATING THE ROAD.

For the year ending May 31, 1869.

Wood, including cost of preparing the same,	\$38,528 60
No. of cords of wood used by locomotives, } do do at stations, }	about 9,000 00
do tons of coal do do	40
Cost of oil and waste for engines and tenders, } do do passenger and baggage cars }	\$6,596 70
do do freight cars,	
Loss and damage of goods, } Loss and damage of baggage, and }	\$3,244 14
animals killed,	
Damages for injuries to persons,	none.
Number of agents,	22
Expenses of passenger department,	\$21,420 35
do freight do	27,031 15
For salaries of trustees, president, directors, } secretaries, treasurer and superintendent, }	\$35,080 07
For printing, stationery and office expenses, }	
For law expenses, and expenses not specified, }	

TABLE H.

EARNINGS, RECEIPTS AND PAYMENTS.

Earnings and Receipts.

From passengers,	\$177,346 47
From freight,	330,549 64
From other sources, viz :	
Expresses,	\$6,000 00
Mails,	11,117 00
Rents,	4,333 97

VALUE OF MATERIALS ON HAND.

Wood, cords of, 20,801,	\$62, 219 00
Coal, tons of 169,	2,275 50
Oil, gallons of, 1110,	1,099 68

Waste, pounds of, 3901,	\$530 38
Iron rails, tons of, old, 95 3-4,	3,830 00
Iron rails, tons of, repaired, 52 3-4,	3,205 00
Chairs, pounds of, 14,213,	710 65
Spikes, pounds of, 11,900,	654 50
Ties, number of, 16,730,	4,182 50
Iron and other metals, unwrought,	16,529 00
Iron and other metals, worked and partly worked,	4,256 00
Lumber,	18,534 64
Articles for general repairs,	34,796 91

DETAILS OF EARNINGS FOR THE YEAR ENDING MAY 31, 1869.

SOURCE.	JULY, 1868.	AUGUST, 1868.	SEPT., 1868.	OCTOBER, 1868.	NOV., 1868.	DEC. 1868.
Passengers,	22,602 92	26,778 77	28,186 55	17,548 16	15,064 35	11,362 11
Freight,	22,568 29	28,566 74	33,119 93	38,362 85	32,859 58	26,655 97
Express,*						
Transport of mails,†						
Use of engines,						
Use of cars,						
Rent,					1,438 43	
Other earnings specified in detail as follows:						
Total,						

* \$500 per month included in passenger earnings. † \$926 42 per month included in passenger earnings.

DETAILS OF EARNINGS—CONTINUED.

SOURCE.	JAN., 1869.	FEB., 1869.	MARCH, 1869.	APRIL, 1869.	MAY, 1869.	JUNE, 1869.
Passengers,	10,040 54	9,951 07	13,386 72	12,650 21	12,917 76	13,974 31
Freight,	23,566 02	19,625 92	24,618 64	22,920 93	32,232 21	25,569 48
Express,						
Transport of mails,						
Use of engines,						
Use of cars,						
Rent,					2,895 54	
Other earnings specified in detail as follows :						
Total,						

TABLE J.

NUMBER OF EMPLOYEES AND COMPENSATION.

3 Conductors of passenger trains,		
Amount of compensation,	\$60 to 70 per month.	
4 Conductors of freight trains,		
Amount of compensation,	\$60	do
1 Master mechanic,		
Amount of compensation,	\$150	do
2 Road masters,		
Amount of compensation,	\$148	do
115 Men in repair shops,		
Amount of compensation,	75 to 3 25 per day.	
4 Enginemen of passenger trains,		
Amount of compensation,	\$3 00	do
3 Enginemen of freight trains,		
Amount of compensation,	\$2 50 to 3 00.	do
1 Engineman of wood and gravel trains,		
Amount of compensation,	\$3 00	do
8 Firemen,		
Amount of compensation,	\$1 67	do
1 Baggage-man,		
Amount of compensation,	\$60 per month.	
2 Switchmen,		
Amount of compensation,	\$1 50 per day.	
27 Section men, (foremen),		
Amount of compensation,	\$1 75 to 2 00	do
81 Section hands,		
Amount of compensation,	\$1 50	do
2 Watchmen,		
Amount of compensation,	\$1 60	do
20 Station agents,		
Amount of compensation,	\$20 to 66 67 per month.	
20 Other laborers,		
Amount of compensation,	\$1 37½ to 1 75 per day.	

1 Clerk connected with passenger business,	
Amount of compensation,	\$40 per month.
1 Clerk connected with freight business,	
Amount of compensation,	\$40 do
Superintendent of bridges—salary,	\$3 25 per day.
Wood agent,	\$2 75 per day.

Trial Balance, July 20, 1869.

Construction,	\$2,996,180 99
Notes receivable,	5,193 94
Superintendent, including stock and materials on hand, and sundry accounts unsettled,	125,847 95
Wood lots,	6,378 14
Agent for collections,	449 04
Cash,	26,640 72
	<hr/>
	\$3,160,690 78
Lyndon Lands,	\$12,760 07
Massawippi Railroad,	5,000 00
Coupons uncalled for,	592 50
Dividends,	1,824 94
Reserve,	93,645 84
Stock issued,	2,084,200 00
Bonded debt,	500,000 00
Notes payable in 1876,	295,000 00
Trustees of sinking fund,	93,000 00
Partial payments account subscription,	3,683 23
Earnings,	68,929 11
New shops,	2,055 09
	<hr/>
	\$3,160,690 78

[E. E.]

N. P. LOVERING, *Treasurer.*

STATE OF VERMONT, }
COUNTY, SS. } day of 186

I, N. P. Lovering, Treasurer of the Connecticut and Passumpsic Rivers Railroad Company, do solemnly swear that the above is a true statement of the condition of the finances of said company, their trustees, or assignees or lessees, on the 20th day of July, 1869.

N. P. LOVERING, *Treasurer.*

Subscribed and sworn to before me, this 30th day of July, A. D. 1869.

HUBBARD HASTINGS,
Master in Chancery.

OFFICERS OF THE COMPANY.

SALARIES.

President,	\$1,200 00
Superintendent,	2,000 00

STATE OF VERMONT, }
CALEDONIA COUNTY, SS. }

I, A. H. Perry, depose and say that the facts set forth, and statements made in the foregoing report, which has been signed by me, are true and correct, according to the best of my knowledge, information and belief.

Signed, A. H. PERRY, *Superintendent.*

Subscribed and sworn to before me this 26th day of July, 1869.

HUBBARD HASTINGS,
Master in Chancery.

ANNUAL REPORT OF THE VT. CENTRAL AND VT. & CANADA RAILROADS,

For the year ending May 31, 1869.

TABLE B.

COST OF CONSTRUCTION.

Total cost of construction, as per last report, \$8,402,054 92

TABLE D.

CHARACTERISTICS OF ROAD.

Length of road, Vermont Central,	117 miles.
“ “ Vermont and Canada,	65½ “
“ “ completed,	182½ “
“ “ branches,	2 “
“ “ side tracks,	33 “
Weight of rail per yard,	56 to 63 pounds.

CHARACTER AND LENGTH OF BRIDGING.

	No. of Structures.	No. of Spans.	Length of bridging in feet.
Trestle bridging,	8		8,085
Truss do., 50 ft. span and under,	45	55	1,595
Truss do., from 50 to 100 ft. span,	6	6	357
Truss do., from 150 to 100 ft. span,	19	30	3,681
Truss do., 150 ft. span and over,	13	27	4,240
Draw bridges,	2		244
Total,	93	118	18,202

Number road crossings at grade,	125
Number of road crossings above and below grade,	34
Number of cross ties per mile,	2,500

Chairs, number per mile,	440 to 600
Whole number of switches on main track,	147

GRADIENTS AND ALIGNMENT.

Amount of straight line, miles, on Vermont Central,	83
Amount of curved line, miles, on Vermont Central,	34
Maximum radius,	16,460 feet.
Minimum radius,	1,146 "

BUILDINGS AND FIXTURES.

Passenger houses,	37
Freight houses,	35
Engine houses,	5
Repair shops,	3
Water stations,	28
Dwellings,	22
Wood sheds,	53
Turn tables,	5
Car houses,	2
Ice houses,	5

TABLE E.

BUSINESS OF THE YEAR.

Miles run by passenger trains,	426,328
Miles run by freight trains,	805,009
Miles run by gravel and construction trains,	40,409
Miles run by wood trains,	34,043
Number of through passengers carried in cars,	149,115
Number of way passengers,	219,785
Number of miles traveled by way passengers,	4,519,784
Average distance traveled by way passengers, 20 6-10 miles.	
Number of tons of through freight,	337,294
Number of tons of way freight,	312,852
Number of tons of way freight carried 1 mile,	17,211,247

Average rate of speed of ordinary passenger trains,	22 m. per hour.
Average rate of speed of express trains,	27 " "
Average rate of speed of freight trains,	12 " "
Rate of fare charged first class through passengers per mile,	3 cents.
Rate of fare charged first class way passengers per mile,	4 " "
Average rate of fare charged second class passengers per mile,	2 " "

On through and way freights of all classes rates vary from one and a half to ten cents per ton a mile, according to season, grade and distance.

Total number of tons of freight carried 1 mile,	54,177,218
Total number of passengers carried 1 mile,	15,650,086

TABLE F.

EXPENSES OF MAINTAINING ROADWAY AND REAL ESTATE.

For the year ending May 31, 1869.

Ordinary repairs of road-bed and superstructure,	\$182,646 33
Cost of new rails used in repairs,	128,496 99
Cost of repairs of rails,	31,961 81
Cost of cross ties used for renewals,	26,189 82
Cost of re-laying rails and ties,	4,470 34
Insurance and taxes on real estate,	11,396 82
Repairs of bridges,	53,538 91
Repairs of stations,	32,479 24
Repairs of fences,	5,191 64
Repairs of depot furniture,	3,112 79
Total,	<u>\$479,484 69</u>

COST OF REPAIRS OF MACHINERY.

Repairs of engines and tenders,	\$113,812 93
Repairs of passenger and baggage cars,	44,699 68
Repairs of freight cars,	120,241 81
Repairs of freight cars,	120,241 81

Repairs of tools and machinery in work shops,	\$16,860 29
Oil used about work shops,	3,719 29
Fuel,	12,225 00
Waste,	302 24
Repairs of gravel, section, hand cars, snow plows,	8,483 70
Repairs of road tools,	3,806 45
Total,	<u>\$324,151 39</u>

TABLE G.

COST OF OPERATING THE ROAD.

For the year ending May 31, 1869.

Wood, including the cost of preparing same,	\$221,164 67
Cost of oil and waste for engines and tenders,	16,437 68
Cost of oil and waste for passenger and baggage cars,	2,315 46
Cost of oil and waste for freight cars,	6,508 17
Loss and damage of goods,	16,451 14
Loss and damage of baggage,	148 55
Damages for injuries to persons,	2,215 85
Damages to property, including fire, and animals killed on road,	872 80
Advertising,	1,822 74
Agents at stations,	39,241 50
Clerks in offices,	20,832 69
Labor loading and unloading freight,	39,751 68
Porters and watchmen,	9,511 39
Switchmen,	13,176 73
Conductors, baggagemen and brakemen,	73,929 67
Enginemen and firemen,	67,904 72
For salaries of trustees, president, directors, secretaries, treasurer and superintendent,	23,963 02
For printing, stationery and office expenses,	5,904 16
For law expenses,	6,032 34
Other expenses,	34,278 50
Total,	<u>\$602,463 46</u>

 RECAPITULATION OF EXPENSES.

Maintaining roadway,	\$379,484 69
Repairs of machinery,	324,151 39
Operating,	692,463 46
Total,	<u>\$1,406,099 54</u>

TABLE H.

EARNINGS, RECEIPTS AND PAYMENTS.

Earnings and Receipts.

From passengers,	\$583,680 60
From freight,	1,319,933 56
Expresses,	15,981 11
Mails,	28,390 44
Rents,	1,072 73
Miscellaneous,	371 99
Total,	<u>\$1,949,430 43</u>

VALUE OF MATERIALS ON HAND.

Wood, cords of, 62,074.	\$216,980 94
Coal, tons of, 75,	688 80
Iron and other metals, unwrought,	55,194 18
Iron and other metals, worked and partly worked, 12,052	77
Lumber,	24,572 55

DETAILS OF EARNINGS FOR THE YEAR ENDING MAY 31, 1869.

SOURCE.	JUNE, 1868.	JULY, 1868.	AUGUST, 1868.	SEPT., 1868.	OCTOBER, 1868.	NOV., 1868.
Through Passengers,	25,193 21	32,977 99	43,272 00	40,669 40	34,504 16	32,338 30
Way Passengers,	14,934 49	18,264 68	19,944 33	22,771 37	16,821 32	36,520 05
Through Freight,	56,713 57	50,797 53	58,423 75	66,742 29	76,020 31	53,738 05
Way Freight,	64,431 96	60,644 19	68,585 53	72,484 52	73,512 05	79,939 28
Express,	1,076 67	1,076 67	1,076 67	1,076 67	1,076 67	1,076 65
Transport of mails,	2,186 08	2,186 08	2,722 72	2,186 08	2,729 60	2,190 11
Use of engines,						
Use of cars,						
Rent,						
Other earnings specified in detail as follows :						
Total,	164,535 98	165,947 14	194,025 00	205,930 33	204,664 11	205,802 44

DETAILS OF EARNINGS—CONTINUED.

SOURCE.	DEC. 1868.	JAN., 1869.	FEB., 1869.	MARCH, 1869.	APRIL, 1869.	MAY, 1869.
Through Passengers,	22,948 29	16,278 33	18,213 11	28,661 47	35,557 19	31,282 15
Way Passengers,	12,277 42	11,224 02	10,557 36	14,015 98	14,998 60	29,455 38
Through Freight,	46,512 57	33,160 99	26,948 58	36,799 93	49,196 46	54,478 05
Way Freight,	51,494 36	37,654 25	36,775 30	50,175 92	52,007 88	62,696 24
Express,	1,076 67	1,688 88	1,688 89	1,688 88	1,688 89	1,688 90
Transport of mails,	2,186 08	2,186 08	2,729 60	2,186 08	2,186 08	2,715 85
Use of engines,						
Use of cars,						
Rent,						1,072 73
Other earnings specified in detail as follows:						371 99
Total,	136,495 39	102,192 55	96,912 84	133,528 26	155,635 10	183,761 29

TABLE I.
ACCIDENTS.

	EMPLOYEES.		OTHERS.	
	Killed.	Injured.	Killed.	Injured.
Trains thrown from track, Struck by bridge while on top of freight cars,		2		1
		1		
Total,		3		1

Total number of persons injured but not killed, 4

JUNE 26, 1868.—Isaac P. Drew, a laborer on section, was drowned at the draw at Missisco Bay, by running a hand-car into the draw when open.

JUNE 29.—John Gardner, from Canada, was run over and killed at St. Albans, in attempting to get on the night express train going south, when the train was in motion.

SEPTEMBER 4.—Charles Patch, conductor of a freight train, fell from his train near Milton, and was run over and killed.

SEPTEMBER 17.—Samuel Connor, an employee in the engine house at White River Junction, was killed there by falling from a car and being run over by an engine.

SEPTEMBER 26.—E. O. Young, conductor of a freight train, when leaning out from his saloon car, was struck by No. 6 Bridge in Berlin, and had his leg broken.

NOVEMBER 28.—J. Crombly, in attempting to get on a freight train at Milton, was run over and killed.

JANUARY 5, 1869.—William Sharpe, a truck man, fell from a gravel train when in motion, and was run over and killed.

APRIL 20.—Alfred Doro, conductor, M. Ryan, fireman, and A. P. Squires, a passenger, on the up night express train, were injured by a train being thrown from the track in consequence of a culvert being washed out near Hartland in the night.

TABLE J.

NUMBER OF EMPLOYEES AND COMPENSATION.

14 Conductors of passenger trains,		
Amount of compensation,	\$50 to 75	per month each.
36 Conductors of freight trains,		
Amount of compensation,	\$50 to 70	do
4 Conductors of wood and gravel trains,		
Amount of compensation,	\$3 to 4	per day each.
2 Master mechanics,		
Amount of compensation, total,	\$5,000	per year.
3 Road masters,		
Amount of compensation,	\$60 to 83 33	per month each.
309 Men in repair shops,		
Amount of compensation,	\$1 to 4 50	per day each.
15 Enginemen of passenger trains,		
Amount of compensation,	\$3 to 3 25	do
31 Enginemen of freight trains,		
Amount of compensation,	\$2 50 to 3 25	do
4 Enginemen of wood and gravel trains,		
Amount of compensation,	\$3 25	do
55 Firemen,		
Amount of compensation,	\$1 75	do
16 Baggage-men,		
Amount of compensation,	\$45 to 50	per month each.
24 Switchmen,		
Amount of compensation,	\$1 50	per day each,
43 Section men, (foremen,)		
Amount of compensation,	\$2 00	do
287 Section hands,		
Amount of compensation,	\$1 50 to 1 60	do
56 Watchmen,		
Amount of compensation,	\$5 to 50 00	per month each.
36 Station agents,		
Amount of compensation,	\$5 to 91 67	do

440 Other laborers,	
Amount of compensation,	75 to 3 00 per day each.
4 Clerks connected with passenger business,	
Amount of compensation,	\$45 to 125 per month each.
10 Clerks connected with freight business,	
Amount of compensation,	\$40 to 250 do
2 Sup'ts of bridges—salary, total,	\$3000 per year.
1 Wood agent,	1,500 do

STATE OF VERMONT, }
FRANKLIN COUNTY, ss. } St. Albans, Aug. 33, 1869.

We depose and say that the facts set forth, and statements made in the foregoing report, which has been signed by us, are true and correct according to the best of our knowledge, information and belief.

Signed,

L. BRAINERD,
JOSEPH CLARK,
J. GREGORY SMITH, } *Trustees and Man-
agers Vt. Central &
Vt. & Canada R. R.*

Subscribed and sworn to before me this 23d day of August, 1869, at St. Albans, Vt.

EUGENE PUTNAM, *Notary Public.*

ANNUAL REPORT OF THE
VERMONT AND MASSACHUSETTS R. R. CO.,

For the Year ending June 30, 1889.

TABLE A.

STOCK AND DEBTS.

Funded debt, redeemable July 1, 1883,	\$550,000 00
Floating debt,	160,154 35
Unpaid dividends,	3,668 50
Rate of interest paid on funded debt, six per cent.	
Rate of interest paid on floating debt, seven per cent.	
on \$134,914 35; eight per cent. on \$10,000,	
and nine per cent. on \$15,000.	
Interest coupons have always been promptly paid	
when due.	

TABLE B.

COST OF CONSTRUCTION.

Total cost of construction, as per last report,	\$3,240,779 31
Expended since last report, viz :	
For graduation and masonry, on Turner's Falls	
branch,	33,127 99

TABLE C.

EQUIPMENT.

Total cost of equipment, as per last report,	\$225,650 20
Total cost of road and equipment,	3,465,878 51
Expended on Turner's Falls branch,	33,127 99

TABLE D.

CHARACTERISTICS OF ROAD.

Length of road,	69 miles.
“ “ completed,	69 “
“ “ branches,	8 “
“ “ side tracks,	5½ “
Weight of rail per yard,	56 lbs.
Length of road, branches and side tracks within the State, 10 miles and 1661 feet; side track, 1 mile.	

CHARACTER AND LENGTH OF BRIDGING.

	No. of structures.	No. of spans.	Length of bridging in feet.
Trestle bridging,			
Truss bridging, 50 feet span and under,	11	11	
Truss do., from 50 to 100 feet span,	5	5	
Truss do., from 100 to 150 feet span,	3	4	
Truss do., 150 feet span and over,	23	41	
Total,	42	61	

Number of road crossings at grade,	60
Number of road crossings above and below grade,	21
Number of cross ties per mile,	2350
Chairs, number per mile,	530
Whole number of switches on main track,	60

GRADIENTS AND ALIGNMENT.

Maximum grade,	58 feet.
Amount of straight line, miles,	31 ¹³ / ₂₀
Amount of curved line, miles,	45 ⁷ / ₁₀

BUILDINGS AND FIXTURES.

Passenger houses,	19
Freight houses,	20
Engine houses,	5
Repair shops,	4
Water stations,	9
Dwellings,	8
Wood sheds,	19
Turn-tables,	5
Office,	1

EQUIPMENT.

Number of locomotives owned by the company on the 30th day of June, 1869 :

	Under 18 tons	16 to 20 tons.	20 to 25 tons.	25 to 30 tons.	30 tons and over
In good repair,			1	7.	
Requiring slight repairs,			1	1	
Requiring heavy repairs,				3	
Worn out,			2		

Number of cars owned by the company, June 30, 1869,	234
First class 8 wheel passenger cars in good repair,	14
First class 8 wheel passenger cars wanting repairs,	2
Second class 8 wheel passenger cars in good repair, none.	
Second class 8 wheel passenger cars wanting repair, none.	
Baggage, express and mail cars in good repair,	7
Baggage, express and mail cars wanting repair,	2
Covered freight and cattle 8 wheel cars in good repair,	103
Covered freight and cattle 8 wheel cars wanting repair,	3
Platform 8 wheel cars in good repair	100
Average weight of passenger cars,	14 tons.
do baggage do	12 "
do box do	8 "
do platform do	6½ "

TABLE E.

BUSINESS OF THE YEAR ENDING NOV. 30, 1868.

Miles run by passenger trains,	137,169
Miles run by freight trains,	81,368
Miles run by gravel, construction and wood trains,	9,453
Number of through passengers carried in cars,	108,578
Number of way passengers,	128,983
Number of passengers having passes,	968
Number of miles traveled by way passengers,	2,064,298
Average distance traveled by way passengers, miles,	16
Number of miles traveled by passengers having passes,	32,949
Number of tons of through freight,	150,184 ³⁸³ / ₁₀₀₀
Number of tons of way freight,	42,609 ²⁹⁹ / ₁₀₀₀
do do do carried 1 mile,	887,742 ⁹¹⁷ / ₁₀₀₀
do do through freight moved toward market,	107,595 ³⁰¹ / ₁₀₀₀
Number of tons through freight moved from market,	42,539 ⁸² / ₁₀₀₀
Number of tons of way freight moved toward market,	27,506 ⁹²⁴ / ₁₀₀₀
Number of tons of way freight moved from market,	15,102 ²⁷³ / ₁₀₀₀
Average rate of speed of ordinary passenger trains,	22 m. per. hour.
Average rate of speed of freight trains,	10 m. per. hour.
Rate of fare charged first-class through passengers per mile	3½ cents.
Rate of fare charged first-class way passengers, per mile,	4 "
Rate per ton per mile charged on 1st class through freight,	7 "
Rate per ton per mile charged on 2d class through freight,	6 "
Rate per ton per mile charged on 3d class through freight,	5 "
Rate per ton per mile charged on 4th class through freight,	4 "
Rate per ton per mile charged on 1st class way freight,	12 "

Rate per ton per mile charged on 2d class way freight,	9 cents.
Rate per ton per mile charged on 3d class way freight,	8 "
Rate per ton per mile charged on 4th class way freight,	5 "
Total number of tons of freight carried one mile,	3,557,670 ⁵⁷³ ₁₀₀₀
Total number of passengers carried one mile,	4,414,382

TABLE F.

EXPENSES OF MAINTAINING ROADWAY AND REAL ESTATE.

For the year ending June 30, 1869.

Ordinary repairs of road-bed and superstructure,	\$92,422 01
Cost of new rails used in repairs,	99,048 79
Number and weight of chairs, weight of spikes, cost of repairs of rails, number of cross ties used for renewals cost of same, cost of re-laying rails and ties, are all included in the above.	
Insurance and taxes on real estate,	5,634 59
Repairs of bridges,	5,816 18
" stations,	13,616 23
" fences,	1,130 55
Total,	<u>\$217,668 27</u>

COST OF REPAIRS OF MACHINERY.

Repairs of engines and tenders, including the cost of three new engines,	\$89,217 09
Repairs of passenger and baggage cars, including the cost of four new passenger cars,	44,234 60
Repairs of freight cars, including the cost of twenty-six new freight cars,	57,689 52
Repairs of tools and machinery in work shops,	3,451 11
Repair of gravel and hand-cars,	910 64
Total,	<u>\$195,502 96</u>

TABLE G.

COST OF OPERATING ROAD.

For the year ending June 30, 1869.

Cost of wood and coal used by locomotives and at stations,	\$42,505 75
Number of cords lost by fire, none.	
The cost of oil and waste for engines and tenders, passenger and baggage cars, and freight cars, was	5,846 05
Damages for injuries to persons and property, including fire, and animals killed on road,	3,336 71
Advertising and stationery,	1,260 68
Expense passenger department,	49,765 51
Expense freight department,	46,186 04
Switchmen, watchmen and gate-keepers,	8,900 30
Wood and water station attendance,	791 79
Rent to Conn. River R. R.,	1,500 00
United States and Mass. State Tax,	20,373
Rent to Troy and Greenfield R. R.,	23,750 00
For salaries of trustees, president, directors, secretaries, treasurer and superintendent,	7,900 01
Incidental expenses,	7,716 09
For law expenses,	1,547 97
Clearing snow,	5,553 40
Total,	<u>\$226,934 04</u>

RECAPITULATION OF EXPENSES.

Maintaining road-way,— <i>See Table F,</i>	\$217,668 27
Repairs of machinery,	195,502 96
Operating,— <i>See Table G,</i>	<u>226,934 04</u>
Total,	<u>\$640,105 27</u>

TABLE H.

EARNINGS, RECEIPTS AND PAYMENTS.

Earnings and Receipts.

From passengers,	\$187,921 50
From freight,	289,507 39
From other sources, viz :	
Tolls,	888 38
Expresses,	9,800 00
Mails,	11,065 27
Rents,	56,017 55
Miscellaneous,	861 77
	<hr/>
	\$555,001 86

Payment other than for Construction.

For transportation expenses, viz :

For passenger, freight and other business,	\$640,105 27
For interest on funded and floating debt,	32,548 07
For dividend No. 5 of 2 per cent. and U. S. Tax,	60,210 52
For carried to surplus fund, (no surplus carried to dividends.)	
For amount of surplus fund, (no surplus carried to dividends.)	

VALUE OF MATERIALS ON HAND.

Wood, 5,840 cords,	\$25,692 50
Oil, 820 gallons,	550 00
Waste, 500 pounds,	100 00
Iron rails, old, iron rails, new, chairs, spikes, ties, iron and other metals, unwrought, iron and other met- als worked and partly worked, and lumber,	49,967 47

DETAILS OF EARNINGS FOR THE YEAR ENDING MAY 31, 1869.

SOURCE.	JULY, 1868.	AUGUST, 1868.	SEPT., 1868.	OCTOBER, 1868.	NOV., 1868.	DEC., 1868.
Through Passengers, Way Passengers, Through Freight, Way Freight, Express, Transport of mails, Tolls, Miscellaneous, Rent,	17,478 97 18,449 49 550 00 500 00 49 00 213 40 3,736 75	21,664 72 22,132 15 550 00 500 00 61 50 38 43 3,293 75	21,375 27 25,137 95 600 00 500 00 81 00 29 04 4,125 00	19,530 42 26,255 22 600 00 500 00 85 63 11 95 3,820 50	15,731 09 24,406 74 1,200 00 500 00 179 00 108 75 7,253 05	12,288 34 24,204 67 900 00 500 00 58 00 74 00 5,138 00
Other earnings specified in detail as follows:						
Total,	40,977 61	48,240 55	51,848 26	50,803 72	49,378 63	43,163 01

DETAILS OF EARNINGS—CONTINUED.

SOURCE,	JAN., 1869.	FEB., 1869.	MARCH, 1869.	APRIL, 1869.	MAY, 1869.	JUNE, 1869.
Through Passengers, Way Passengers, Through Freight, Way Freight, Express, Transport of mails, Tolls, Miscellaneous, Rent, Other earnings specified in detail as follows :	11,237 84 23,348 91 900 00 500 00 55 75 18 36 5,012 00	10,284 17 18,921 58 900 00 500 00 58 25 61 90 4,275 00	13,004 56 26,252 83 900 00 500 00 56 75 7 90 5,125 00	14,467 86 27,296 75 900 00 500 00 52 25 55 60 4,797 25	13,581 76 26,136 22 900 00 500 00 106 50 84 67 4,310 00	17,276 50 25,964 88 900 00 5,565 27 44 75 97 77 5,131 25
Total,	40,072 86	35,000 90	45,847 04	48,069 71	45,619 15	54,980 42

TABLE I.

ACCIDENTS.

Total number of persons killed, two.

NOVEMBER 18, 1869.—At gravel pit between Gardner and Templeton, Dennis Murphy of Gardner, and Michael Morun of Fitchburg, at work in gravel pit were killed by gravel slide. coroner's jury rendered verdict, "An unavoidable accident."

TABLE J.

NUMBER OF EMPLOYEES AND COMPENSATION.

5 Conductors of passenger trains,	
Amount of compensation,	\$75 per month.
2 Conductors of freight trains,	
Amount of compensation,	\$70 to 75 do
1 Conductor of wood and gravel trains,	
Amount of compensation,	\$100 do
1 Master mechanic,	
Amount of compensation,	\$100 do
2 Road masters,	
Amount of compensation,	\$83 33 to 100 do
55 men in repair shops,	
Amount of compensation,	\$1 00 to 3.50 per day.
7 Enginemen of passenger trains,	
Amount of e non,	\$75 to 90 per month.
3 Enginemen of freight trains,	
A mount of compensation,	\$60 to 75 do
1 Engineman of wood and gravel trains,	
Amount of compensation,	\$100 do
11 Firemen,	
Amount of compensation,	\$50 do
6 Baggage men,	
Amount of compensation,	\$50 to 60 do
6 Switchmen,	
Amount of compensation,	\$45 to 60 do

21 Section men, foremen,	
Amount of compensation,	\$2 00 to 2 50 per day.
121 Section hands,	
Amount of compensation,	\$1 60 to 1 85 do
7 Watchmen,	
Amount of compensation,	\$48 to 60 per month.
24 Station agents,	
Amount of compensation,	\$15 to 90 do
15 Other laborers,	
Amount of compensation,	\$12 to 45 do
2 Clerks connected with passenger business,	
Amount of compensation,	\$65 to 90 do
3 Clerks connected with freight business,	
Amount of compensation,	\$60 to 90 do
1 Superintendent of bridges, salary,	\$100 do
2 Freight receiptors,	
Amount of compensation,	\$12 50 to 55 do
1 General passenger agent,	
Amount of compensation,	\$150 do
1 Civil engineer,	
Amount of compensation,	\$100 do
1 Assistant civil engineer,	
Amount of compensation,	\$2 50 per day.
Other agents—how employed—and the salary of each, as follows, viz :	
3 Flagmen,	\$30 to 47 50 per month.
1 Master carpenter,	\$80 do
1 Master of transportation,	\$125 do
4 Telegraph operators,	\$10 to 15 do
1 Toll collector,	\$20 do
3 Brakemen on passenger trains,	\$45 to 50 do
12 do freight trains,	\$45 to 50 do
41 Gravel train men,	\$1 00 to 2 00 per day.
1 Master mechanic's clerk,	\$60 per month.

Amount of net earnings for one year ending
June 30, 1868,

\$101,071 01

Amount of net earnings for one year ending

June 30, 1869,

Amount of funded and floating debt June 30, 1869, \$713,822 85

June 30, 1869, amount of cash in

Treasury,	\$3,958 12
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June 30, 1869, amount of loans,	40,500 00
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June 30, 1869, \$9,500 Vt. &

Mass. R. R. Co. bonds,	8,272 56
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	\$52,730 68
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Amount of interest paid during twelve months

ending June 30, 1869, (more than received

for loans),

\$32,548 70

COMMONWEALTH OF MASS. }

Suffolk County, ss }

24th day of August, 1869.

I, Franklin N. Poor, Treasurer of the Vermont and Massachusetts Railroad Company, do solemnly swear that the above is a true statement of the condition of the finances of said company, heir trustees, or assignees or lessees, on the 30th day of June, 1869.

FRANKLIN N. POOR, *Treasurer.*

Sworn to before me,

P. E. TESCHEMAKER, *Justice of the Peace.*

OFFICERS OF THE COMPANY.

J. C. HOWE, WM. MINOT, Jr., JAMES MCGREGOR, *Trustees.*

DANIEL S. RICHARDSON, *President.*

O. T. RUGGLES, *Superintendent.*

FRANKLIN N. POOR, *Treasurer.*

B. D. LOCKE, *Clerk.*

SALARIES.

Trustees,	\$300, 100 100
President,	2,000 per annum.
Superintendent,	4,000 do
Treasurer,	3,000 do
Clerk,	50 do

COMMONWEALTH OF MASS. }
Suffolk County ss. }

We depose and say that the facts set forth, and statements made in the foregoing report, which has been signed by us, are true and correct according to the best of our knowledge, information and belief.

GEORGE F. FAY, WM. H. HILL, D. S. RICHARDSON, JAS. A. DUPEE, F. GOODHUE,	}	<i>Directors of the Vt. and Mass. R. R. Co.</i>
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Subscribed and sworn to before me, this 24th day of August, 1869, by D. S. Richardson and Jas. A. Dupee.

S. G. SNELLING, *Justice of the Peace.*

Subscribed and sworn to before me, this 24th day of August, 1869, by Wm. H. Hill,

P. E. TESCHEMACHER, *Justice of the Peace.*

Subscribed and sworn to before me, this 25th day of August, 1869, by George F. Fay.

HENRY A. WILLIS, *Justice of the Peace.*

Subscribed and sworn to before me, this 27th day of August, 1869, by F. Goodhue,

R. W. CLARKE, *Notary Public.*

ANNUAL REPORT OF THE ATLANTIC & ST. LAWRENCE RAILROAD CO.,

For the year ending June 30, 1869.

TABLE A.

STOCK AND DEBTS.

Capital stock authorized by charter,	\$4,000,000 00
The amount paid in is	2,494,900 00
Funded debt,	3,324,800 00
Floating debt,	nothing.
Interest on bonds at rate of six per cent.	
Interest on stock at rate of four per cent.	
The coupons are paid by the lessees.	

TABLE B.

COST OF CONSTRUCTION.

Total cost of construction, as per last report,	\$6,788,354 72
Charged to construction account,	nothing.

Two thirds of the road were built at a stipulated price per mile, consequently cannot be apportioned.

TABLE C.

EQUIPMENT.

Total cost of equipments as per last report,	\$865,734 76
Total cost of road and equipment,	7,654,089 48

TABLE D.

CHARACTERISTICS OF ROAD.

Length of road, within State of Vermont,	30½ miles.
“ “ completed, do	do

Length of road branches, of Vermont,	none.
“ “ side tracks, do	about $1\frac{1}{2}$ miles.
Weight of rail per yard,	63 lbs.

CHARACTER AND LENGTH OF BRIDGING.

	No. of structures.	No. of spans.	Length of bridging in feet.
Pile bridging,	1		1,519*
Truss bridging, 50 feet span and under,	10	10	400
Truss do., from 50 to 100 feet span,	2	2	120
Truss do., from 100 to 150 feet span,	14	14	1,750
Truss do., 150 feet span and over,			
Stone arch bridges,	2	2	1
Total,	29	28	3,909

Number of road crossings at grade,	63
Number of road crossings above and below grade,*	6
Number of cross ties per mile,	2400
Chairs, number per mile, about	600
Whole number of switches on main track, about	100

In addition to above bridges, there are on the line 16 iron bridges, 3 of which are 300 feet each in length, in spans of 75 feet; one over the Connecticut river, one over Wild river, and the other over Presumpscot river; whole length of above named bridges, 1510 feet.

GRADIENTS AND ALIGNMENT.

Level, number of miles, and grades to 20 feet,	97.27
Maximum grade,	60 feet.
Amount of straight line, miles,	89
Amount of curved line, miles,	$59\frac{1}{2}$
Maximum radius,	5730 feet.
Minimum radius,	955 do
Sum of ascents going in one direction,	1929 do

* With one draw.

Sum of ascents going in opposite direction, 757 feet.
 Height of termini and summit above tide water, 1178 do

BUILDINGS AND FIXTURES.

Passenger houses,	29
Freight houses,	22
Engine houses,	8
Repair shops,	6
Water stations,	17
Dwellings,	2
Wood sheds,	26
Turn tables,	10

Other buildings, as follows :

Two hotels, 10 store houses on wharves in Portland, for ocean and Boston steamers.

EQUIPMENT.

Number of locomotives used by the company on the 30th day of June, 1869, on the Portland line :

	Under 15 tons	16 to 20 tons.	20 to 25 tons.	25 to 30 tons.	30 tons and over
In good repair,			3	16	12
Requiring slight repairs,				4	2
Requiring heavy repairs,				3	
Worn out,					

Number of cars appropriated by the Company to the
 Portland line, June 30, 1869, 565

NOTE.—The Charter of the Atlantic & St. Lawrence Railroad, within the State of Vermont, extends from the Connecticut River at Bloomfield to the boundary line in Norton. But the entire line from Portland to Montreal is worked in divisions, of which the point of junction is at Island Pond, in Brighton.

TABLE E.

BUSINESS OF THE YEAR ENDING DEC. 31, 1868.

Miles run by passenger trains,	146,269
Miles run by freight trains,	599,707
Miles run by gravel, construction and wood trains,	33,012
Number of through passengers carried in cars,	52,912
Number of way passengers,	142,224
Average rate of speed of ordinary passenger trains,	19 miles per hour.
Average rate of speed of express trains, 19	do
Average rate of speed of freight trains, 11	do

TABLE F.

EXPENSES OF MAINTAINING ROADWAY AND REAL ESTATE.

Books so kept, cannot be answered in detail.

TABLE G.

COST OF OPERATING THE ROAD.

For the year ending Dec. 31, 1868.

Books not classified to conform to above table; answer cannot be given.

RECAPITULATION OF EXPENSES.

Total, year ending Dec. 31, 1868,	\$891,915 14
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TABLE H.

EARNINGS, RECEIPTS AND PAYMENTS.

Earnings and Receipts, for the year ending Dec. 31, 1868.

From passengers,	\$259,469 96
From freight,	795,807 08

From other sources, viz :

Expresses and mails,	\$31,730 67
Rents and miscellaneous,	3,589 21

Payments other than for Construction.

For dividends, 1st of Jan. and 1st of July at rate of 4 per cent. less Government tax.

The statement of receipts and expenses apply to the Portland Division, extending from Island Pond to Portland.

DETAILS OF EARNINGS FOR THE YEAR ENDING 186 .

SOURCE.	JULY.	AUGUST.	SEPTEMBER.	OCTOBER.	NOVEMBER.	DECEMBER.
Through Passengers, Way Passengers, Through Freight, Way Freight, Express, Transport of mails, Use of engines, Use of cars, Rent, Other earnings specified in detail as follows:						
Total,						

NOTE.—These items cannot be given in detail.

DETAILS OF EARNINGS—CONTINUED.

SOURCE.	JANUARY.	FEBRUARY.	MARCH.	APRIL.	MAY.	JUNE.
Through Passengers, Way Passengers, Through Freight, Way Freight, Express, Transport of mails, Use of engines, Use of cars, Rent, Other earnings specified in detail as follows :						
Total,						

NOTE.—These items cannot be given in detail.

TABLE I.
ACCIDENTS.

	EMPLOYEES.		OTHERS.	
	Killed.	Injured.	Killed.	Injured.
Trains thrown from track,	0	0	0	0
Struck by bridge while on top of freight cars,	0	0	0	0
Run over while walking on track,	0	0	0	0
Injured at road crossing,	0	0	0	0
Total,	0	0	0	0

Total number of persons injured but not killed, 1

MARCH 24, 1868.—Joseph Coyel, a laborer—not in the Company's service—had his leg broken by endeavoring to get on to a train in motion at Brighton, Vermont, against the rules of the Company.

TABLE J.

NUMBER OF EMPLOYEES AND COMPENSATION.

Other agents, how employed, and the salary of each, as follows, viz :

The employees of the Company, being employed upon two divisions of the road, one south of Island Pond extending to Portland, the other north of Island Pond extending to the boundary line and Montreal, no specific statement can be made under the head of inquiry, as applied to the State of Vermont.

The Treasurer is required to state the amount of surplus, (if any,) the amount of net earnings, on 186 , also the net earnings up to 186 , and to state amount of payments to surplus fund payments of interest, coupons, on funded debt, and other

disbursements in detail, so as to show the true condition of the finances of the Company on the day of , 186 Such statement may be in the form of a general account, and must be verified by the oath of the Treasurer.

The earnings of the road are received by the lessees. The lessors have no interest in them while the rent is paid, and the accounts are so kept by the Grand Trunk Railway Company as not to admit of an answer to the above as contemplated.

STATE OF MAINE, }
CUMBERLAND COUNTY, SS. } 6th day of September, 1869.

I, Charles E. Barrett, Treasurer of the Atlantic and St. Lawrence Railroad Company, do solemnly swear that the above is a true statement of the condition of the finances of said Company at the time stated.

CHAS. E. BARRETT, *Treasurer.*

Sworn before me,

S. B. HASKELL, *Justice of the Peace.*

OFFICERS OF THE COMPANY.

SALARIES.

Grand Trunk Railway Company of Canada, Lessees.

ST. JOHN SMITH, *President.*

HENRY BAILEY, *Superintendent.*

CHAS. E. BARRETT, *Treasurer.*

STATE OF VERMONT, }
COUNTY, SS. }

We, the undersigned, Directors of the Atlantic and St. Lawrence Railroad Company, depose and say that the facts set forth, and statements made in the foregoing report, which

has been signed by Charles E. Barrett, Treasurer, are true and correct according to the best of our knowledge, information and belief.

Signed,

Subscribed and sworn to before me this day of
1869.

CHAS. E. BARRETT,
N. NARMORE,
H. I. LIBBY,
BYRON GREENOUGH,
P. BARNES.

BLANK FORM OF RAILROAD REPORT.

The following is the blank form furnished the Officers and Managers of the several Vermont Railroads :

<i>Annual Report of the Company, for the year ending</i>	<i>Railroad 186</i>
--	-------------------------

TABLE A.

STOCK AND DEBTS.

NOTE.—Under this head state the amount paid for interest, discount, &c., amount of funded debt, floating debt—rate of interest—amount of interest coupons due and unpaid.

TABLE B.

COST OF CONSTRUCTION.

Total cost of construction as per last report,
 Expended since last report, viz :
 For graduation and masonry,
 do bridges,
 do rails,
 do chairs, spikes and ties,
 do laying superstructure,
 do buildings and fixtures,

NOTE.—State, also, the progress of the work, cost of graduation, superstructure, and all incidental expenses, or any extension or alteration of road, to

186

Also, state the amount for interest, discount, &c., charged to construction account

TABLE C.

EQUIPMENT.

Total cost of equipment as per last report,
 Expended since last report,
 Total cost of road and equipment,

TABLE D.

CHARACTERISTICS OF ROAD.

Length of road,
 do completed,
 do branches,
 do side tracks,

Weight of rail per yard,

NOTE.—State length of road, branches and side tracks within the State.

CHARACTER AND LENGTH OF BRIDGING.

	No. of structures.	No. of spans.	Length of bridging in feet.
Trestle bridging,			
Truss bridging, 50 feet span and under,			
Truss do., from 50 to 100 feet span,			
Truss do., from 100 to 150 feet span,			
Truss do., 150 feet span and over,			
Draw bridges,			
Total,			

Number road crossings at grade,
 Number of road crossings above and below grade,
 Number of cross ties per mile,
 Chairs, number per mile,
 Whole number of switches on main track,

GRADIENTS AND ALIGNMENT.

Level, number of miles,
 Maximum grade,
 Amount of straight line, miles,
 Amount of curved line, miles,
 Maximum radius,
 Minimum radius,
 Sum of ascents going in one direction,

Sum of ascents going in opposite direction,
 Hight of termini and summit above tide water,

BUILDINGS AND FIXTURES.

Passenger houses,
 Freight houses,
 Engine houses,
 Repair shops
 Water stations,
 Dwellings,
 Wood sheds,
 Turn-tables,

Other buildings, as follows :

EQUIPMENT.

Number of locomotives owned by the company on the
 day of 186

	Under 15 tons	16 to 20 tons.	20 to 25 tons.	25 to 30 tons.	30 tons and over
In good repair,					
Requiring slight repairs,					
Requiring heavy repairs,					
Worn out,					

Number of cars owned by the company, 186

First class 8 wheel passenger cars in good repair,
 First class 8 wheel passenger cars wanting repairs,
 Second class 8 wheel passenger cars in good repair,
 Second class 8 wheel passenger cars wanting repair,
 Baggage, express and mail cars in good repair,
 Baggage, express and mail cars wanting repair,
 Covered freight and cattle 8 wheel cars in good repair,
 Covered freight and cattle 8 wheel cars wanting repair,
 Platform 8 wheel cars in good repair,
 Other freight cars,

Gravel cars,
 Average weight of passenger cars,
 do baggage do
 do box do
 do platform do

TABLE E.

BUSINESS OF THE YEAR.

Miles run by passenger trains,
 Miles run by freight trains,
 Miles run by gravel and construction trains,
 Miles run by wood trains,
 Number of through passengers carried in cars,
 Number of way passengers,
 Number of passengers having passes,
 Number of miles traveled by way passengers,
 Average distance traveled by way passengers,
 Number of miles traveled by passengers having *passes*,
 Number of tons of through freight,
 Number of tons of way freight,
 Number of tons of way freight carried 1 mile,
 Number of tons of through freight moved towards market,
 Number of tons of through freight moved from market,
 Number of tons of way freight moved towards market,
 Number of tons of way freight moved from market,
 Average rate of speed of ordinary passenger trains,
 Average rate of speed of express trains,
 Average rate of speed of freight trains,
 Rate of fare charged first class through passengers per mile,
 Rate of fare charged first class way passengers per mile,
 Average rate of fare charged second class passengers per mile,
 Rate per ton per mile charged on 1st class through freight,
 Rate per ton per mile charged on 2d class through freight,
 Rate per ton per mile charged on 3d class through freight,
 Rate per ton per mile charged on 4th class through freight,

Rate per ton per mile charged on 1st class way freight,
 Rate per ton per mile charged on 2d class way freight,
 Rate per ton per mile charged on 3d class way freight,
 Rate per ton per mile charged on 4th class way freight,
 Total number of tons of freight carried 1 mile,
 Total number of passengers carried 1 mile,

TABLE F.

EXPENSES OF MAINTAINING ROADWAY AND REAL ESTATE.

For the year ending	186 .
Ordinary repairs of road-bed and superstructure,	\$
Extraordinary repairs of road-bed,	
Cost of new rails used in repairs,	
Number and weight of chairs,	
Weight of spikes,	
Cost of repairs of rails,	
Number of cross ties used for renewals,	
Cost of same,	
Cost of re-laying rails and ties,	
Insurance and taxes on real estate,	
Repairs of bridges,	
do stations,	
do fences,	
do masonry,	
Total,	

COST OF REPAIRS OF MACHINERY.

Repairs of engines and tenders,
 Repairs of passenger and baggage cars,
 Repairs of freight cars,
 Repairs of tools and machinery in work shops,
 Oil used about work shops,
 Fuel,
 Waste,
 Other items in detail as follows :
 Total,

TABLE G.

COST OF OPERATING THE ROAD.

For the year ending 186 .

Wood, including the cost of preparing the same,	
Number of cords of wood used by locomotives,	
Number of tons of coal used by locomotives,	
Number of cords of wood used at stations,	
Number of tons of coal used at stations,	
Number of cords lost by fire,	
Number of gallons of oil,	
Number of pounds of waste,	
Cost of oil and waste for engines and tenders,	
Cost of oil and waste for passenger and baggage cars,	
Cost of oil and waste for freight cars,	
Loss and damage of goods,	
Loss and damage of baggage,	
Damages for injuries to persons,	
Damages to property, including fire, and animals killed on road,	
Office expenses and stationery,	
Number of agents,	
Number of clerks,	
Labor loading and unloading freight,	
Porters and watchmen,	
Switchmen,	
Wood and water station attendance,	
Conductors and baggagemen,	
Brakemen,	
Enginemen and firemen,	
For salaries of trustees, president, directors, secretaries, treasurer and superintendent,	
For printing, stationery and office expenses,	
For law expenses,	
Other expenses in detail, as follows :	
Total,	

RECAPITULATION OF EXPENSES.

Maintaining roadway,
 Repairs of machinery,
 Operating,
 Proportion of expenses due to passenger business,
 Proportion of expenses due to freight business,
 Total,

TABLE H.

EARNINGS, RECEIPTS AND PAYMENTS.

Earnings and Receipts.

From passengers,
 From freight,
 From other sources, namely :
 Expresses,
 Mails,
 Rents,
 Miscellaneous,

Payments other than for Construction.

For transportation expenses, viz :
 For passenger business,
 For freight business,
 For other business, and what,
 For interest on funded debt,
 For interest on floating debt,
 For dividends,
 For carried to surplus fund,
 For amount of surplus fund,

VALUE OF MATERIALS ON HAND.

Wood, cords of,
 Coal, tons of,
 Oil, gallons of,
 Waste, pounds of,

Iron rails, tons of, old,
Iron rails, tons of, new,
Chairs, pounds of,
Spikes, pounds of,
Ties, number of,
Iron and other metals, unwrought,
Iron and other metals, worked and partly worked,
Lumber,

Other items specified as follows :

COST OF TRANSPORTATION.

Actual cost of transporting freight per ton, per mile,
Actual cost of transporting passengers, per mile,

DETAILS OF EARNINGS FOR THE YEAR ENDING 18

SOURCE.	JULY.	AUGUST.	SEPTEMBER.	OCTOBER.	NOVEMBER.	DECEMBER.
Through Passengers, Way Passengers, Through Freight, Way Freight, Express, Transport of mails, Use of engines, Use of cars, Rent, Other earnings specified in detail as follows :						
Total,						

DETAILS OF EARNINGS—CONTINUED.

SOURCE,	JANUARY.	FEBRUARY	MARCH.	APRIL.	MAY.	JUNE.
Through Passengers, Way Passengers, Through Freight, Way Freight, Express, Transport of mails, Use of engines, Use of cars, Rent, Other earnings specified in detail as follows:						
Total,						

TABLE I.

ACCIDENTS.

State the number of persons injured in life or limb, and the cause thereof, and whether passengers or persons employed.

State whether any such accidents have arisen from carelessness or negligence of any person in the employ of the corporation, and whether such person is retained in the service of the corporation.

	EMPLOYEES.		OTHERS.	
	Killed.	Injured.	Killed.	Injured.
Trains thrown from track, Struck by bridge while on top of freight cars, Run over while walking on track, Injured at road crossing,				
Total,				

Total number of persons killed,

Total number of persons injured but not killed,

In addition to which must be given a statement of the date of each accident, the place where it occurred, the train, the cause and the extent of the injuries inflicted upon each person, and the name of such person.

TABLE J.

NUMBER OF EMPLOYEES AND COMPENSATION.

Conductors of passenger trains,

Amount of compensation,

Conductors of freight trains,

Amount of compensation,

Conductors of wood and gravel trains,

Amount of compensation,

Master mechanics,
Amount of compensation,
Road masters,
Amount of compensation,
Men in repair shops,
Amount of compensation,
Enginemen of passenger trains,
Amount of compensation,
Enginemen of freight trains,
Amount of compensation,
Enginemen of wood and gravel trains,
Amount of compensation,
Firemen,
Amount of compensation,
Baggagemen,
Amount of compensation,
Switchmen,
Amount of compensation,
Section men, (foremen,)
Amount of compensation,
Section hands,
Amount of compensation,
Watchmen,
Amount of compensation,
Station agents,
Amount of compensation,
Other laborers,
Amount of compensation,
Clerks connected with passenger business,
Amount of compensation,
Clerks connected with freight business,
Amount of compensation,
Superintendent of bridges—salary,
Wood agent—salary,

Other agents—how employed—and the salary of each,
as follows viz :

The Treasurer is required to state the amount of surplus,
(if any,) the amount of net earnings, on

186 , also the net earnings up to 186
and to state amount of payments to surplus fund
payments of interest, coupons, on funded debt, and other
disbursements in detail, so as to show the true condition of
the finances of the Company on the day of
186 Such statement may be in the form of a general
account, and must be verified by the oath of the Treasurer.

STATE OF VERMONT, }
COUNTY, SS. } day of 186

I, , Treasurer of the
Railroad Company, do solemnly
swear that the above is a true statement of the condition of
the finances of said company, their trustees, or assignees or
lessees, on the day of , 186 .
Treasurer.

Sworn before me,

Justice of the Peace.

OFFICERS OF THE COMPANY.

SALARIES.

Trustees,
President,
Superintendent,
Treasurer,

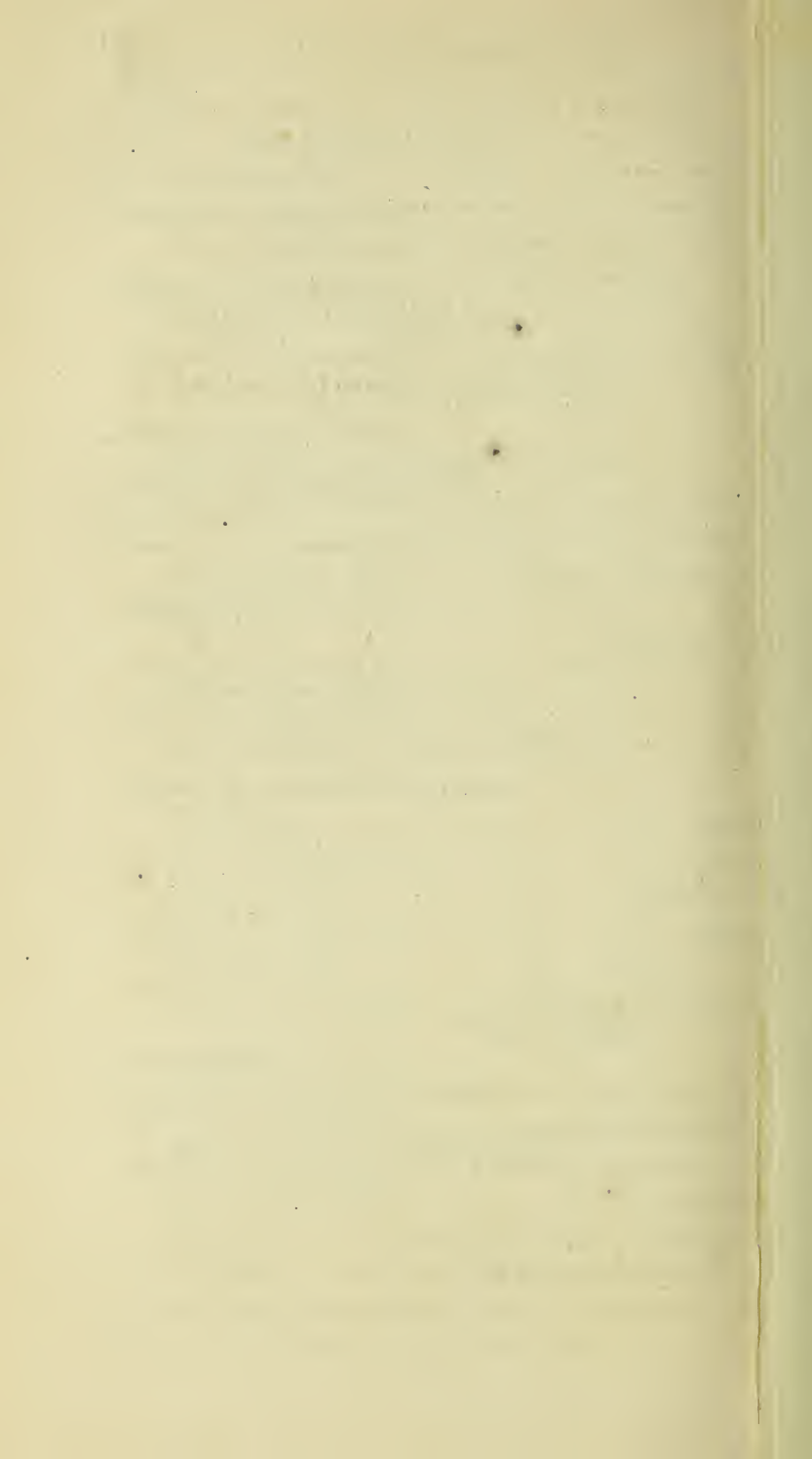
NOTE.—State the amount of each.

STATE OF VERMONT, }
COUNTY, SS. }

depose and say
that the facts set forth, and statements made in the foregoing re-
port, which has been signed by
are true and correct according to the best of knowledge,
information and belief.

Signed,

Subscribed and sworn to before me this day of
186



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BIENNIAL REPORT

OF THE

Railroad Commissioner

OF THE

STATE OF VERMONT,

FOR

1871-72.



MONTPELIER:

FREEMAN STEAM PRINTING HOUSE AND BINDERY.

1872.



BIENNIAL REPORT

OF THE

RAILROAD COMMISSIONER,

FOR

1871-72

THE
LIBRARY
OF THE
MUSEUM
OF
COMPARATIVE ZOOLOGY
AT
HARVARD UNIVERSITY
CAMBRIDGE, MASS.

365.3

V59

1870/72

REPORT.

To His Excellency, the Governor of Vermont:

The undersigned has the honor to submit the following report upon the condition and management of the Vermont railroads.

Agreeably to the statute defining the duties of Railroad Commissioner, I furnished the officers and managers of the several railroad companies in Vermont with the necessary blank forms of reports required of them, and so far as I am in receipt of those reports, they are herewith submitted.

I have traveled over the different railroads in Vermont several times during the past two years, and have made such examination of the several roads and their management as I could under the circumstances. Some of our roads have improved considerable since my last report; at that time they had not got the roads entirely repaired from the damages occasioned by the violent storm of October, 1869, but at the present time the effects of that storm upon the roads have almost entirely disappeared, and the roads are now in very good repair through the State.

I am happy to state that during the past two years, while in other States and the Canadas accidents on railroads have been frequent and many of them disastrous, the Vermont roads have been free from accidents of any magnitude, only those minor ones that have always followed the running of railroads. Great credit is due to the managers of railroads in Vermont for the past two years, for the safety with which

they have carried the traveling public ; and may all railroad managers and employees be ever mindful of the great responsibility that rests upon them, where so many thousands of lives are committed to their care.

The general management of passenger trains upon Vermont roads has been without complaint so far as I have known. Conductors and other employees of the several roads have been uniformly courteous and attentive, as well as efficient for the safety of passengers, while passing over their respective roads.

There is one very important fact that should be brought to the attention of the Legislature, and that is that there is now no law in this State regulating the speed of passenger trains run upon Vermont roads. My attention has been more particularly called to this fact at my late examination of the roads the past summer, and from my own observation upon the subject I am fully confirmed in my mind as to the need of some law upon that particular subject ; it is dangerous in my opinion to leave so important a matter to be determined by the courage or ambition of one single man, unless he is subject to some severe penalty for violating any rule or law that may govern him, when so many lives are at stake.

Our railroads are not laid in straight lines, but are built along the hill sides and river banks in curved lines, so that speed must necessarily be slower, with the same amount of danger, than on straight lines ; our roads are not perfect and never can be, though comparatively in as good repair as those of any State in the Union ; yet there should be some rule or law to govern the speed of passenger trains, and a penalty corresponding to the magnitude of the trust should be imposed upon those violating such rule or law.

I would recommend that passenger trains should not be run at a speed faster than twenty-five miles to the hour.

Within the past two years quite a number of miles of new roads have been constructed and put in operation within the State, and still more are now in process of construction.

The Montpelier and Wells River Railroad, extending from Montpelier to Wells River, a distance of some thirty-eight miles, now nearly graded and ready for the rails and ties. I herewith submit a statement furnished me by the officers of said road.

The Portland and Ogdensburg Railroad Line, Vermont division, is one hundred and seventeen miles in length. Nearly fifty miles are now in operation. The iron is now going down upon twenty miles additional, of the remaining forty-seven miles a considerable part is graded. The average cost per mile thus far, for building and equipping the road has been \$25,000. The cash stock subscription amounts to about \$1,200,00, as reported by the officers and executive committee, building said road. The Vermont division comprises the entire road through Vermont, commencing at the Connecticut River, and by the way of St. Johnsbury and the Lamoille Valley to Swanton. The eastern division, extending from the Connecticut River to Portland, is about one hundred and ten miles in length. Sixty miles are now in operation, and the work is now proceeding energetically upon the unfinished portion, and will be completed to connect with the Vermont division at an early day. These roads when completed, are to be a trunk line forming a connection from the Great Lakes to the Ocean, and when constructed and in operation will develop much of the resources and add to the business prosperity and wealth of the State.

The Missisquoi Railroad has been built some twenty-eight miles, and is in operation; also the Missisquoi and Clyde Rivers road is now being constructed.

There has been about one hundred miles of railroad built and now in operation in the State, within the last year. This increase of roads in our State would in a short time

give us railroad facilities equal to any State in the Union. There are quite a number of new projects for roads that have not yet obtained charters, and a very grave question has been raised in the State, in relation to building railroads on the narrow gauge plan.

The narrow gauge system received its impetus in the hills of Wales. There is where it was conceived, and practically developed. The entire system grew from a little tramway, first used in bringing slate from the mountains, and which grew to be a passenger, and then to a general freight road. The question of narrow gauge road and its adaption to all parts of the country, has been lately very thoroughly investigated, and as far back as 1850, both the Stevensons in England recommended for passenger trains, engines of only seventeen tons weight, and for freight eighteen tons weight, with carriages and cars three to three and one-half tons. They made frequent mention of the importance of the proper proportioning of the "dead" to the remunerative weight in all the rolling stock; they even advocated a light description of engine and car combined, for local travel.

The civil engineer of the Denver and Rio Grand Railroad, says: "The question as to what shall be the standard for these railways in America seems now to be no longer open, all best authorities agreeing with us in the adoption of three feet as the gauge which best combines the strong points of both systems, while having the fewest of their disadvantages. It was adopted by the managers after careful consultation with the best authorities in this and in transatlantic countries."

The Denver and Rio Grand Railroad has now a completed length of about 120 miles, which has been in successful operation for some time, built upon the narrow gauge plan of three feet gauge and built at a cost per mile, with equipments of about \$13,500. This road is practically demon-

strating the superiority of the new system over the old, by having undertaken to construct a three feet gauge road over the almost entirely undeveloped country lying between the Union Pacific and the Rio Grand, a distance of about eight hundred and fifty miles.

Now assuming that a passenger train on the standard guage should contain five passenger cars, and to carry the same number of passengers on the three feet gauge should take seven cars, and we have the following :

ON 4 FEET $8\frac{1}{2}$ INCH GAUGE.

Five passenger cars, at 34,000 lbs. each,	85 tons
One baggage car,	13 "
One express car,	13 "
Engine and tender,	45 "
Weight of passengers,	10 "
	<hr/>
Total weight of train,	166 "

ON 3 FEET GAUGE.

Seven passenger cars, at 16,000 lbs. each.	56 tons
One baggage car,	7 "
One express car,	7 "
Engine and tender,	25 "
Weight of passengers,	10 "
	<hr/>
Total weight of train,	105 "

This gives the narrow gauge sixty-one tons less "dead" weight to haul, to carry the same number of passengers in one train, than the ordinary gauge. The above table may be considered a fair statement, although it does not go so far in favor of the narrow gauge as several statements that have been made by railroad men and engineers, and the same or nearly the same rules may be applied to trains carrying freight.

The general development of the country can only be accomplished by an extended system of railways, and

quick developments can best be obtained by some system of cheap transportation; and after comparing the cost of constructing and operating the narrow gauge with the standard gauge, we must safely conclude that the narrow gauge railway is by far the best means for a general and quick development of our resources. Costing only about one-half as much as the broad gauge, it is within the means of all sections to build them, and will enable certain sections to avail themselves of railway facilities where otherwise they would be compelled to dispense with them. From their small costs, light operating expenses and small interest account, they will prove to be paying investments to the stockholders. Cheapening transportation, they will develop dormant interest more rapidly and penetrate sections of country where the more costly roads cannot be built, and enhance the value of property largely in excess of their cost. The great want of the age is cheaper transportation, and this we cannot have without cheaper railways.

We have at this time about sixty thousand miles of broad-gauge railway in operation in America. Poor's Railway Manual estimates the average cost of these roads at fifty thousand dollars per mile, and in round numbers they represent an expenditure of three thousand million dollars.

I have thus briefly referred to this new system of narrow gauge, that the public and railroad men may investigate the subject, when brought to their notice in connection with Vermont Railroads.

R. F. PARKER, *Railroad Commissioner.*

Wolcott, August 31, 1872.

BIENNIAL REPORT OF THE VERMONT CENTRAL AND VERMONT & CANADA RAILROAD COMPANIES.

For the two Years ending May 31, 1872.

TABLE D.

CHARACTERISTICS OF ROAD.

Total length of road, Vermont Central,	117 miles.
Total length of road, Vermont and Canada,	65½ "
Total length of road in Vermont,	182½ "
Length of road completed,	182½ "
Length of branches,	2 "
Length of side track,	39 "
Weight of rails per yard,	58 to 63 pounds.

CHARACTER AND LENGTH OF BRIDGING.

	No. of structures.	No. of spans.	Length of bridging in feet.
Trestle bridging,	7		7985
Truss bridging, 50 feet span and under,	46	57	1695
Truss do. from 50 to 100 feet span,	6	6	357
Truss do. from 100 to 150 feet span,	19	30	3681
Truss do. 150 ft. span and over	13	27	4240
Draw bridges,	2		244
Total,	93	120	18202

Number of road crossings at grade,	126
Number of road crossings above and below grade,	34

Number of cross ties per mile,	2600
Chairs, number per mile,	400 to 520
Whole number of switches on main track,	150

GRADIENTS AND ALIGNMENT.

Amount of straight line, miles,	83
Amount of curved line, miles,	34
Maximum radius,	11460 feet.
Minimum radius,	1146 “

BUILDINGS AND FIXTURES.

Passenger houses,	38
Freight houses,	44
Engine houses,	5
Repair shops,	3
Water stations,	30
Dwellings,	25
Wood sheds,	46
Turn tables,	5

TABLE E.

BUSINESS OF THE LAST TWO YEARS.

Miles run by passenger trains,	891,429
Miles run by freight trains,	2,004,336
Miles run by gravel and construction trains,	113,991
Miles run by wood trains,	65,626
Number of through passengers carried in cars,	460,514 1-2
Number of way passengers,	492,802 1-2
Number of miles traveled by way passengers,	9,963,821
Average distance traveled by way passengers,	20 1-5 miles.
Number of tons of through freight,	1,189,824
Number of tons of way freight,	643,836
Number of tons of way freight carried 1 mile,	41,524,228
Average rate of speed of ordinary passenger trains,	22 miles per hour.
Average rate of speed of express trains,	27 “ “
Average rate of speed of freight trains,	10 “ “

Rate of fare charged first class through passengers per mile.	3 to 4 cents.
Average rate of fare charged second class passengers per mile,	2 to 3 cents.
Total number of tons of freight carried 1 mile,	163,850,864
Total number of passengers carried 1 mile,	35,622,212

TABLE F.

The total expenses of maintaining roadway and real estate for the two years ending July 31, 1872, were	\$941,202 87
The total cost of repairs of machinery was	\$618,126 24

TABLE G.

COST OF OPERATING THE ROAD FOR THE LAST TWO YEARS ENDING
JULY 31st, 1872.

Wood, including the cost of preparing the same,	\$502,347 75
Cost of oil and waste for engines and tenders,	
passenger, baggage and freight cars,	74,649 62
Freight expense,	19,071 58
Passenger expense.	23,926 02
Mail expense.	297 66
Advertising,	5,205 16
Loss and damage of goods,	10,141 81
Loss and damage of baggage,	475 01
Damages for injuries to persons,	15,240 87
Damages to property, including fire, and animals killed on road,	2,156 17
Agents at stations,	92,219 57
Clerks at general office	47,468 48
Labor, loading and unloading freight,	88,889 10
Porters and watchmen,	19,551 35
Switchmen,	19,987 53
Conductors, baggagemen and brakemen,	206,048 74
Depot and station furniture and tools,	10,183 96
Enginemen and firemen,	150,073 19

For salaries of trustees, president, directors, secretaries, treasurer and superintendent,	37,692 13
For printing, stationery and office expenses,	25,844 39
For law expenses,	7,572 88
Other expenses,	86,519 26
Total,	\$1,445,561 23

RECAPITULATION OF EXPENSES.

Maintaining roadway,	941,202 87
Repairs of machinery,	618,126 24
Operating,	1,445,561 23
Total,	\$3,004,890 34

TABLE H.

EARNINGS, RECEIPTS AND PAYMENTS.

Earnings and Receipts.

From passengers,	\$1,255,679 08
From freight,	3,030,393 84
From other sources, viz :	
Expresses,	40,533 25
Mails,	68,729 12
Miscellaneous,	48 88
Total,	\$4,395,384 17

The gross earnings of the Rutland Railroad and branches for 17 months to May 31st, 1872, were \$1,869,584 64.

Expenses for same period, \$1,333,636 48.

TABLE I.

ACCIDENTS.

JULY 29, 1870.—J. Sullivan, laborer on a gravel train, in attempting to get upon a freight train at West Hartford, fell under the car and was killed.

OCTOBER 10, 1870.—A freight train ran over an ox at Braintree, throwing ten cars from the track, and C. A. Bell,

a brakeman, was crushed amongst the broken cars and killed.

NOVEMBER 11, 1870.—A. E. Stockwell, a brakeman on a freight train, was killed in Roxbury. Several cars in the train being thrown from the track by breaking of an axle.

NOVEMBER 16, 1870.—The body of an unknown man was found by the track near Jewett's crossing, about three miles north from St. Albans station. Supposed to have been run over by a passenger train during the night.

JANUARY 6, 1871.—A. Bucklass, a freight brakeman, had his hand severely injured while shackling cars at Swanton.

JANUARY 17, 1871.—S. E. Kent, a freight conductor, fell from a car at Northfield, breaking one of his ribs and one of his wrists, and bruised his body badly.

MARCH 14, 1871.—Anthony Gearney, employed in the wood shed at St. Albans station, was run over by a passing train, and injured so that he died the following day.

APRIL 8, 1871.— — Gannon, a freight brakeman, fell from a car at Royalton, and bruised his knee badly.

APRIL 15, 1871.—Matthew Curran, in charge of a car of horses, attempted to get upon a freight train as it was leaving the station at St. Albans, fell under the train and was injured so that he died April 18.

JULY 1, 1871.—D. B. Morgan, a fireman, fell from a locomotive when near White River Junction, and was killed almost instantly.

JULY 22, 1871.—The mutilated body of William Kirkpatrick, an employee in the paint shop of the road at St. Albans, was found in the passenger depot, in that town, in the morning, apparently having been run over by passenger cars, whilst being placed in the depot during the night.

AUGUST 15, 1871.—L. L. Alexander, a freight conductor, was struck by a bridge, while on top of a car near Essex Junction, and was injured so that he died the same day.

SEPTEMBER 11, 1871.—F. N. Parker, a freight brakeman, while attempting to disconnect a train at Waterbury, had his hip dislocated.

SEPTEMBER 12, 1871.—A child of Patrick Fox, a sectionman, and living near the track about one mile from Essex Junction towards Burlington, was struck by the locomotive of a freight train, and injured so that it died.

OCTOBER 2, 1871.—George Brown, a freight brakeman, fell between cars in the yard at White River Junction, and was badly bruised.

DECEMBER 14, 1871.—A. S. Braley, a freight brakeman, fell off train at Sharon, and was badly hurt.

DECEMBER 15, 1871.—S. Mott, a freight brakeman, was struck by water spout at Jonesville station. Collar bone broken and received a severe scalp wound.

JUNE 28, 1872. Mrs. Mary Kelley, an elderly woman, while attempting to drive a cow from the railroad track at Essex Junction, was run over by a freight train and injured so that she died the same day.

	EMPLOYEES.		OTHERS.	
	Killed.	Injured.	Killed.	Injured.
Trains thrown from track,	2			
Struck by bridge while on top of freight cars,	1			
Run over while walking on track,			1	
Injured at road crossing,				
Total,	3		1	

Total number of persons killed, eleven.

Total number of persons injured but not killed, seven.

TABLE J.

NUMBER OF EMPLOYEES AND COMPENSATION.

17 Conductors of passenger trains,
Amount of compensation, \$60 to 75 per month.

42 Conductors of freight trains,	
Amount of compensation,	\$50 to 70 per month.
5 Conductors of wood and gravel trains,	
Amount of compensation,	\$3 to 4 per day
3 Master mechanics,	
Amount of compensation,	\$6,400 per year.
4 Road masters,	
Amount of compensation,	\$60 to 83 33 per month.
525 Men in repair shops,	
Amount of compensation,	\$1 to 4 50 per day.
16 Enginemen of passenger trains,	
Amount of compensation,	\$2 50 to 3 25 per day.
47 Enginemen of freight trains,	
Amount of compensation,	\$250 to 3 25 per day.
5 Enginemen of wood and gravel trains,	
Amount of compensation,	\$3 00 to 3 25 per day.
68 Firemen,	
Amount of compensation,	\$1 75 per day.
18 Baggage men,	
Amount of compensation,	\$35 to 45 per month.
37 Switchmen,	
Amount of compensation,	\$1 to 1 75 per day.
52 Section men, (foremen),	
Amount of compensation,	\$2 to 2 50 per day.
380 Section hands,	
Amount of compensation,	\$1 50 per day.
54 Watchmen,	
Amount of compensation,	\$1 to 1 50 per day.
38 Station agents,	
Amount of compensation,	\$12 to 100 per month.
495 other laborers,	
Amount of compensation,	\$1 to 3 per day.
8 Clerks connected with passenger business,	
Amount of compensation,	
28 Clerks connected with freight business,	
Amount of compensation,	

2 Superintendents of bridges—salary

1 Wood agent

“

Does any conductor or engineman in the employ of your road indulge in intoxication?

Not to our knowledge.

Please answer the same question as applied to section men, brakemen and switchmen.

Not to our knowledge.

Are the trains on your road allowed to stand across highway crossings to the unnecessary delay of highway travel or business?

They are not.

Do your station men allow boys without business to frequent and remain about the trains while stopping at stations?

They do not ordinarily.

Are your road fences and cattle-guards such as the law demands?

We think they are.

STATE OF VERMONT, } We, J. Gregory Smith, Jo
FRANKLIN COUNTY, ss. } seph Clark, and Worthington
C. Smith, depose and say that the facts set forth, and statements made in the foregoing report, which has been signed by ourselves, are true and correct according to the best of our knowledge, information and belief.

Signed,

J. GREGORY SMITH.

JOSEPH CLARK.

WORTHINGTON C. SMITH.

Subscribed and sworn to before me this 14th day of October, A. D. 1872.

GUY C. NOBLE,

Master in Chancery.

BIENNIAL REPORT OF THE
CONNECTICUT AND PASSUMPSIC RIVERS AND
MASSAWIPPI VALLEY RAILROAD COMPANY.

For the years ending June 30, 1872.

TABLE B.

COST OF CONSTRUCTION.

Total cost of construction as per last report, (July '71)	\$314,538,00
Expended since last report, viz :	
For graduation and masonry, bridges, rails, chairs, spikes and ties, laying super- structure, and for buildings and fixtures,	240,155 60

TABLE D.

CHARACTERISTICS OF ROAD.

Total length of road,	148 miles.
Total length of road in Vermont and through what towns it runs,	110 “
Length of road completed,	148 “
Length of branches,	3 “
Weight of rails per yard,	66 lbs.

	No of structures.	No. of spans.	Length of bridging in feet.
Trestle bridging,	22		319
Truss bridging, 50 feet span and under,	12	12	428
Truss do., from 50 to 100 feet span,	21	21	1489
Truss do., from 100 to 150 feet span,	8	11	1393
Truss do., 150 feet span and over,	7	7	1137
Draw bridges,			
Totals,			

BUILDINGS AND FIXTURES.

Passenger houses,	31
Freight houses,	32
Engine houses,	4
Repair shops,	1
Water stations,	19
Dwellings,	25
Woodsheds,	30
Turn tables,	3

EQUIPMENT.

Number of locomotives owned by the company on 30th day of June, 1872.

	Under 18 tons.	16 to 20 tons.	20 to 25 tons.	25 to 30 tons.	30 tons and over
In good repair,			4	4	14
Requiring slight repairs,					
Requiring heavy repairs,					
Worn out,					
Total,					22

First class 8 wheel passenger cars in good repair,	13
Baggage, express and mail cars in good repair,	7
Baggage, express and mail cars wanting repair,	2
Covered freight and cattle 8 wheel cars, in good repair,	235
Platform 8 wheel cars in good repair,	330
Other freight cars, platforms with racks,	93
Gravel cars,	20
Freight saloons,	7

TABLE E.

BUSINESS OF THE LAST TWO YEARS.

Miles run by passenger trains,	494,673
Miles run by freight trains,	416,552
Miles run by gravel and construction trains,	51,433
Miles run by wood trains,	11,243
Average rate of speed of ordinary passenger trains,	25 miles per hour.
Average rate of speed of express trains,	30 " "
Average rate of speed of freight trains,	15 " "
Total number of tons of freight carried one mile,	20,771,835
Total number of passengers carried one mile,	10,603,046

TABLE F.

The expenses of maintaining roadway and real estate for the two years, including ordinary and extraordinary repairs of road bed and superstructure, new rails used in repairs and relaying the same, insurance and taxes on real estate, were	\$333,102 29
Number and weight of chairs, weight of spikes, and number of cross ties used for renewals,	844,72
Repairs of bridges,	\$17,406 53
New buildings,	30,980 14

COST OF REPAIRS OF MACHINERY.

Repairs of engines, tenders, passenger, baggage and freight cars,	\$169,925 09
Repairs of tools and machinery in workshops,	7,055 22
Waste and oil used about workshops and trains,	13,001 64
Total,	<u>\$189,981 95</u>

TABLE G.

COST OF OPERATING THE ROAD FOR THE LAST TWO YEARS.

Wood, including the cost of preparing the same,	\$106,732 52
Loss and damage of goods, baggage, injuries to persons, and damages to property, including fire and animals killed on road,	25,633 99
Number of agents at stations,	34
Number of clerks,	13
Porters and watchmen,*	9
Switchmen,*	2
16 conductors and 5 baggagemen,	21
Brakemen,	48
Enginemen 27 and firemen 27,	54
For printing and stationery,	8,073 39
For law expenses, insurance, interest, taxes, and salaries, and general expenses.	42,315 95

RECAPITULATION OF EXPENSES.

Maintaining roadway,	333,102 28
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TABLE H.

EARNINGS, RECEIPTS AND PAYMENTS.

From passengers,	\$494,145 96
From freight,	961,126 20

* Work mostly done by train men and station agents.

From other sources, viz :

Rents,	\$111,514 72
Expresses,	15,000 00
Mails,	26,567 93
	<hr/>
	\$1,508,354 81

Payments other than for construction.

For transportation expenses, viz :

For passenger business,	\$62,857 16
For freight business,	89,913 70

VALUE OF MATERIALS ON HAND.

Wood, cords of,	38,216
Coal, tons of,	10
Oil, gallons of,	1,312
Waste, pounds of,	2,179
Iron rails, tons of, old,	77
Iron rails, tons of, new,	57
Chairs, pounds of,	14,212
Spikes, pounds of,	13,945
Ties, number of,	5,581
Iron and other metals, unwrought, worked and partially worked, and lumber,	\$58, 198 10

Statement of Balances on Books, August 22, 1872.

DEBIT.

Construction account,	\$3,385,535 60
Wood lots,	5,368 14
Memphremagog House,	57,783 30
Excise Tax,	156 16
Notes Receivable,	12,938 97
Superintendent, stock of wood and materials on hand,	218,327 21

Missisquoi and Clyde River R. R. Bonds,	89,000 00
Cash account,	1,308 94
Interest account,	94 29
	<hr/>
	\$3,770,512 61

CREDIT.

Coupons and dividends unc'd for,	\$5,863 37
Lyndon Lands account,	28,763 65
Stock issued,	2,124,500 00
Stock partially paid,	2,773 23
Funded Debt,	1,242,100 00
Floating Debt,	115,000 00
Trustees of Sinking Fund,	97,900 00
Reserved,	113,549 90
U. S. Government,	141 25
Earnings,	34,921 21
Grand Trunk Railway,	5,000 00
	<hr/>
	\$3,770,512 61

SUFFOLK, SS.

Boston, September 19, 1872. }

I, Nath. P. Lovering, Treasurer of the Connecticut & Passumpsic Rivers R. R. Company, do solemnly swear that the above is a true statement of the balances on the books of said company, on the 22d day of August, 1872.

N, P. LOVERING, *Treasurer.*

Sworn before me,

E. PICKERING, *Justice of the Peace.*

OFFICERS OF THE COMPANY.

Emmons Raymond, President, Boston, Mass.

Josiah Stickney, Vice President, Boston, Mass.,

N. P. Lovering, Treasurer, Boston, Mass.

Elijah Cleveland, Secretary, Coventry, Vt.

L. W. Palmer, Superintendent, Lyndonville, Vt.

H. Hastings, Cashier, Lyndonville, Vt.

H. A. Alden, Master Mechanic, Lyndonville, Vt.

H. P. Alden, General Freight Agent, Lyndonville, Vt.

N. P. Lovering, Jr., General Transportation Agent, Lyndonville, Vt.

MISCELLANEOUS QUESTIONS.

Does any conductor or engineman in the employ of your road indulge in intoxication? No.

Please answer the same question as applied to section men, brakemen and switchmen? No.

Are the trains on your road allowed to stand across highway crossings to the unnecessary delay of highway travel or business? No.

Do your station men allow boys without business to frequent and remain about the trains while stopping at stations? No.

STATE OF VERMONT, } I, L. W. PALMER, Super-
CALEDONIA COUNTY, ss. } intendent, depose and say
that the facts set forth, and statements made in the foregoing report, which has been signed by me, are true and correct according to the best of my knowledge, information and belief.

Signed, L. W. PALMER,
Superintendent C. & P. R. & M. V. R. R.

Subscribed and sworn to before me, this 9th day of October, 1872.

HUBBARD HASTINGS,
Master in Chancery.

BIENNIAL REPORT OF THE ATLANTIC AND ST. LAWRENCE RAILROAD COM- PANY.

For the two Years ending December 31, 1871.

[Accounts not being made up to June 31, 1872, statistics can only be given to December 31, 1871.]

TABLE A.

STOCK AND DEBTS.

Capital stock,	\$3,994,900
City loan,	787,000
Mortgage bonds,	1,499,916
Mortgage bonds,	712,932
standing bonds,	484,000
	<hr/>
	\$7,478,748

TABLE B.

COST OF CONSTRUCTION.

Total cost of construction as per last report,	\$6,788,354 72
Expended since last report, viz :	
For graduation and masonry, }	
bridges, }	
rails, }	
chairs, spikes and ties, }	
laying superstructure, }	
buildings and fixtures }	
	Nothing unless expended by lessees.

The road worked by lessees and particulars cannot be given.

TABLE C.

EQUIPMENT.

Total cost of equipment as per last report,	\$865,734 76
Total cost of road and equipment,	7,654,089 48

NOTE.—The Atlantic and St. Lawrence Railroad is under lease to the Grand Trunk Railway Company of Canada, and extends from Portland, Me., to Island Pond, Vt. The statistics given are for the whole length of the road, the accounts being so kept by the lessees as not to show the particulars for that part of the road running through the State of Vermont.

TABLE D.

CHARACTERISTICS OF ROAD.

*Total length of road 149 miles, from Portland, Me., to Island Pond, Vt.

Total length of road in Vermont and through what towns it runs, 14 miles.

Length of road completed, 149 miles.

Length of side track, about 28 miles on whole length of road; in Vermont, 1 1-5 mile.

Weight of rails per yard, 63 to 70 lbs.

The road was chartered Feb. 10, 1845, and completed February, 1853, to Island Pond.

CHARACTER AND LENGTH OF BRIDGING.

	No. of structures.	No. of spans.	Length of bridging in feet
File bridging,	1		1519 with one draw
Trus " 50 ft. span and under	10	10	400
Trus " from 50 to 100 ft span	2	2	120
Trus " " 100 to 150 ft span	14	14	1750
Trus " 150 ft. span and over			
Stone arch bridges,	2	2	120
Totals,	29†	28	3909

* The charter of the Atlantic and St. Lawrence Railroad within the State of Vermont, extends from Bloomfield to boundary line—30½ miles—but the 16½ miles from Island Pond to boundary line is not controlled by this Company.

† In addition to above bridges, there are on the line sixteen iron bridges, three of which are three hundred feet each in length, in spans of seventy-five ft.; one over the Connecticut River, one over Wild River, and one over Presumpscot River. Whole length of above named bridges, 1510 feet.

Number of road crossings at grade,	63
Number of road crossings above and below grade	6
Number of cross ties per mile,	2400
Chairs, number per mile,	about 600 originally, —a different course now adopted in laying rails.
Whole number of switches on main track,	about 100.

GRADIENTS AND ALIGNMENT.

Level, number of miles, and grades to 20 feet,	97.27
Maximum grade,	60
Amount of straight line, miles,	89 1-2
Amount of curved line, miles,	59 1-2
Maximum radius,	5.730
Minimum radius,	.955
Sum of ascents going in one direction,	1,929
Sum of ascents going in opposite direction,	.757
Height of termini and summit above tide water,	1.178

BUILDINGS AND FIXTURES.

Passenger houses,	29
Freight houses,	22
Engine houses,	6
Repair shops,	10
Water stations,	17
Dwellings,	2
Wood sheds,	26
Turn tables,	10

Other buildings as follows: Two hotels, ten store houses in Portland, for steamers.

EQUIPMENT.

Number of cars owned by the company, July 31, 1871:

First class 8 wheel passenger cars in good repair,	19
First class 8 wheel passenger cars wanting repair,	1
Baggage, express and mail cars in good repair,	7
Covered freight, cattle and platform 8 wheel cars,	541
Smoking and mail cars,	2

TABLE E.

BUSINESS OF THE LAST TWO YEARS TO DECEMBER 31, 1871.

Miles run by passenger trains,	330,661
Miles run by freight trains,	1,162,456
Miles run by gravel, construction and wood trains,	117,081
Number of through passengers carried in cars,	71,605
Number of way passengers,	316,133
Average speed of ordinary passenger and express trains,	20
Average speed of freight trains,	11
Rate of fare charged first class through passengers per mile,	about .03

In respect to Tables F and G, the books are so kept the questions cannot be answered in detail.

RECAPITULATION OF EXPENSES.

Total for two years ending Dec. 31, 1871, \$2,208,797 46

TABLE H.

EARNINGS, RECEIPTS AND PAYMENTS.

Earnings and receipts for two years ending Dec. 31, 1871 :

From passengers,	\$500,979 76
From freight,	1,589,719 60
From all other sources,	19,359 37
Expresses and Mails,	69,971 97
Total,	<hr/> \$2,180,030 70
Total expenditure for two years,	\$2,208,797 46

The value of materials on hand cannot be given.

NOTE.—The statement of receipts and expenses apply to the Portland Division, extending from Island Pond to Portland.

TABLE 1,

ACCIDENTS.

October 15, 1871, G. Hoofman, switchman, Island Pond, injured coupling cars,

December 1, 1871, C. Lyon, switchman, Island Pond, injured setting brake,

The employees of the Company, being employed upon two divisions of the road, one south of Island Pond, extending to Portland, the other north, extending to boundary line and Montreal, no specific statement can be made under this head, (Number of Employees and Compensation,) as applied to the State of Vermont

The earnings of the road are received by the lessees. The lessors have no interest in them while the rent is paid, and the accounts are so kept by the Grand Trunk Railway Company as not to admit of an answer to the above as contemplated—as regards surplus on hand, net earnings, disbursements, &c.

STATE OF MAINE, } This 31st day of August, 1872,
CUMBERLAND COUNTY, SS } I, Charles E. Barrett, Treasurer
of the Atlantic & St. Lawrence Railroad Company, do solemnly
swear that the above is a true statement of the condition of the
finances of said company, on the 31st day of December, 1871,
according to my best knowledge and belief,

CHARLES E. BARRETT, *Treasurer.*

Sworn before me,
W. T. SMALL, *Justice of the Peace.*

OFFICERS OF THE COMPANY.

Lessees, the Grand Trunk Railway Company of Canada.

President, J. B. Brown, Portland, Me.

Superintendent, Henry Bailey, Montreal, Ca.

Treasurer, C. E. Barrett, Portland, Me.

Does any conductor or engineman in the employ of your road indulge in intoxication? Not to our knowledge.

Please answer the same question as applied to section men, brakemen and switchmen. Not to our knowledge.

Are the trains on your road allowed to stand across highway crossings to the unnecessary delay of highway travel or business? No.

Do your station men allow boys without business to frequent and remain about the trains while stopping at stations? Not allowed, to our knowledge.

Are your road-fences and cattle-guards such as the law demands? Yes, so far as I learn.

STATE OF MAINE, } Portland, August 31, 1872. I
CUMBERLAND COUNTY, SS. } depose and say that the facts set forth, and statements made in the foregoing report, which has been signed by Samuel E. Spring, Vice President, are true and correct according to the best of my knowledge, information and belief.

Signed, S. E. SPRING, *Vice President.*

Subscribed and sworn to before me this 31st day of August, 1872.

W. T. SMALL, *Justice of the Peace.*

REPORT OF THE HARLEM EXTENSION RAILROAD COMPANY.

From Jan. 11, 1871, to Sept. 30, 1871.

STOCK AND DEBTS.

Capital stock, as by charter,	\$4,000,000 00
Amount of stock subscribed,	4,000,000 00
Amount paid in, as by last report,	4,000,000 00
Total amount now paid in of capital stock,	4,000,000 00
Funded debt, as by last report,	4,000,000 00
Total amount now of funded debt,	4,000,000 00
Floating debt, as by last report,	40,509 19
Average rate, per annum, of interest on funded debt,	7 per cent.

The equipment is owned by the lessee. The road was leased to W. B. Duncan, January 11, 1871. This report is made from January 11, 1871, to September 30, 1871, 8 months and 20 days.

COST OF ROAD AND EQUIPMENT.

For graduation and masonry, bridges, super-structure, including iron, passenger and freight stations, buildings and fixtures, engine and car houses, machine shops, machinery and fixtures, land, land damages and fences.	\$8,000,000 00
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CHARACTERISTICS OF ROAD.

Length of road in New York,	53 miles.
Length of road in Vermont,	63 "
Length of road laid,	116 "
Length of branches owned by the company, laid,	2 "

Weight of rail, per yard, on main track,	56 & 60 pounds.
Number of engine houses and shops,	2
Number of engines,	9
Number of 1st class passenger cars, (rated as 8-wheel cars,)	3
Number of 2d class and emigrant passenger cars, (rated as 8-wheel cars,	2
Number of baggage, mail and express cars (rated as 8-wheel cars,)	1
Number of freight cars (rated as 8-wheel cars,)	146
Length of main line of road from Chatham Four Corners to Rutland,	114 miles.

DOINGS OF THE YEAR IN TRANSPORTATION, AND TOTAL MILES-RUN.

Number of miles run by passenger trains,	51,060
Number of miles run by freight trains,	99,027
Number of miles run by gravel trains,	24,327
Number of passengers (all classes) carried in cars,	109,915
Number of miles traveled by passengers, or number of passengers carried one mile,	1,538,862
Number of tons, of 2,000 pounds, of freight carried in cars,	64,494
Total movement of freight, or number of tons carried one mile,	2,754,216
Average rate of speed adopted by ordinary passenger trains, including stops, (miles per hour),	20
Rate of speed of same when in motion,	25
Average rate of speed adopted by freight trains, including stops,	12
Rate of speed of same when in motion,	15
Average weight, in tons, of passenger trains, exclusive of passengers and baggage,	76
Average weight, in tons, of freight trains, exclusive of freight,	170

The amount of freight, specifying the quantity in tons, is as follows :

Of the products of the forest,	8,503
Of animals,	2,663
Of vegetable food,	679
Other agricultural products,	4,099
Manufactures,	16,431
Merchandise,	5,542
Other articles,	26,577
Total,	<hr/> 64,494

The rate of fare, per mile, for passengers is as follows :

For first class through passengers, 3 and 4 cents.

For first class way passengers, 3 and 4 cents.

EXPENSES OF MAINTAINING THE ROAD, OR REAL ESTATE, OF THE CORPORATION.

AMOUNT.	ALLOTTED TO	
	Passenger Transportation.	Freight Transportation.
Repairs of road-bed and railway, excepting cost of iron.....	\$34,919 13	\$34,919 14
Cost of iron used in repairs.....	3,107 14	5,207 88
Taxes on real estate.....	650 00	688 57
	-----	-----
Totals	\$38,676 27	\$40,815 59

EXPENSES OF REPAIRS OF MACHINERY.

	AMOUNT.	ALLOTTED TO	
		Passenger Transportation.	Freight Transportation.
Repairs of engines and tenders,	\$14,733 73	\$7,366 86	\$7,366 87
Repairs of passenger and baggage cars,	1,200 00	1,200 00	
Repairs of freight cars,	6,500 00		6,500 00
Repairs of tools and machinery in shops,	350 00	175 00	175 00
Incidental expenses, including oil, fuel, clerks, watchmen, &c., about shops,	1,825 00	912 50	912 50
Totals	<u>\$24,608 73</u>	<u>\$9,654 36</u>	<u>\$14,954 37</u>

EXPENSES OF OPERATING THE ROAD.

	AMOUNT.	ALLOTTED TO	
		Passenger Transportation.	Freight Transportation.
Office expenses, stationery, &c.....	\$3,227 71	\$1,613 85	\$1,613 86
Agents and clerks.....	10,903 57	5,951 78	5,951 79
Labor: loading and unloading freight..	3,321 18		3,321 18
Porters, watchmen and switch tenders.....	1,068 55	534 27	534 28
Wood and water station attendance.....	401 50	200 75	200 75
Conductors, baggage and brakemen.....	6,742 94	2,700 00	4,042 94
Enginemen and firemen.....	9,847 49	1,350 00	8,497 49

EXPENSES OF OPERATING THE ROAD.—*Concluded.*

	AMOUNT.	ALLOTTED TO	
		Passenger Transportation.	Freight Transportation.
Fuel: cost and labor of preparing for use,.....	23,800 00	8,100 00	15,700 00
Oil and waste for engines and tenders..	1,615 80	500 00	1,115 80
Oil and waste for freight cars.....	1,079 38		1,077 38
Oil and waste for passenger and baggage cars.....	538 61	538 61	
General superintendence	4,641 82	2,320 91	2,320 91
Contingencies.....	3,526 67	1,763 33	1,763 34
Totals...	<u>\$70,713 22</u>	<u>\$25,573 50</u>	<u>\$45,139 72</u>

EARNINGS AND CASH RECEIPTS AND PAYMENTS.

1st, Earnings.

From passengers,	\$61,554 49
From freight,	136,582 32
From other sources,	8,975 89
Total,	<u>\$207,112 70</u>

2d, Receipts.

From passengers,	\$61,554 49
From freight,	136,582 32
Mail,	4,031 44
Express,	4,694 45
Rents.	250 00
Total,	<u>\$207 112 70</u>

3d, Payments, other than for construction.

For transportation expenses.	\$174,813 81
Car service and rent of rolling stock,	42,183 25
Total,	<u>\$216,997 06</u>

NAMES AND RESIDENCE OF OFFICERS OF THE COMPANY.

Directors.—W. Butler Duncan, S. P. Slater, A. A. Selover, New York City; Trenor W. Park, No. Bennington, Vt.; Moses R. Tilden, New Lebanon; Samuel J. Tilden, Oliver Chadwick, W. Snyder, R. C. Root, New York City; James P. Hodgskin, Brooklyn, N. Y.; I. P. Hall, New York City; C. Blood, Vergennes, Vt.; H. A. Baxter, Rutland, Vt.

W. BUTLER DUNCAN, President, New York City. S. P. SLATER, Treasurer, New York City. R. C. ROOT, Secretary, New York City. F. C. WHITE, Superintendent, Bennington, Vt.

Communications intended for this Company should be addressed to General Office, New Lebanon, Columbia County, N. Y., or to F. C. White, Superintendent, Bennington, Vt.

REPORT

OF THE

MONTPELIER AND WELLS RIVER RAILROAD COMPANY.

HON. R. F. PARKER,

Railroad Commissioner of the State of Vermont:

DEAR SIR:—Herewith I hand you an abstract of the present condition and progress of the Montpelier and Wells River Railroad, its officers, their post office address, &c.

The railroad was chartered Nov. 6, 1867, by and under the name of the Montpelier and Wells River Railroad.

OFFICERS OF THE COMPANY.

Messrs. Roderick Richardson, Boston, Mass.; Isaac N. Hall, Groton, Vt.; Charles H. Heath, Plainfield, Vt.; Jacob Smith, Montpelier, Vt.; James G. French, Montpelier; Joel Foster, Jr., Montpelier; Eli S. Pitkin, Marshfield, Vt.; Geo. B. Fessenden, Wells River, Vt.; Isaac W. Brown, Montpelier.

President—Hon. Roderick Richardson.

Vice President—Hon. Isaac N. Hall.

Clerk—Joel Foster, Jr.

Contractor—Norman C. Munson, Boston.

Chief Engineer—Chas. K. Walker, Manchester, N. H.
Treasurer—Hon. John A. Page, Montpelier.

The road connects with the Vermont Central at Montpelier, and passes through portions of the following towns, to wit: Montpelier, Berlin, Barre, East Montpelier, Plainfield, Marshfield, Peacham, Groton, Ryegate and Newbury, connecting with the Passumpsic, and Boston, Concord and Montreal Railroads at Wells River Village in the town of Newbury, Vt.

The length of road, 37 miles and 5,040 feet. The subgrading of the road is nearly completed. The masonry is all finished except the completion of one set of bridge abutments. There are eight river bridges. Timber nearly all delivered. Three are built, two more in process of construction.

All the ties for the completion of the road are under contract; seven-eighths of them are cut and two thirds are delivered on the line of the road,

The treasurer has paid to the contractor three hundred and seventy-eight thousand dollars in money, two hundred thousand in railroad bonds, secured by a mortgage of the road.

The directors contracted the building of the road to Norman C. Munson for the sum of ten hundred and sixty-five thousand (1,065,000) dollars; four hundred thousand dollars to be paid in money, and the balance, six hundred and sixty-five thousand, to be paid in railroad bonds secured by a mortgage on the road, and for the consideration of the said money and bonds, the said Munson agrees to build the main line with two miles side track to the full completion, including grading, masonry, bridges, iron and all the other utensils necessary to complete the road for the rolling stock.

The stock subscription in money is four hundred and ten thousand dollars. The stockholders have authorized the

directors, by vote, to issue eight hundred thousand dollars in bonds, and secured by a first mortgage on the road.

All of which I respectfully submit, according to my best knowledge.

Yours truly,

JOEL FOSTER, Jr.

Clerk of M. & W. R. R. R. Co.

Montpelier, August 14, 1872.

BLANK FORM OF RAILROAD REPORT.

—————

The following is the blank form furnished the Officers and Managers of the several Vermont Railroads :

<i>Biennial report of the</i>	<i>Railroad</i>
<i>Company, for the years ending</i>	<i>187</i>

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TABLE A.

STOCK AND DEBTS.

NOTE.—Under this head state the amount paid for interest, discount, &c., amount of funded debt, floating debt, rate of interest, amount of interest coupons due and unpaid.

TABLE B.

COST OF CONSTRUCTION.

Total cost of construction as per last report,

Expended since last report, viz :

For graduation and masonry,

Bridges,

Rails,

Chairs, spikes and ties,

Laying superstructure,

Buildings and fixtures,

NOTE.—State, also, the progress of the work, cost of graduation, superstructure, and all incidental expenses, or any extension or alteration of road, to

187

Also state the amount for interest, discount, etc., charged to construction account.

TABLE C.

EQUIPMENT.

Total cost of equipment as per last report,

Expended since last report,

Total cost of road and equipment,

TABLE D.

CHARACTERISTICS OF ROAD.

Total length of road,
 Total length of road in Vermont, and through what towns
 it runs,
 Length of road completed,
 Length of branches,
 Length of side track,
 Weight of rails per yard,

NOTE.—State when chartered, and when completed, and ready for use.

CHARACTER AND LENGTH OF BRIDGING.

	No. of structures.	No. of spans.	Length of bridging in feet.
Trestle bridging,			
Truss bridging, 50 feet span and under,			
Truss do. from 50 to 100 feet span,			
Truss do. from 100 to 150 feet span,			
Truss do. 150 ft. span and over			
Draw bridges,			
Totals,			

Number of road crossings at grade,
 Number of road crossings above and below grade,
 Number of cross ties per mile,
 Chairs, number per mile,
 Whole number of switches on main track,

GRADIENTS AND ALIGNMENT.

Level, number of miles,
 Maximum grade,
 Amount of straight line, miles,
 Amount of curved line, miles,

Maximum radius,
 Minimum radius,
 Sum of ascents going in one direction,
 Sum of ascents going in opposite direction,
 Hight of termini and summit above tide water,

BUILDINGS AND FIXTURES.

Passenger houses,
 Freight houses,
 Engine houses,
 Repair shops,
 Water stations,
 Dwellings,
 Wood sheds,
 Turn tables,

Other buildings as follows :

EQUIPMENT.

Number of locomotives owned by the company on the
 day of 187

	Under 18 tons.	16 to 20 tons.	20 to 25 tons.	25 to 30 tons.	30 tons and over
In good repair,					
Requiring slight repairs,					
Requiring heavy repairs,					
Worn out,					
Total,					

Number of cars owned by the company,
 First class 8 wheel sleeping cars in good repair,
 First class 8 wheel passenger cars in good repair,
 First class 8 wheel passenger cars wanting repair,
 Second class 8 wheel passenger cars in good repair,
 Second class 8 wheel passenger cars wanting repair,
 Baggage, express and mail cars in good repair,
 Baggage, express and mail cars wanting repair,

Covered freight and cattle 8 wheel cars in good repair,
 Covered freight and cattle 8 wheel cars wanting repair.
 Platform 8 wheel cars in good repair,
 Other freight cars,
 Gravel cars,
 Average weight of passenger cars,
 do do baggage do
 do do box do
 do do platform do
 do do sleeping do

TABLE E.

BUSINESS OF THE LAST TWO YEARS.

Miles run by passenger trains,
 Miles run by freight trains,
 Miles run by gravel and construction trains,
 Miles run by wood trains,
 Number of through passengers carried in cars,
 Number of way passengers,
 Number of passengers having passes,
 Number of miles traveled by way passengers,
 Average distance traveled by way passengers,
 Number of miles traveled by passengers having passes.
 Number of tons of through freight,
 Number of tons of way freight,
 Number of tons of way freight carried 1 mile,
 Average rate of speed of ordinary passenger trains,
 Average rate of speed of express trains,
 Average rate of speed of freight trains,
 Rate of fare charged first class through passengers, per
 mile,
 Average rate of fare charged second class passengers per
 mile,
 Rate per ton per mile charged on 1st class through freight,
 Rate per ton per mile charged on 2d class through freight,

Rate per ton per mile charged on 3d class through freight,
 Rate per ton per mile charged on 4th class through freight,
 Rate per ton per mile charged on 1st class way freight,
 Rate per ton per mile charged on 2d class way freight,
 Rate per ton per mile charged on 3d class way freight,
 Rate per ton per mile charged on 4th class way freight,
 Total number of tons of freight carried 1 mile,
 Total number of passengers carried 1 mile,

TABLE F.

EXPENSES IN MAINTAINING ROADWAY AND REAL ESTATE FOR THE TWO
YEARS ENDING 187

Ordinary repairs of road bed and superstructure,
 Extraordinary repairs of road bed,
 Cost of new rails used in repairs,
 Number and weight of chairs,
 Weight of spikes,
 Cost of repairs of rails,
 Number of cross ties used for renewals,
 Cost of same,
 Cost of relaying rails and ties,
 Insurance and taxes on real estate,
 Repairs of bridges,
 do stations,
 do fences,
 do masonry,
 Total,

COST OF REPAIRS OF MACHINERY.

Repairs of engines and tenders,
 Repairs of passenger and baggage cars,
 Repairs of freight cars,
 Repairs of tools and machinery in workshops,
 Oil used about workshops,

Fuel,

Waste,

Other items in detail as follows :

Total,

TABLE G.

COST OF OPERATING THE ROAD FOR THE LAST TWO YEARS ENDING
187

Wood, including the cost of preparing the same,

Number of cords of wood used by locomotives,

do tons of coal do do

do cords of wood used at stations,

do tons of coal do do

do cords lost by fire,

do gallons of oil,

do pounds of waste,

Cost of oil and waste for engines and tenders,

do do passenger and baggage cars,

do do freight cars,

Loss and damage of goods,

Loss and damage of baggage,

Damages for injuries to persons,

Damages to property, including fire, and animals killed on
road,

Office expenses and stationery,

Number of agents,

Number of clerks,

Labor, loading and unloading freight,

Porters and watchmen,

Switchmen,

Wood and water station attendance,

Brakemen,

Conductors and baggagemen,

Enginemen and firemen,

For salaries of trustees, president, directors, secretaries,
treasurer and superintendent,

For printing, stationery and office expenses,
 For law expenses,
 Other expenses, in detail as follows :
 Total,

RECAPITULATION OF EXPENSES.

Maintaining roadway,
 Repairs of machinery,
 Operating,
 Proportion of expenses due to passenger business.,
 Proportion of expenses due to freight business,
 Total,

TABLE H.

EARNINGS, RECEIPTS AND PAYMENTS.

Earnings and Receipts.

From passengers,
 From freight,
 From other sources, viz :
 Expresses,
 Mails,
 Rents,
 Miscellaneous,

Payments other than for construction.

For transportation expenses, viz :
 For passenger business,
 For freight business,
 For other business, and what,
 For interest on funded debt,
 For interest on floating debt,
 For dividends,
 For carried to surplus fund,
 For amount of surplus fund,

VALUE OF MATERIALS ON HAND.

Wood, cords of,
Coal, tons of,
Oil, gallons of,
Waste, pounds of,
Iron rails, tons of, old,
Iron rails, tons of, new,
Chairs, pounds of,
Spikes, pounds of,
Ties, number of,
Iron and other metals, unwrought,
Iron and other metals, worked and partially worked,
Lumber,

Other items specified as follows :

COST OF TRANSPORTATION.

Actual cost of transporting freight per ton, per mile,
Actual cost of transporting passengers, per mile,

DETAILS OF EARNINGS FOR THE YEAR ENDING

187

SOURCE.	SEPTEMBER.	OCTOBER.	NOVEMBER.	DECEMBER.	JANUARY.	FEBRUARY.
Through passengers,						
Way passengers,						
Through freight,						
Way freight,						
Express,						
Transport of mails,						
Use of engines,						
Use of cars,						
Rent,						
Other earnings specified in detail as follows:						
Total,						

DETAILS OF EARNINGS—CONTINUED.

SOURCE.	MARCH.	APRIL.	MAY.	JUNE.	JULY.	AUGUST.
Through passengers,						
Way passengers,						
Through freight,						
Way freight,						
Express,						
Transport of mail,						
Use of engines,						
Use of cars,						
Rent,						
Other earnings specified in detail as follows:						
Total,						

DETAILS OF EARNINGS FOR THE YEAR ENDING 187

SOURCE.	SEPTEMBER.	OCTOBER.	NOVEMBER.	DECEMBER.	JANUARY.	FEBRUARY.
Through passengers,						
Way passengers,						
Through freight,						
Way freight,						
Express,						
Transport of mails,						
Use of engines,						
Use of cars,						
Rent,						
Other earnings specified in detail as follows:						
Total,						

DETAILS OF EARNINGS—CONTINUED.

SOURCE.	MARCH.	APRIL.	MAY.	JUNE.	JULY.	AUGUST.
Through passengers,						
Way passengers,						
Through freight,						
Way freight,						
Express,						
Transport of mail,						
Use of engines,						
Use of cars,						
Rent,						
Other earnings specified in detail as follows:						
Total,						

TABLE I.

ACCIDENTS.

State the number of persons injured in life or limb, and the cause thereof, and whether passengers or persons employed.

State whether any such accidents have arisen from carelessness or negligence of any person in the employ of the corporation, and whether such person is retained in the service of the corporation.

	EMPLOYEES		OTHERS	
	Killed.	Injured.	Killed.	Injured.
Trains thrown from track,				
Struck by bridge while on				
top of freight cars,				
Run over while walking on				
track,				
Injured at road crossing,				
Total,				

Total number of persons killed,

Total number of persons injured but not killed,

In addition to which must be given a statement of the date of each accident, the place where it occurred, the train, the cause and the extent of the injuries inflicted upon each person, and the name of such person.

TABLE J.

NUMBER OF EMPLOYEES AND COMPENSATION.

Conductors of passenger trains,

Amount of compensation,

Conductors of freight trains,

Amount of compensation,

Conductors of wood and gravel trains,

Amount of compensation,

Master mechanics,

Amount of compensation,

Road masters,
Amount of compensation,

Men in repair shops,
Amount of compensation,

Enginemen of passenger trains,
Amount of compensation,

Enginemen of freight trains,
Amount of compensation,

Enginemen of wood and gravel trains,
Amount of compensation,

Firemen,
Amount of compensation,

Baggagemen,
Amount of compensation,

Switchmen,
Amount of compensation,

Section men, (foremen),
Amount of compensation,

Section hands,
Amount of compensation,

Watchmen,
Amount of compensation,

Station agents,
Amount of compensation,

Other laborers,
Amount of compensation,

Clerks connected with passenger business,
Amount of compensation,

Clerks connected with freight business,
Amount of compensation.

Superintendent of bridges—salary

Wood agent “

Other agents—how employed—and the salary of each as follows, viz :

The Treasurer is required to state the amount of surplus (if any), the amount of net earnings, on 187 , also the net earnings up to 187 , and to state amount of payments to surplus fund payments of interest, coupons, on funded debt, and other disbursements in detail, so as to show the true condition of the finances of the company on the day of , 187 . Such statement may be in the form of a general account and must be verified by the oath of the Treasurer.

STATE OF VERMONT, }
COUNTY, SS. } day of 187

I, Treasurer of the
railroad company, do solemnly swear that the
above is a true statement of the condition of the finances of
said company, their trustees, or assignees or lessees, on the
day of , 187 .

Treasurer.

Sworn before me,

Justice of the Peace.

OFFICERS OF THE COMPANY.—SALARIES.

Trustees,
President,
Superintendent,
Treasurer,

NOTE.—State the amount of each.

State names of officers, with their post office address,

Does any conductor or engineman in the employ of your road indulge in intoxication?

Please answer the same question as applied to section men, brakemen and switchmen.

Are the trains on your road allowed to stand across highway crossings, to the unnecessary delay of highway travel or business?

Do your station men allow boys without business to frequent and remain about the trains while stopping at stations?

Are your road fences and cattle-guards such as the law demands?

STATE OF VERMONT, }
COUNTY, SS. }

depose and say that the facts set forth, and statements made in the foregoing report, which has been signed by , are true and correct according to the best of knowledge, information and belief.

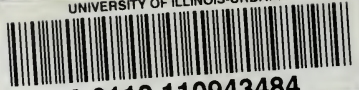
Signed,

Subscribed and sworn to before me this day of
187 . .





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